

Action Transmittal

Transportation Advisory Board



Meeting Date: January 19, 2023

Date: January 12, 2023

Action Transmittal: 2023-10

Program Year Extension Request: Saint Paul Kellogg Bridge Replacement

To: TAC Funding & Programming Committee

Prepared By: Joe Barbeau, Senior Planner, phone 651-602-1705

Requested Action

Saint Paul requests a program year extension for its Kellogg Bridge replacement (SP# 164-158-028) from fiscal year 2023 to fiscal year 2024.

Recommended Motion

That the Funding & Programming Committee recommend that TAB approve Saint Paul's request to extend its Kellogg Bridge replacement (SP# 164-158-028) from fiscal year 2023 to fiscal year 2024.

Summary

Saint Paul was awarded \$7M in the 2020 Regional Solicitation to replace the Kellogg/3rd Street Bridge from Broadway Street to Maria Avenue. The city requests that the project be extended from 2023 to 2024 after supply-chain and cost concerns.

Background and Purpose

In the 2020 Regional Solicitation, the City of Saint Paul was awarded \$7M to replace the Kellogg/3rd Street Bridge (Broadway St. to Maria Ave). The project was programmed for fiscal year 2023. The city is requesting that the project be extended to fiscal year 2024 due to supply-chain shortages and 17.5% inflation for this type of bridge from 2020 to 2021.

Relationship to Regional Policy

The Transportation Advisory Board (TAB) adopted the Program Year Policy in April 2013 (updated in August 2014) to assist with management and timely delivery of transportation projects awarded federal funding through the TAB's Regional Solicitation. The policy includes a procedure to request a one-year extension based on extenuating circumstances within certain guidelines.

Staff Analysis

Per the Program Year Policy's progress assessment (attached) a minimum score of 7 is needed to be eligible for an extension. This process helps assess whether the project is in position to be able to be obligated with the one-year extension. The request obtained a score of 10. Therefore, staff recommends approval of the request.

An extension of the program year does not guarantee federal funding will be available in that year. The project sponsor is responsible for completing the project in the new program year and

covering the federal share of the project until federal funding becomes available. At this time the project would be in line for 2028 reimbursement of federal funds, though an earlier reimbursement may occur if funding becomes available.

Routing

To	Action Requested	Date Completed (Scheduled)
TAC Funding & Programming Committee	Review & Recommend	<i>January 19, 2023</i>
Technical Advisory Committee	Review & Recommend	<i>February 1, 2023</i>
Transportation Advisory Board	Review & Adopt	<i>February 15, 2023</i>





December 27, 2022

Mr. Michael Thompson
Chair, TAC Funding and Programming Committee
Metropolitan Council
390 Robert Street North
St. Paul, MN 55101-1805

RE: PROGRAM YEAR EXTENSION REQUEST FOR SP 164-158-028, MSAS 158 (KELLOGG/3RD ST) FROM BROADWAY ST TO 0.04 MI W OF MARIA AVE IN ST PAUL – REPLACE BRIDGE NOS. 62080 AND 62080A WITH NEW BRIDGE 62666 INCLUDING APPROACH ROADWAYS AND INTERSECTIONS

Dear Mr. Thompson,

I am writing on behalf of the City of Saint Paul to respectfully request that the Funding and Programming Committee consider the program year extension for the reconstruction of Bridge No. 62080/62080A as referenced above.

“Kellogg-Third” Bridge No. 62080/62080A is a significant structure with a complex history and regional importance, connecting Lowertown Saint Paul with Dayton’s Bluff and T.H. 61 (Mounds)/I-94. The bridge functions as a gateway to major downtown facilities including CHS Field, Farmer’s Market and Union Depot.

The existing bridge was designed in 1980 and built in 1982 by MnDOT. Soon after, MnDOT re-routed T.H. 5 to follow E 7th Street and transferred ownership of Kellogg Boulevard and the Kellogg-Third Bridge to the City by Commissioner’s Orders. The structure is the longest (and one of the tallest) bridges owned and maintained by the City of Saint Paul. In 2014, structural analysis of bridge supports found that traffic cannot be supported on the outer portions of the bridge deck. Traffic was immediately removed from those portions of the bridge. City of Saint Paul committed local funds and sought external state and federal funds for a replacement bridge. In 2020, the project was awarded \$7M in Federal Regional Solicitation Grants, and State Legislature dedicated an additional \$52M of General Obligation Bonds to the project.

Design and engineering tasks are currently at 95% phase, on schedule to be “shovel-ready” in 2023.

Over the past two years, the bridge and road construction industry experienced widespread supply chain shortages and unprecedented cost escalations. Consecutive-year double-digit cost increases to numerous items (such as prestressed beams, disc bearings and steel reinforcement) outpaced historic inflation rates





SAINT PAUL
PUBLIC WORKS

DEPARTMENT OF PUBLIC WORKS
SEAN KERSHAW, DIRECTOR

25 West 4th Street, 1500 City Hall Annex
Saint Paul, MN 55102
Tel: 651-266-6100 | Fax: 651-266-6222

and resulted in a funding shortfall that must now be addressed to successfully deliver the project. As the City looks to close the funding gap in 2023, we respectfully ask that your office, with the help of your chairmanship, will embrace this project and provide your support for extension of the program year to 2024.

Should you require additional information, please do not hesitate to contact me directly at 651-266-6203 or at Paul.Kurtz@ci.stpaul.mn.us.

Sincerely,

Paul Kurtz, City Engineer
Department of Public Works

CC: Colleen Brown, MnDOT Federal Aid
Brent Christensen, Project Engineer (Saint Paul)
Glenn Pagel, City Bridge Engineer (Saint Paul)
Anne Weber, TAC F&P Committee Member (Saint Paul)

REQUEST FOR PROGRAM YEAR EXTENSION

For

SP 164-158-028

MSAS 158 (KELLOGG/3RD ST) FROM BROADWAY ST TO 0.04 MI W OF MARIA AVE IN ST PAUL – REPLACE BRIDGE NOS. 62080 AND 62080A WITH NEW BRIDGE 62666 INCLUDING APPROACH ROADWAYS AND INTERSECTIONS

City of Saint Paul, MN

REQUESTED BY:

Paul Kurtz

Phone: +1-651-266-6203

Email:

Paul.Kurtz@ci.stpaul.mn.us

1. PROJECT BACKGROUND

a) Project Name: SP 164-158-028, MSAS 158 (KELLOGG/3RD ST) FROM BROADWAY ST TO 0.04 MI W OF MARIA AVE IN ST PAUL – REPLACE BRIDGE NOS. 62080 AND 62080A WITH NEW BRIDGE 62666 INCLUDING APPROACH ROADWAYS AND INTERSECTIONS

b) Location Map: City of Saint Paul, MN

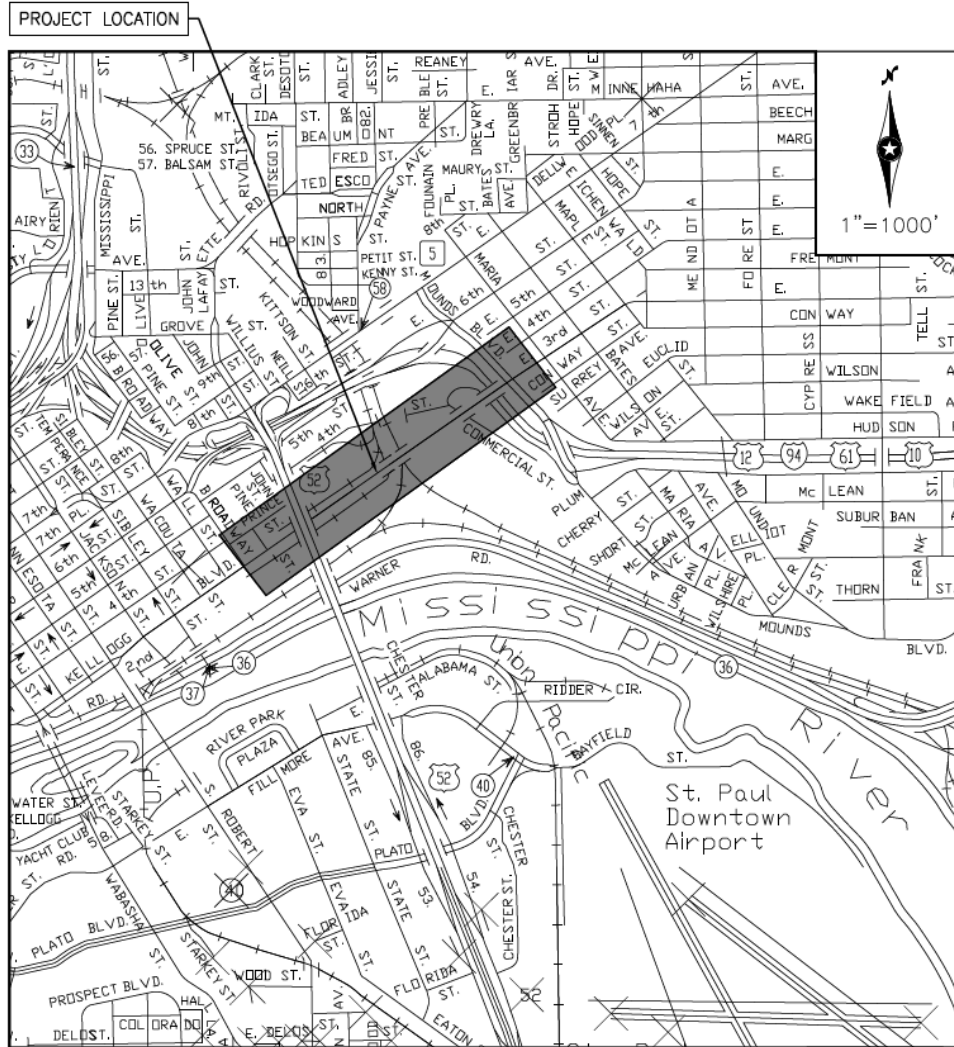


Figure 1 – Project Location Map

c) Sponsoring Agency: City of Saint Paul, MN

d) Other Participating Agencies: N/A

e) Project Description: Reconstruct Bridge Nos. 62080/62080A, walls, approach roadways and intersections including signal systems, on Kellogg Boulevard/Third Street from Broadway Street to Mounds Boulevard

f) Funding Category:

The project is funded as tabulated below:

Source	Amount (\$)
Federal Regional Solicitation Grant LBRP FY 2023	7,000,000.00
State GO Bonds STBGP	52,000,000.00
Local Sources	3,737,000.00
Total Budget	62,737,000.00

g) Federal Funds Allocated:

Federal funds in the amount of \$7,000,000 have been secured for Fiscal Year 2023.

2. PROJECT PROGRESS

a) Project Schedule:

The list below outlines the project schedule from preparation of preliminary plans to approval of project memorandum to approval of final plans for construction. The dates highlight current project status and upcoming milestones for the project. Future activity dates anticipate program year extension.

Completed/In Progress Activity / Milestone Date Completed

- MnDOT Level 2 Layout Completed – 9/16/2020
- BNSF/UP Railroad Concept Plan Submittal 7/16/21
- BNSF/UP Railroad 30% Plan Submittal 10/27/21
- 60% Roadway Plans Submitted 11/05/21
- Preliminary Bridge Plans Approved 11/08/21
- Draft Project Memorandum (CATEX) Submitted – 12/1/21
- 60% Bridge Plans Submitted 4/18/22
- 90% Bridge & Roadway Plans, Specifications, Estimates, and 60% Design comment responses Submitted 9/12/22
- BNSF/UP Railroad Final Plan Submittal 9/12/22
- UP Railroad Final Plan Approval 10/18/22
- BNSF Railroad Final Plan Approval 11/8/22
- Final Project Memorandum (CATEX) Approved 11/16/2022
- UP Railroad Construction & Maintenance Agreement – Started 12/1/22 (in progress)
- BNSF Railroad Construction & Maintenance – Started 12/1/22 (in progress).

Future Activity / Anticipated Completion Date

- ROW Certificate #1 – September 2023*
- Final Plans Approval and Authorization – October 2023
- Project Bid Opening – December 2023
- Desired Construction Start – January/February 2024

*See part (b) for detail related to ROW Certificate completion and subsequent future activity dates.

b) Right of Way Acquisition:

The City began conducting its Real Estate activities in February 2020, clarifying land ownership and existing property rights upon the 22 parcels that are affected by project activities. Several right-of-way tasks were successfully completed as appropriate and necessary to reconstruct the bridge. For example, in September 2021 the City recorded an amendment to existing DNR Conservation Easement on city-owned parcels within Bruce Vento Nature Sanctuary (below the central portion of the bridge). Also starting in 2021, the City approached all property owners to coordinate construction needs and acquire permanent and/or temporary easements. The City performed title work and property valuation of all affected parcels. Offer letters were sent out in Spring 2022, and City Real Estate staff negotiated terms and easement offers with property owners into Summer 2022. At the time of the 95% engineer's cost estimate, the City was on track to acquire all necessary easements by year-end 2022 and to submit Right of Way Certificate #1 early in 2023, supporting a June 2023 project authorization date and bidding and construction activities in the second half of 2023. Upon confirming a project funding shortfall, the City approached property owners to determine willingness to amend terms such that the shortfall could be adequately addressed. Options considered included specifying a 'floating' start date that would be tied to Contract Award, or increasing the duration of easements from three years to four or five years. Property owners presented with such options indicated opposition to open-ended start dates and concerns about future valuations or the taking of longer easements that could restrict the sale or redevelopment of their property. Likewise, the City recognizes a significant future risk in entering into easements before securing necessary project funds, should they expire during construction. Negotiating new easements or extensions during construction would be complicated and costly, especially considering the number of affected parcels.

Notably, at the time that the funding shortfall was identified in September 2022, the City had already made offers, met with all affected property owners, and negotiated mutually-acceptable terms for permanent and (three-year) temporary easements. It is expected that when funding is resolved and a construction timeframe is re-established, right-of-way acquisition will resume and be completed in a timely manner and without need for condemnation. Project schedule dates noted in section (a) above assume that project funding will be secured during the first half of 2023, and allows for a 90-day period to secure right-of-way acquisition agreements from property owners (which include private owners, county agency, and railroads).

Right-of-way plan sheets are included as attachments to this narrative.

c) Plans:

The City's design consultant (SRF Consulting) completed 95% construction plans in September 2022 and submitted to review agencies: MnDOT State Aid, Union Pacific Railroad, and BNSF Railway. Final comments were received by all external agencies. The City has since put final plan and specification revisions on temporary hold, acknowledging the critical path priorities of project financing and right-of-way acquisition. The City and its consultant expect that the minor remaining design tasks can be completed within 90 days.

Cover sheets, general layouts, typical sections, right-of-way plans, and bridge general plan and elevation sheets are provided as attachments to this narrative.

d) Permits:

Table 1 – Permits required.

PERMITS		
Permitting Agency	Required (Y/N)	Permit Status
DNR	No	No DNR properties will be impacted
MPCA – NPDES	Yes	Application completed, awaiting submittal of permit fee by City.
Capitol Regions Watershed District	Yes	Conditionally approved, awaiting approval of NPDES permit.
US Army Corp. of Engineers	Yes	Completed – Falls under USACOE Transportation Region General Permit
Board of Water and Soil Resources	No	Concerns waived under the USACOE General Permit
Railroad	Yes	In progress by Union Pacific and BNSF Railroads.
MnDOT	Yes	Detour permit application to be submitted with final design plans.
FAA	Yes	Completed – FAA obstruction permit obtained on 9/19/2022

e) Approvals:

The following is a list of agencies with approval authority and the status of each approval:

Table 2 – Agency Approval Required

AGENCY APPROVAL REQUIRED		
Agency for Approval	Requirement	Status
MnDOT State Aid	Project Memorandum	Approved 11/16/2022
	Preliminary Plans	Prelim bridge approved 11/8/21 60% roadway plans submitted 11/05/21 60% bridge plans submitted 4/18/22 90% roadway & bridge plans submitted 9/12/22
	Final Plans and Specifications	Anticipated March 2023
Union Pacific Railroad	Concept Submittal	Completed – 7/16/21
	30% Submittal	Completed – 10/27/21
	Final Submittal	Approved – 10/18/22
BNSF Railway	Concept Submittal	Completed – 7/16/21
	30% Submittal	Completed – 10/27/21
	Final Submittal	Approved – 11/08/22

f) Identified Funds Spent to Date on Project:

City of Saint Paul committed local project funds to advance the design phase to 95% completion status. Approximate expenditures to December 2022 include \$3M for City staff project management and administration, consultant engineering services, and external review agency fees.

3) Justification for Extension Request:

a) What is unique about this project that requires an extension of the program year?

The existing bridge was built in 1982 by MnDOT. Soon after, MnDOT re-routed T.H. 5 to follow E 7th Street and transferred ownership of Kellogg Boulevard and the Kellogg-Third Bridge to the City by Commissioner's Orders. The structure became the longest (and one of the tallest) bridges owned and maintained by the City of Saint Paul. The bridge replacement need was a sudden and unanticipated result of a 2014 structural analysis of the bridge supports, which found that traffic cannot be supported on the outer portions of the bridge deck. The City sought project funding for several years, and in 2020 was awarded \$7M Federal Regional Solicitation Grant and \$52M State General Obligation Bond funds. At the time of funding, total budget was adequate to deliver the project, and the City began design.

Recent world events, including COVID-19 pandemic and Russian invasion of Ukraine, contributed to widespread supply chain shortages and unprecedented inflation rates. Two reports that serve to illustrate and quantify these increases are provided for reference:

- 1) The most recently published (2021) MnDOT State Aid Bridge Office Cost Report recorded a 17.5% increase in average bridge cost from 2020 to 2021, for prestressed precast concrete beam bridge type. https://edocs-public.dot.state.mn.us/edocs_public/DMResultSet/download?docid=17981678
- 2) Associated General Contractors (AGC) published a July 2022 Construction Inflation Alert that utilized U.S. Bureau of Labor Statistics data and indicated alarming production price index (PPI) trends: for example, the steel mill index more than doubled between April 2020 and June 2022, and the bid price index soared 19.8% from June 2021 to June 2022. https://www.agc.org/sites/default/files/users/user21902/Construction%20Inflation%20Alert%20Cover_Jul2022_V4.pdf

Comparison of project cost estimates prepared at key stages of design (preliminary in January 2020, interim in May 2021, and final in 2022) track successive project cost increases that resemble the 2020-2021 and 2021-2022 bid price reports noted above. In Summer 2022, the project team attempted to offset price increases with cost-reduction measures. However, because the replacement bridge already utilizes the most economical methods of bridge construction available (driven steel pile foundation, reinforced concrete substructures, prestressed precast concrete beams, structural concrete deck, standard slip-form barrier) very few material substitutions, scope reductions, or design alternatives exist to reduce the cost of the bridge as-proposed.

Though not without challenge, a local agency such as the City of Saint Paul could potentially mitigate unprecedented cost inflation on its local bridge projects, by re-allocation of its local road and bridge improvement program, and deferral of some lesser priority projects. Kellogg-Third Bridge is no ordinary local bridge structure, however, and the immense scale of the bridge, multiplied by actual 2020-2022 bid price increases, results in a substantial and unforeseeable funding gap.

Extension of the program year is necessary to preserve the \$7M Federal Regional Solicitation Grant funds that have been allocated to the project while the City acts to resolve the funding gap in 2023.

b) What are the financial impacts if this project does not meet its current program year?

If the project is not delivered in FY 2023, the existing Federal funds are at-risk and the project funding shortfall will increase by an additional \$7M.

c) What are the implications if the project does not obtain the requested extension?

If the project does not obtain the requested extension, the City of Saint Paul may be unable to secure necessary right-of-way (temporary easements that require known construction duration) and advance the project to authorization. Consequently, the existing Federal funds are at-risk and the project funding shortfall will increase by an additional \$7M.

d) What actions will the agency take to resolve the problems facing the project in the next three to six months?

The City's top priority will be to solve the funding gap associated with the Kellogg-Third Bridge Replacement Project. The City will continue to evaluate and pursue all potential sources for additional funding at local, state and federal levels.

- As a recurrent program, Regional Solicitation Grant is one important component of capital project planning.
- Between 2015 and 2020, the City requested direct appropriation for the State legislature, and is considering submitting a 2023 request, citing the unprecedented cost escalations that were experienced in years following State contribution to the project.
- Other special Federal Grant sources have been evaluated for funding viability, including the Bridge Improvement Program (BIP) and Reconnecting Communities. The City will submit the Kellogg-Third Bridge Replacement Project to any and all grants for which the project appears to be a likely candidate for program award.

ATTACHMENTS:

95% Roadway Set

Cover Sheet

General Layout

Typical Sections (8 sheets)

Right-of-Way Plans (4 sheets)

95% Bridge Set

Cover Sheet

General Plan and Elevation (4 sheets)

Bridge Underpass Detail and Typical Section (2 sheets)

Regional Program Year Policy

TAB Adopted: April 17, 2013
Administrative Modifications: August 20, 2014

Attachment 1: Progress Schedule for Program Year Extension

Enter request date

INSTRUCTIONS:

1. Check status of project under each major heading.
2. Enter dates as requested for each major heading.
3. Enter points as suggested by each applicable response.
4. Total points received in the TOTAL POINTS line on the last page. **The minimum score to be eligible to request an extension is seven points.**

ENVIRONMENTAL DOCUMENTATION

PROJECT MEMORANDUM

Reviewed by State Aid If checked enter 4. _____
Date of approval 05/18/22

Completed/Approved If checked enter 5. 5
Date of approval 11/16/22

EA

Completed/Approved If checked enter 2. _____
Date of approval _____

EITHER

Not Complete
Anticipated Date of Completion _____
If prior to January 31 of the program year, enter 1. _____

OPPORTUNITY FOR PUBLIC HEARING (not necessary for project memorandum)

Completed
Date of Hearing _____ If checked enter 2. _____

Not Complete
Anticipated Date of Completion _____
If prior to February 28 of the program year, enter 1. _____

FINAL ENVIRONMENTAL ASSESSMENT (not required for project memorandum)

Completed/FONSI Approved If checked enter 2. _____
Date of approval _____

Not Complete
Anticipated Date of Completion _____
If prior to March 31 of the program year, enter 1. _____

STUDY REPORT (required for Environmental Assessment Only)

Complete/Approved If checked enter 1. _____
Date of Approval _____

Not Complete
Anticipated Date of Completion _____

Regional Program Year Policy

TAB Adopted: April 17, 2013

Administrative Modifications: August 20, 2014

CONSTRUCTION PLANS

_____ Completed (includes signature of District State Aid Engineer)
Date _____ If checked enter 3. _____
 Completed (approved by District State Aid as to SA Standards but not signed)
Date 10/05/22 If checked enter 2. 2
_____ Not Complete
Anticipated Date of Completion _____
If prior to June 30 of the program year, enter 1. _____

RIGHT OF WAY ACQUISITION

_____ Completed (includes approval of R/W Cert. #1 or #1A) If checked enter 2. _____
Date _____
 Not Complete
Anticipated Date of Completion 09/31/23
If prior to December 31 of the year following the original program year, enter 1. 1

ENGINEERS ESTIMATE OF COSTS

Completed If checked enter 2. 2
Date 09/12/22
_____ Not Complete
Anticipated Date of Completion _____
If prior to December 31 of the year following the original program year, enter 1. _____

AUTHORIZED

Anticipated Letting Date 12/01/23.
Anticipated letting date must be prior to June 30
in the year following the original program year,
so that authorization can be completed prior to
June 30 of the extended program year.

TOTAL POINTS

10

MINNESOTA DEPARTMENT OF TRANSPORTATION CITY OF ST. PAUL RAMSEY COUNTY, MINNESOTA

BRIDGE NO. 62666
S.P. 164-158-028
C.P. B-1202
LOCATED ON: KELLOGG BOULEVARD
FROM: BROADWAY STREET
TO: 0.04 MILES W. OF MARIA AVE

GROSS LENGTH3720.86... FEET0.705... MILES
BRIDGE LENGTH2106.50... FEET0.399... MILES
EXCEPTIONS LENGTH FEET MILES
NET LENGTH3718.86... FEET0.704... MILES

PLANS FOR: GRADING, AGGREGATE BASE, BITUMINOUS PAVING, SIGNAL LAYOUTS, DRAINAGE, RETAINING WALLS, LIGHTING,
LANDSCAPE DESIGN, AND BRIDGE NO. 62666

PLAN SYMBOLS

PRESENT RIGHT-OF-WAY LINE	----
PROPERTY LINE	----
CONC. RETAINING WALL	----
RAILROAD	----
RAILROAD RIGHT-OF-WAY LINE	----
RIVER OR CREEK	----
DRAINAGE DITCH	----
DRAIN TILE	----
CULVERT	----
DROP INLET	----
GUARD RAIL	----
WOVEN WIRE FENCE	----
CHAIN LINK FENCE	----
REMOVE FENCE	----
REMOVE RETAINING WALL	----
RAILROAD CROSSING SIGN	----
RAILROAD CROSSING BELL	----
ELECTRIC WARNING SIGN	----
CROSSING GATE	----
MEANDER CORNER	----
MAIL BOX CORNER	----
FIRE HYDRANT	----
BUILDING (One Story Frame)	----
F-FRAME	----
S-STONE	----
B-BRICK	----
IRON PIPE OR ROD	----
MONUMENT (STONE, CONCRETE, OR METAL)	----
WOODEN HUB	----

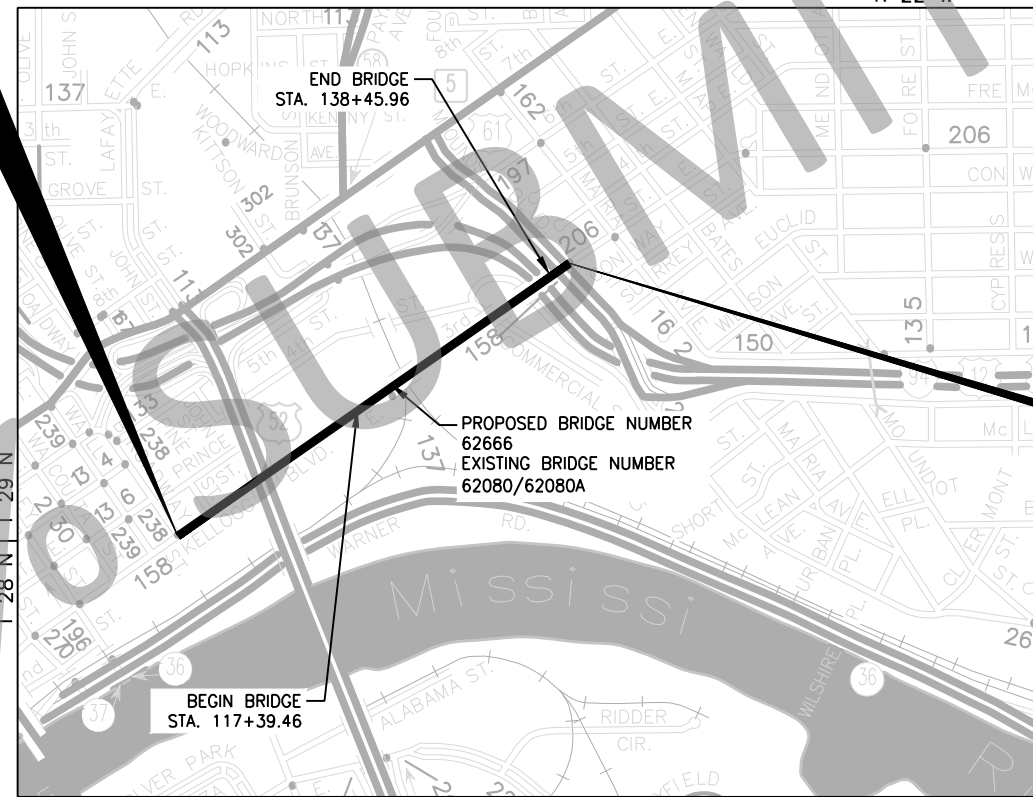
UTILITY SYMBOLS

POWER POLE LINE	----
TELEPHONE POLE LINE	----
JOINT TELEPHONE AND POWER ON POWER POLES	----
JOINT TELEPHONE AND POWER ON TELEPHONE POLES	----
ANCHOR	----
EXISTING STORM STRUCTURES	----
EXISTING WATERMAIN STRUCTURES	----
EXISTING STREET LIGHT (PEDESTAL)	----
EXISTING STREET LIGHT (COBRA)	----
PEDESTAL (TELEPHONE CABLE TERMINAL)	----
GAS MAIN	----
WATER MAIN	----
CONDUIT	----
TELEPHONE MANHOLE	----
ELECTRIC MANHOLE	----
BURIED TELEPHONE CABLE	----
BURIED ELECTRIC CABLE	----
AERIAL TELEPHONE CABLE	----
SEWER, (SANITARY)	----
SEWER, (STORM)	----
REMOVE STORM SEWER	----
SANITARY MANHOLE	----
HANDHOLE	----

BEGIN CONSTRUCTION
S.P. 164-158-028
C.P. B-1202
KELLOGG-EB
STA. 103+03.06

AGREEMENT NO.
CITY OF SAINT PAUL
SP 6283-261 (TH94=108)

METRO DISTRICT



END CONSTRUCTION
S.P. 164-158-028
C.P. B-1202
KELLOGG-EB
STA. 140+23.95

DESIGN DESIGNATION FOR: KELLOGG

R-VALUE	30
ESALS (20)	1667000
ADT CURRENT YEAR (2021)	9900
ADT FUTURE YEAR (2041)	10500
PAVEMENT DESIGN	10 TON
FUNCTIONAL CLASSIFICATION	MAJOR COLLECTOR
NO. OF TRAFFIC LANES	4
NO. OF PARKING LANES	1
Design Speed	30 MPH
Height of eye / Height of Object	3.5' / 2.0'
Design Speed not achieved at:	N/A

SCALES

INDEX MAP	750'
GENERAL LAYOUT	150'
PLAN	25'
PROFILE	2.5' HORIZ. / 2.5' VERT.
X-SECTION	5' HORIZ. / 5' VERT.

PROJECT LOCATION

COUNTY	RAMSEY
DISTRICT	METRO
SECTION	32

PLAN REVISIONS

DATE	SHEET NO.	APPROVED BY

MINN. PROJ. NO.

GOVERNING SPECIFICATIONS
THE 2020 EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR CONSTRUCTION", SHALL GOVERN.

ALL TRAFFIC CONTROL DEVICES SHALL CONFORM AND BE PLACED IN ACCORDANCE WITH THE MOST RECENT EDITION OF "MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MN MUTCD) AND PART VI, "FIELD MANUAL FOR TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS".

CITY OF ST. PAUL 2021 STANDARD SPECIFICATION FOR UTILITY AND STREET CONSTRUCTION.

CITY ENGINEERS ASSOCIATION (CEAM) STANDARD SPECIFICATIONS, 2018 EDITION

INDEX

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221-235	SIGNAL PLANS
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WR26-WR31	RETAINING WALL A
WR32-WR38	RETAINING WALL B
WR39-WR41	RETAINING WALL C
WR42-WR44	RETAINING WALL D
WR45-WR48	WEST APPROACH BARRIER & WALK DETAILS
XS1-XS3	CROSS SECTIONS MATCHLINE LAYOUT
XS4-XS33	CROSS SECTIONS
B1-B207	BRIDGE # 62666

THIS PLAN CONTAINS 524 SHEETS



I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

SIGNATURE _____
DATE _____ LIC. NO. 45427 PRINT NAME GEORGINA STANLEY-WOJCYLA

APPROVED _____ CITY ENGINEER, CITY OF SAINT PAUL

APPROVED _____ DISTRICT TRANSPORTATION ENGINEER

DISTRICT STATE AID ENGINEER: REVIEWED FOR COMPLIANCE WITH STATE AND FEDERAL AID RULES/POLICY

APPROVED FOR STATE AND FEDERAL AID FUNDING: STATE AID ENGINEER

MRedinger, B. 2022 - 10:33:43AM September 8, 2022 - 10:33:43AM C:\srf-pw\pms31346\11371_TSH01.dwg: TSH-01

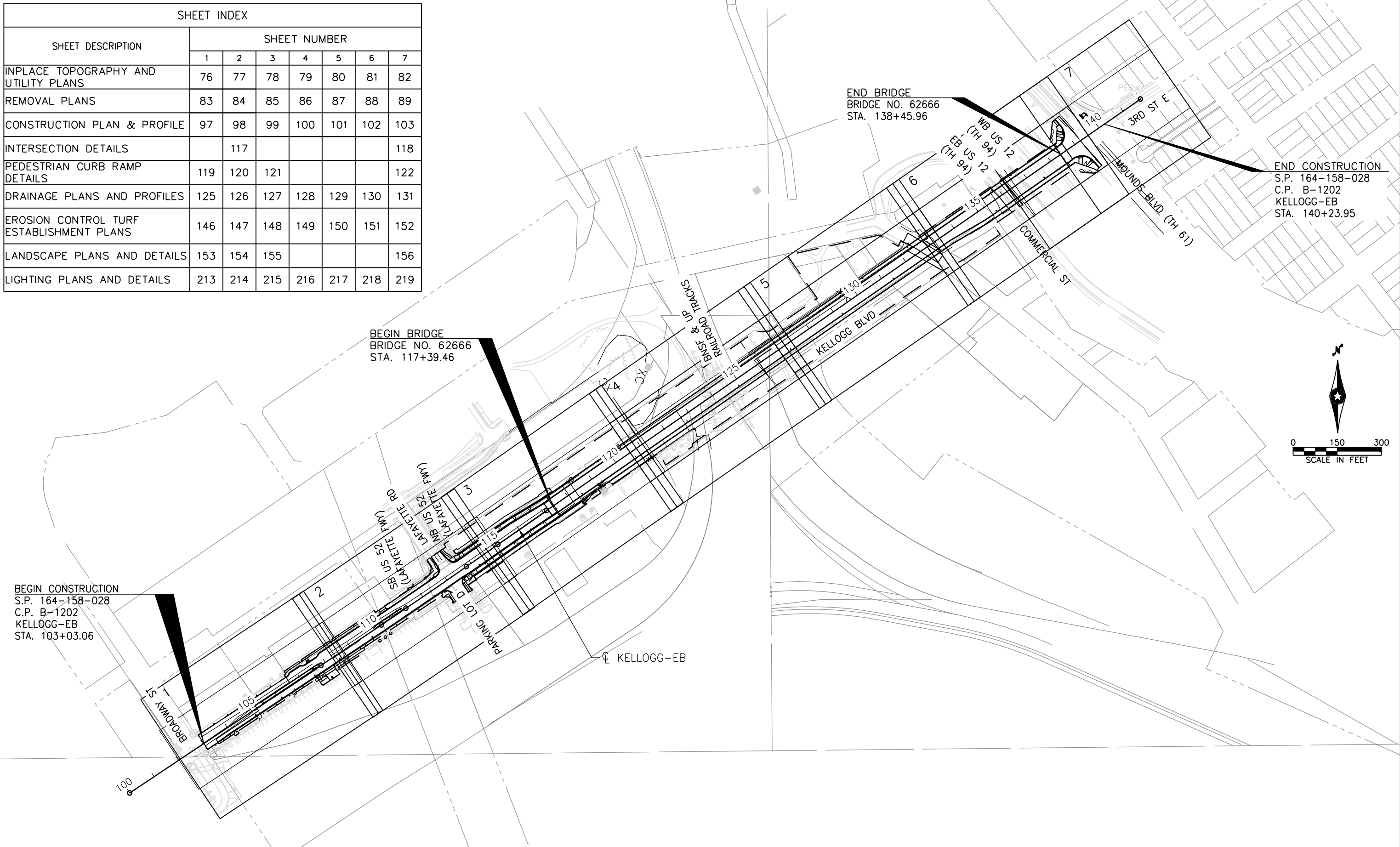
THIS PLAN AND/OR SPECIFICATION WAS PREPARED SPECIFICALLY FOR THIS PROJECT, AND ANY RE-USE OF DETAILS OR SPECIFICATIONS ON OTHER PROJECTS IS NOT INTENDED OR AUTHORIZED BY THE DESIGNER. LIABILITY FOR ANY RE-USE ON OTHER PROJECTS IS THE RESPONSIBILITY OF THE PERSON, AGENCY, OR CORPORATION USING PLAN OR SPECIFICATION DATA FROM THIS PROJECT.

THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO GUIDELINES OF CI/ASCE 38-02. ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA".

S.P. 164-158-028 S.P. 6283-261 (TH 94-108)
C.P. B-1202

SHEET NO. 1 OF 235 SHEETS

SHEET INDEX							
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INPLACE TOPOGRAPHY AND UTILITY PLANS	76	77	78	79	80	81	82
REMOVAL PLANS	83	84	85	86	87	88	89
CONSTRUCTION PLAN & PROFILE	97	98	99	100	101	102	103
INTERSECTION DETAILS		117					118
PEDESTRIAN CURB RAMP DETAILS	119	120	121				122
DRAINAGE PLANS AND PROFILES	125	126	127	128	129	130	131
EROSION CONTROL TURF ESTABLISHMENT PLANS	146	147	148	149	150	151	152
LANDSCAPE PLANS AND DETAILS	153	154	155				156
LIGHTING PLANS AND DETAILS	213	214	215	216	217	218	219



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 2315 (LMS TECH) | C:\Users\bokemper\AppData\Local\Temp\Rooming\Autodesk\C3D_2020\enu\

NO	DATE	BY	CKD	APPR

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.
 Print Name: **MARK REDINGER**
 Date: **--/--** License # **59198**

STATE AID PROJECT NO. 164-158-028
 STATE PROJECT NO. 6283-261 (TH 94)
 COUNTY PROJECT NO.
 CITY PROJECT NO. B-1202

DRAWN BY BDB
 DESIGNED BY ADG
 CHECKED BY SAS
 COMM. NO. 11371



SAINT PAUL, MINNESOTA
GENERAL LAYOUT
 KELLOGG/3RD STREET BRIDGE REPLACEMENT

SHEET 2 OF 235

GENERAL NOTES:

ALL CROSS SLOPES ARE IN FT/FT

ROADWAY DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED

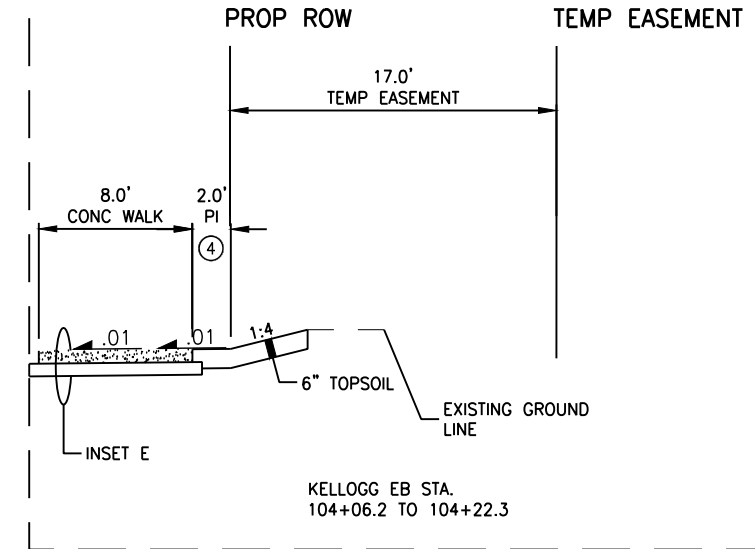
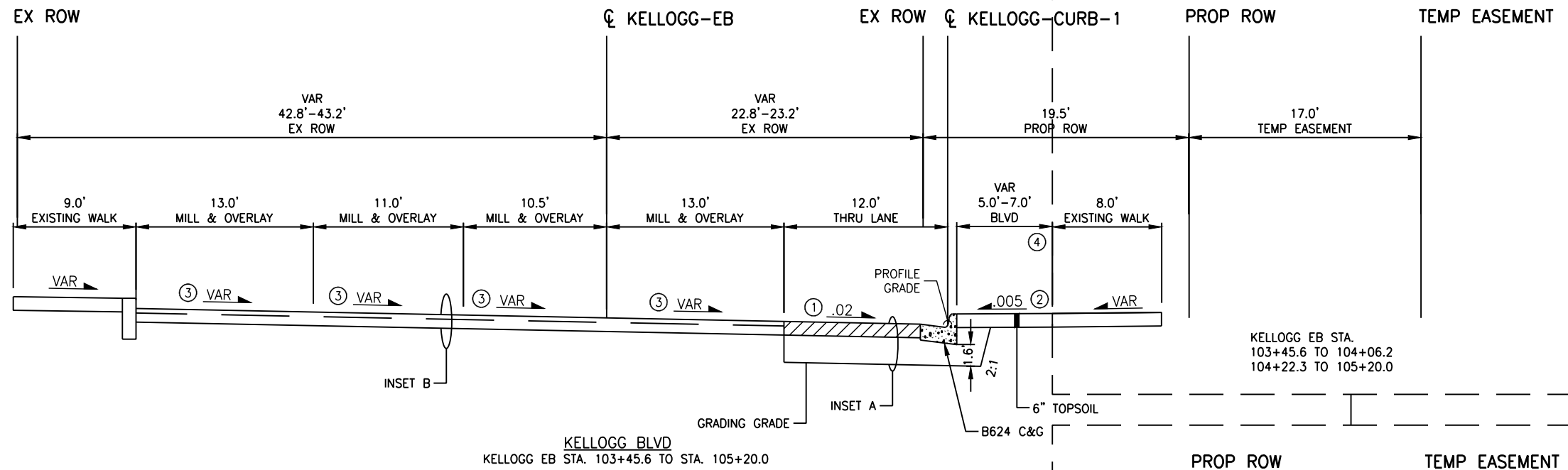
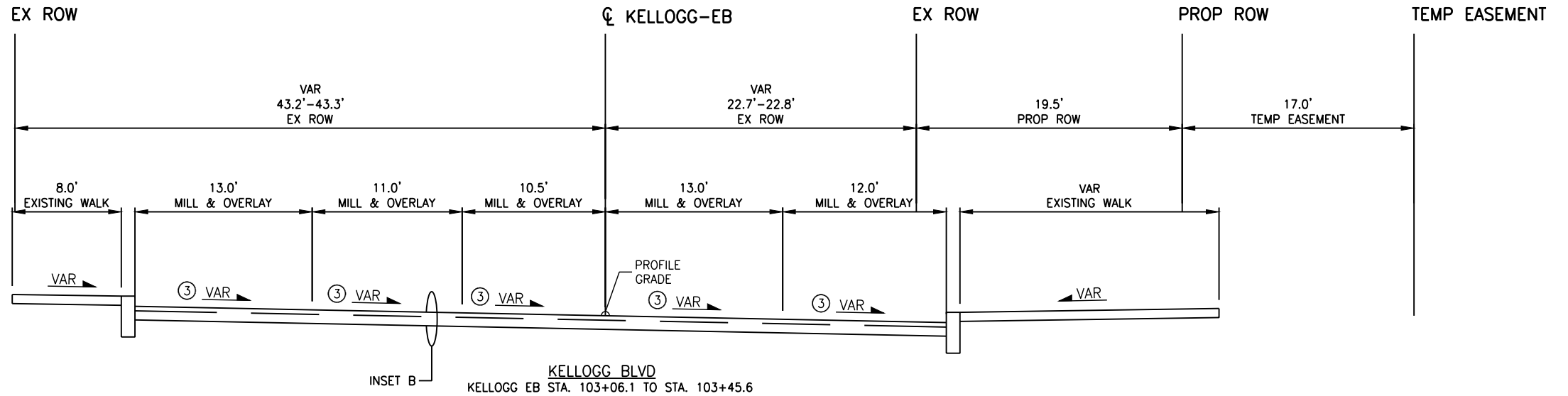
BOULEVARD DIMENSIONS START FROM BACK OF CURB UNLESS OTHERWISE NOTED.

SEE RETAINING WALLS PLAN SHEETS WR27 - WR43 FOR RETAINING WALL INFORMATION

SEE SIGNING AND STRIPING PLAN SHEETS 192 - 211 FOR LANE CONFIGURATIONS AND MARKINGS.

NOTES:

- ① SEE SUPERELEVATION PLANS SHEETS 113 - 116 FOR ROAD CROSS SLOPE
- ② VARIABLE BOULEVARD GRADE 0.5% TO 4.0% KELLOGG EB STA. 104+10.0 TO 104+25.0
- ③ MATCH EXISTING ROAD CROSS SLOPE
- ④ 2.0' OBSTACLE CLEAR ZONE FROM EDGE OF WALK



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SAINT PAUL, MINNESOTA
 TYPICAL SECTIONS
 KELLOGG/3RD STREET BRIDGE REPLACEMENT

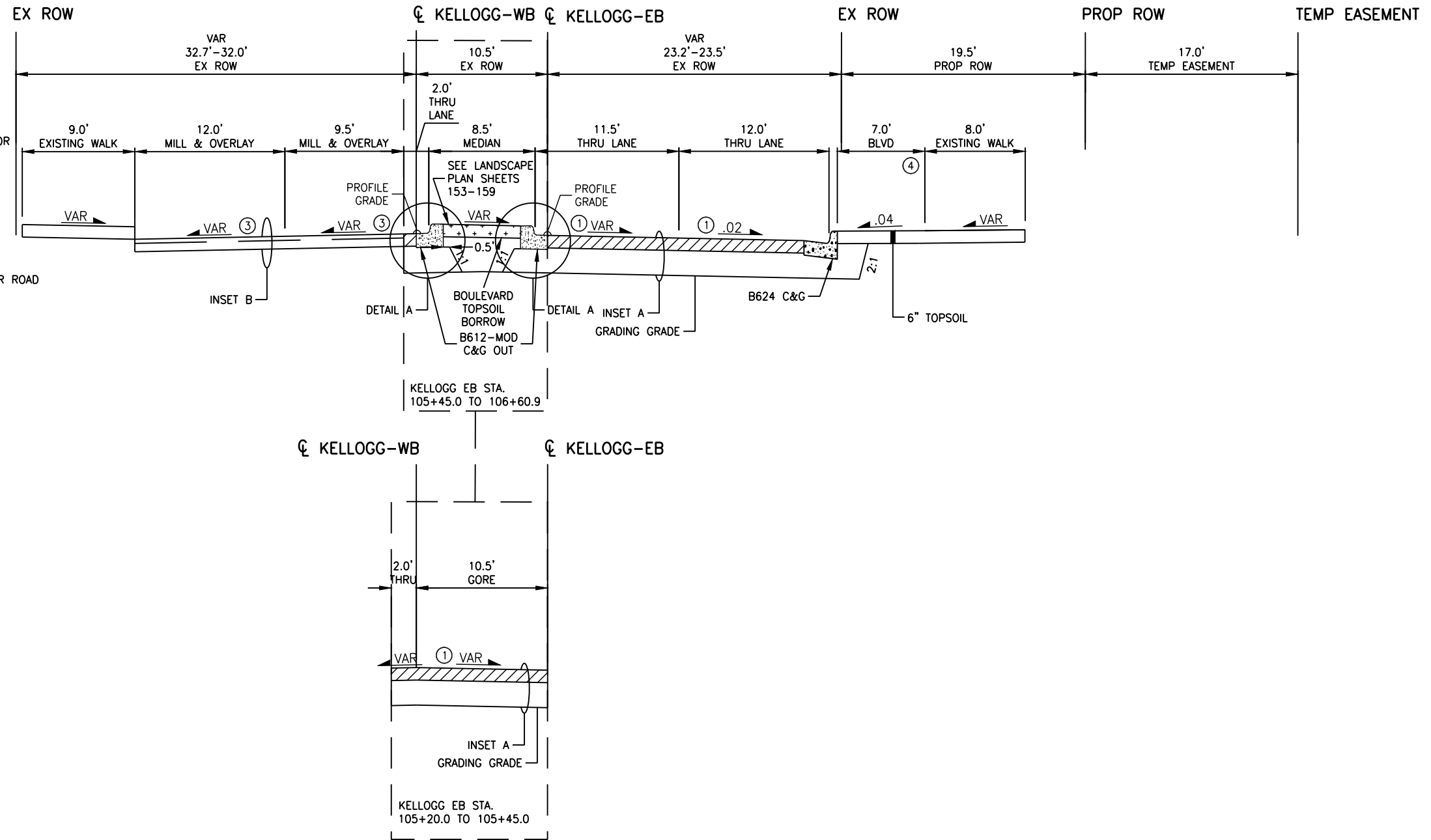
SHEET 48 OF 235

GENERAL NOTES:

ALL CROSS SLOPES ARE IN FT/FT
 ROADWAY DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED
 BOULEVARD DIMENSIONS START FROM BACK OF CURB UNLESS OTHERWISE NOTED.
 SEE RETAINING WALLS PLAN SHEETS WR27 - WR43 FOR RETAINING WALL INFORMATION
 SEE SIGNING AND STRIPING PLAN SHEETS 192 - 211 FOR LANE CONFIGURATIONS AND MARKINGS.

NOTES:

- ① SEE SUPERELEVATION PLANS SHEETS 113 - 116 FOR ROAD CROSS SLOPE
- ③ MATCH EXISTING ROAD CROSS SLOPE
- ④ 2.0' OBSTACLE CLEAR ZONE FROM EDGE OF WALK



KELLOGG BLVD
 KELLOGG EB STA. 105+20.0 TO STA. 106+60.9

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 COUNTY PROJECT NO.
 CITY PROJECT NO. **B-1202**

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 DESIGNED BY **ADG**
 CHECKED BY **SAS**
 COMM. NO. **11371**



SAINT PAUL, MINNESOTA
TYPICAL SECTIONS
 KELLOGG/3RD STREET BRIDGE REPLACEMENT

SHEET **49**
 OF **235**

GENERAL NOTES:

ALL CROSS SLOPES ARE IN FT/FT

ROADWAY DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED

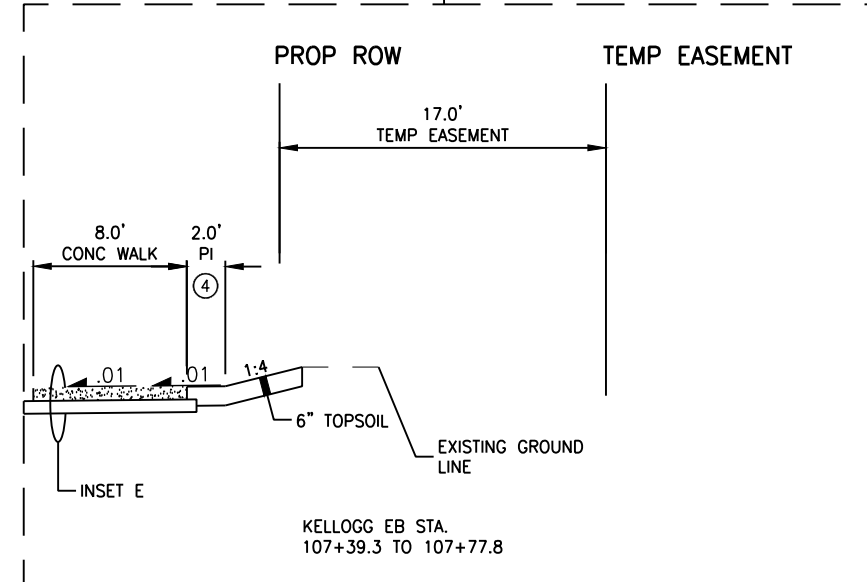
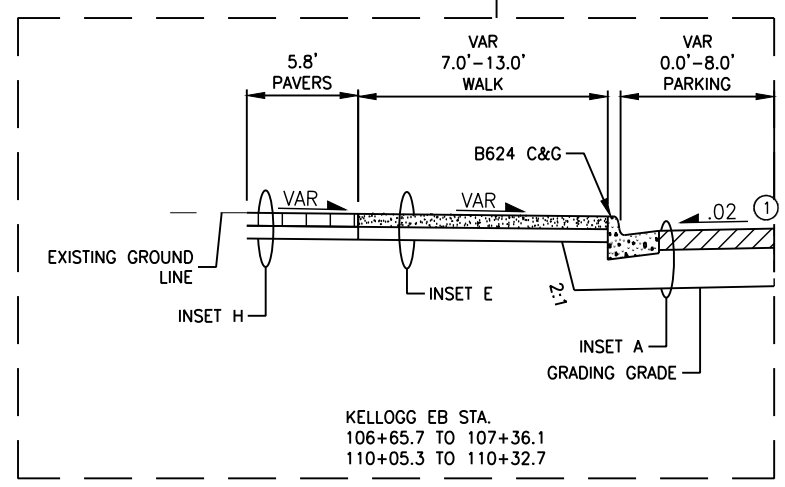
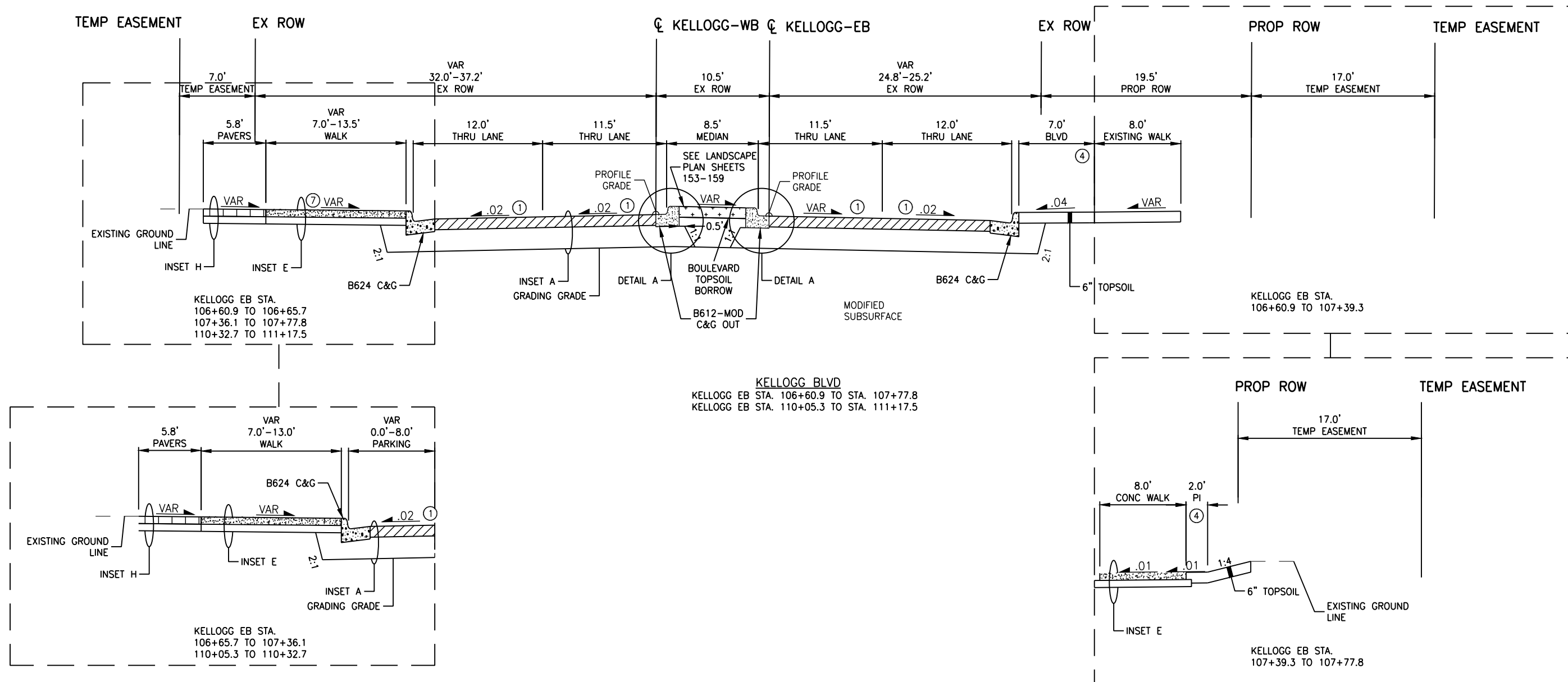
BOULEVARD DIMENSIONS START FROM BACK OF CURB UNLESS OTHERWISE NOTED.

SEE RETAINING WALLS PLAN SHEETS WR27 - WR43 FOR RETAINING WALL INFORMATION

SEE SIGNING AND STRIPING PLAN SHEETS 192 - 211 FOR LANE CONFIGURATIONS AND MARKINGS.

NOTES:

- ① SEE SUPERELEVATION PLANS SHEETS 113 - 116 FOR ROAD CROSS SLOPE
- ④ 2.0' OBSTACLE CLEAR ZONE FROM EDGE OF WALK
- ⑦ VARIABLE CROSS SLOPE MAXIMUM SLOPE MUST NOT EXCEED 0.02 FT/FT IN ANY DIRECTION.



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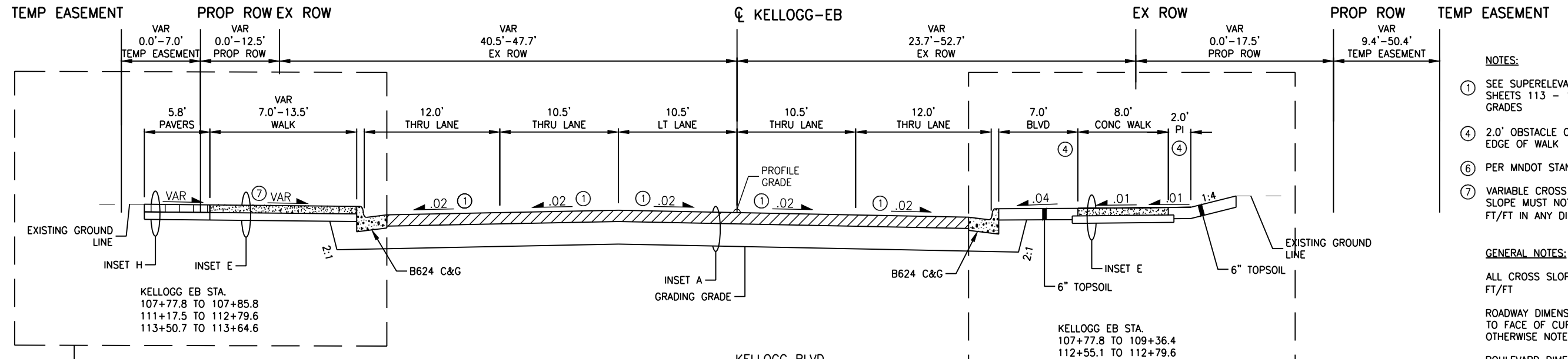
STATE AID PROJECT NO. 164-158-028
 STATE PROJECT NO. 6283-261 (TH 94)
 COUNTY PROJECT NO.
 CITY PROJECT NO. B-1202

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 CHECKED BY SAS
 COMM. NO. 11371



SAINT PAUL, MINNESOTA
TYPICAL SECTIONS
 KELLOGG/3RD STREET BRIDGE REPLACEMENT

SHEET 50 OF 235



- NOTES:**
- ① SEE SUPERELEVATION PLANS SHEETS 113 - 116 FOR ROAD GRADES
 - ④ 2.0' OBSTACLE CLEAR ZONE FROM EDGE OF WALK
 - ⑥ PER MNDOT STANDARD PLATE 7100
 - ⑦ VARIABLE CROSS SLOPE MAXIMUM SLOPE MUST NOT EXCEED 0.02 FT/FT IN ANY DIRECTION.

GENERAL NOTES:

ALL CROSS SLOPES ARE IN FT/FT

ROADWAY DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED

BOULEVARD DIMENSIONS START FROM BACK OF CURB UNLESS OTHERWISE NOTED.

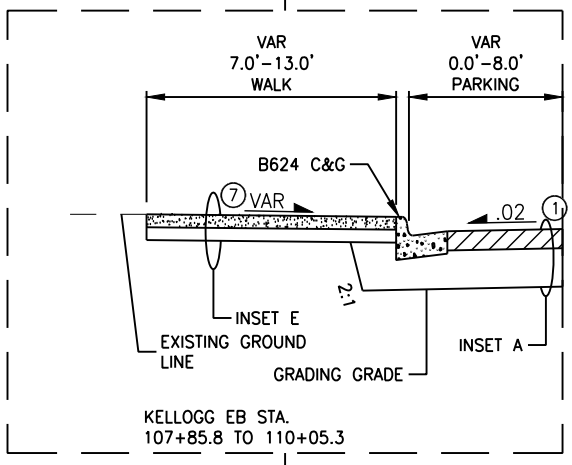
SEE RETAINING WALLS PLAN SHEETS WR27 - WR43 FOR RETAINING WALL INFORMATION

SEE SIGNING AND STRIPING PLAN SHEETS 192 - 211 FOR LANE CONFIGURATIONS AND MARKINGS.

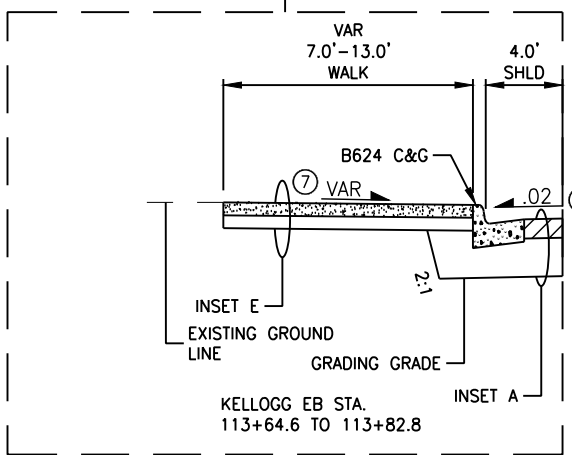
KELLOGG EB STA.
107+77.8 TO 107+85.8
111+17.5 TO 112+79.6
113+50.7 TO 113+64.6

KELLOGG BLVD
KELLOGG EB STA. 107+77.8 TO STA. 110+05.3
KELLOGG EB STA. 111+17.5 TO STA. 112+79.6
KELLOGG EB STA. 113+50.7 TO STA. 113+82.8

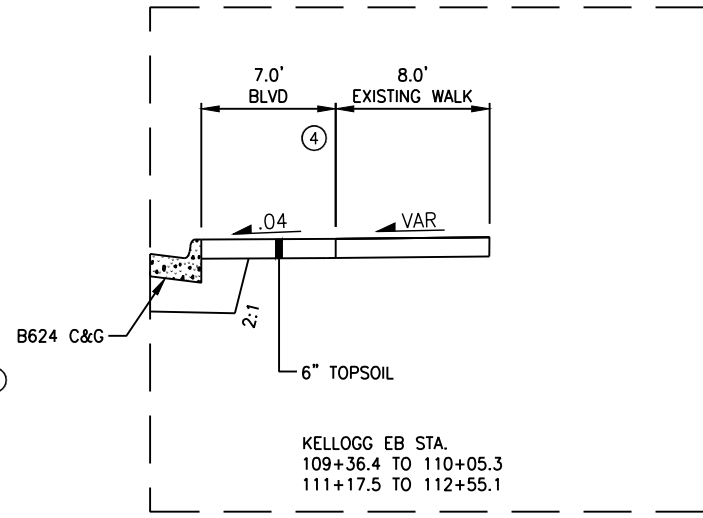
KELLOGG EB STA.
107+77.8 TO 109+36.4
112+55.1 TO 112+79.6



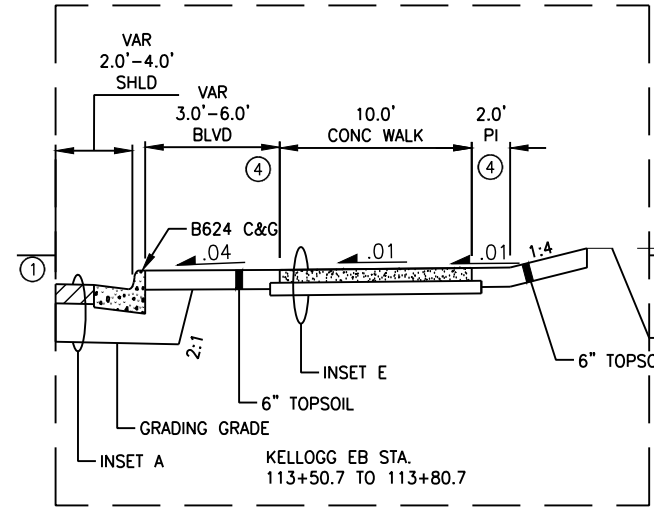
KELLOGG EB STA.
107+85.8 TO 110+05.3



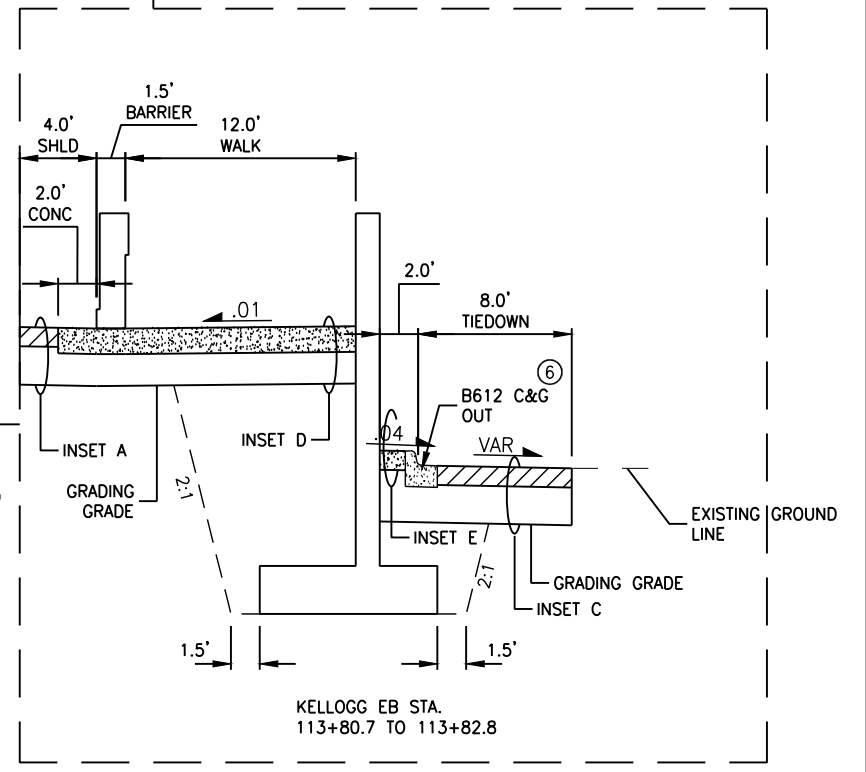
KELLOGG EB STA.
113+64.6 TO 113+82.8



KELLOGG EB STA.
109+36.4 TO 110+05.3
111+17.5 TO 112+55.1



KELLOGG EB STA.
113+50.7 TO 113+80.7



KELLOGG EB STA.
113+80.7 TO 113+82.8

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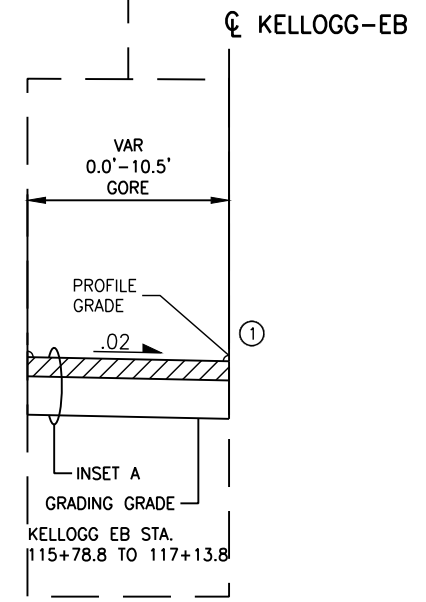
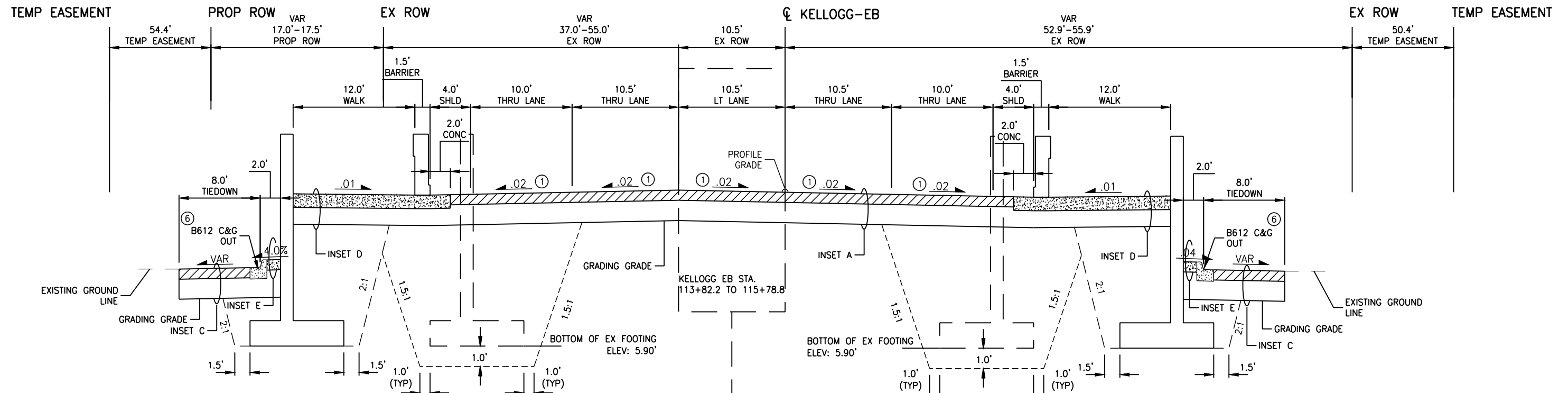
STATE AID PROJECT NO. 164-158-028
 STATE PROJECT NO. 6283-261 (TH 94)
 COUNTY PROJECT NO.
 CITY PROJECT NO. B-1202

DRAWN BY BDB
 DESIGNED BY ADG
 CHECKED BY SAS
 COMM. NO. 11371



SAINT PAUL, MINNESOTA
 TYPICAL SECTIONS
 KELLOGG/3RD STREET BRIDGE REPLACEMENT

SHEET 51 OF 235



GENERAL NOTES:

- ALL CROSS SLOPES ARE IN FT/FT
- ROADWAY DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED
- BOULEVARD DIMENSIONS START FROM BACK OF CURB UNLESS OTHERWISE NOTED.
- SEE RETAINING WALLS PLAN SHEETS WR27 - WR43 FOR RETAINING WALL INFORMATION
- SEE SIGNING AND STRIPING PLAN SHEETS 192 - 211 FOR LANE CONFIGURATIONS AND MARKINGS.

NOTES:

- ① SEE SUPERELEVATION PLANS SHEETS 113 - 116 FOR ROAD GRADES
- ⑥ PER MNDOT STANDARD PLATE 7100

KELLOGG BLVD
KELLOGG EB STA. 113+82.8 TO STA. 117+13.8

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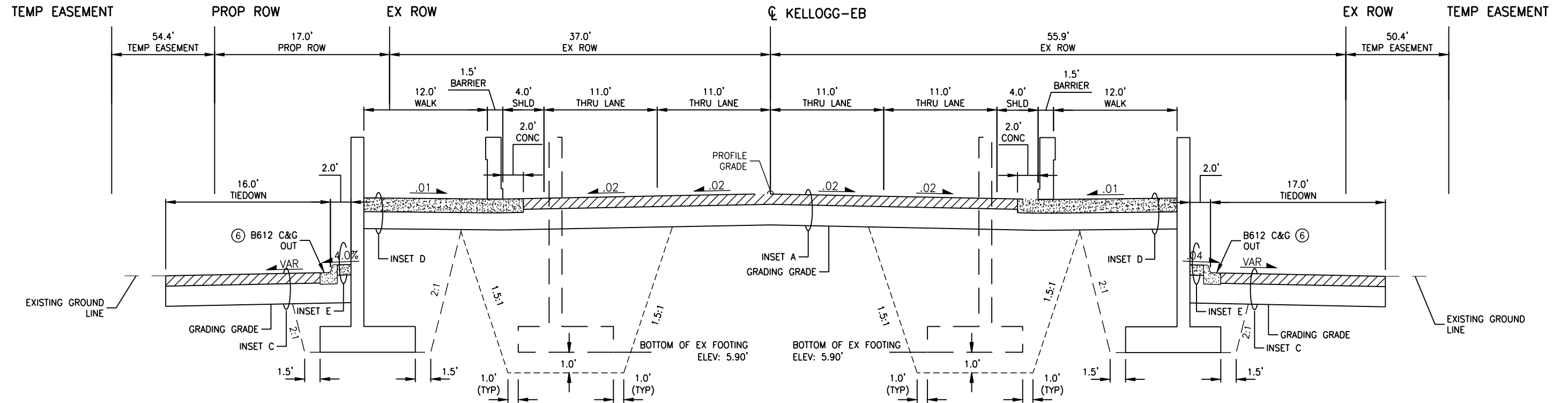
STATE AID PROJECT NO. 164-158-028
 STATE PROJECT NO. 6283-261 (TH 94)
 COUNTY PROJECT NO.
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SAINT PAUL, MINNESOTA
 TYPICAL SECTIONS
 KELLOGG/3RD STREET BRIDGE REPLACEMENT

SHEET 52 OF 235



KELLOGG BLVD
 KELLOGG EB STA. 117+13.8 TO STA. 117+19.5

GENERAL NOTES:

- ALL CROSS SLOPES ARE IN FT/FT
- ROADWAY DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED
- BOULEVARD DIMENSIONS START FROM BACK OF CURB UNLESS OTHERWISE NOTED.
- SEE RETAINING WALLS PLAN SHEETS WR27 - WR43 FOR RETAINING WALL INFORMATION
- SEE SIGNING AND STRIPING PLAN SHEETS 192 - 211 FOR LANE CONFIGURATIONS AND MARKINGS.

NOTES:

- ⑥ PER MNDOT STANDARD PLATE 7100

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 Date: **8/29/2022** License # **59198**

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 STATE PROJECT NO. 6283-261 (TH 94)
 COUNTY PROJECT NO.
 CITY PROJECT NO. B-1202

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 CHECKED BY SAS
 COMM. NO. 11371



SAINT PAUL, MINNESOTA
 TYPICAL SECTIONS
 KELLOGG/3RD STREET BRIDGE REPLACEMENT

SHEET 53 OF 235

GENERAL NOTES:

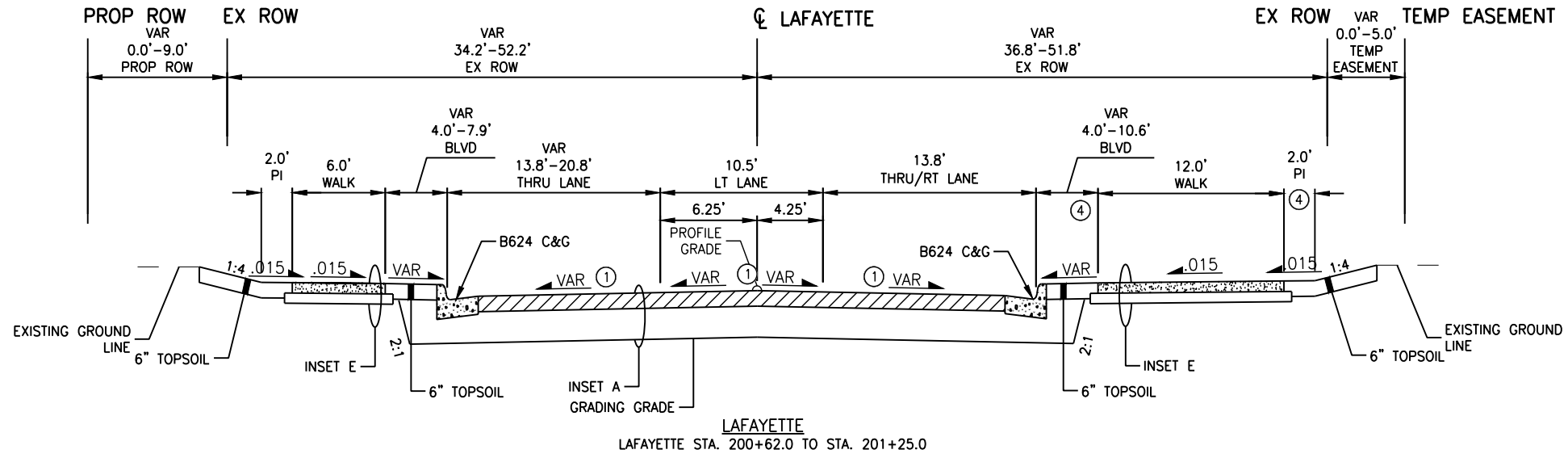
ALL CROSS SLOPES ARE IN FT/FT

ROADWAY DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED

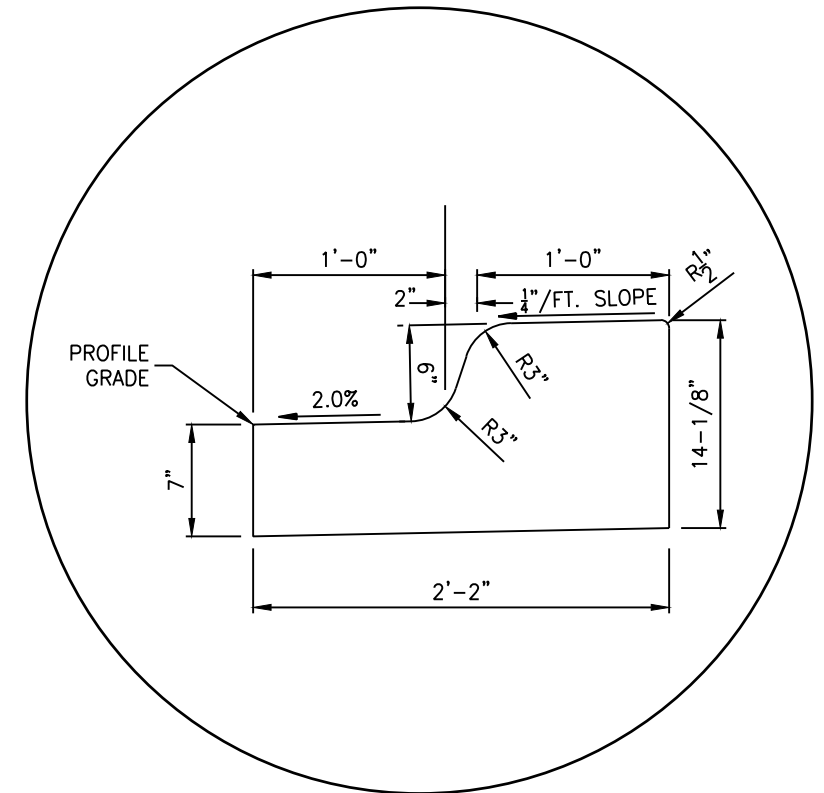
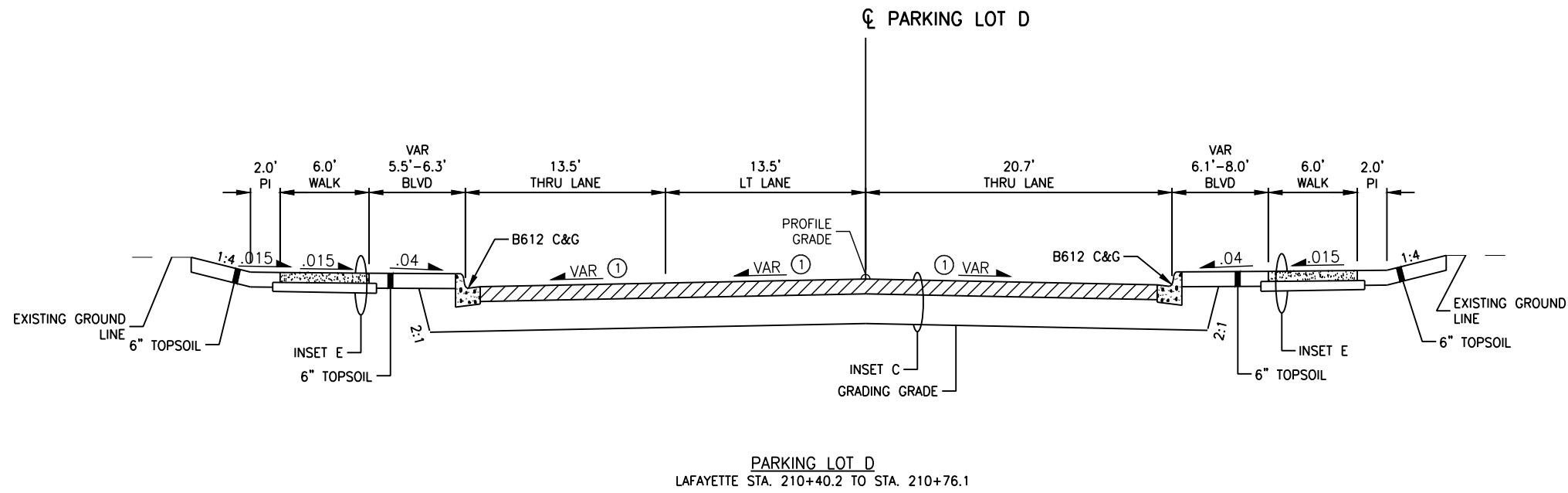
BOULEVARD DIMENSIONS START FROM BACK OF CURB UNLESS OTHERWISE NOTED.

SEE RETAINING WALLS PLAN SHEETS WR27 - WR43 FOR RETAINING WALL INFORMATION

SEE SIGNING AND STRIPING PLAN SHEETS 192 - 211 FOR LANE CONFIGURATIONS AND MARKINGS.



- NOTES:**
- ① SEE SUPERELEVATION PLANS SHEETS 113 - 116 FOR ROAD GRADES
 - ④ 2.0' OBSTACLE CLEAR ZONE FROM EDGE OF WALK



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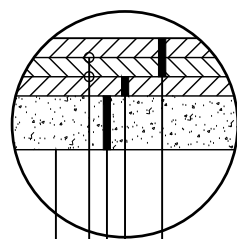
STATE AID PROJECT NO.
164-158-028
 STATE PROJECT NO.
6283-261 (TH 94)
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BDB
 DESIGNED BY
ADG
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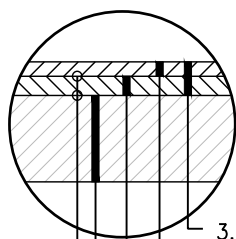
SAINT PAUL, MINNESOTA
 TYPICAL SECTIONS
 KELLOGG/3RD STREET BRIDGE REPLACEMENT

SHEET
54
 OF
235



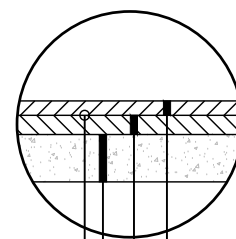
2 - 1.5" TYPE SP 9.5 WEARING COURSE MIXTURE
MnDOT SPEC. 2360, SPWEA340C
2.5" TYPE SP 12.5 NON-WEARING COURSE MIXTURE
MnDOT SPEC. 2360, SPNWB330C
12" AGGREGATE BASE CLASS 5 (CV)
MnDOT SPEC. 2111
TACK COAT MnDOT SPEC. 2357 (INCIDENTAL)
GRADING GRADE

INSET A
BITUMINOUS PAVEMENT



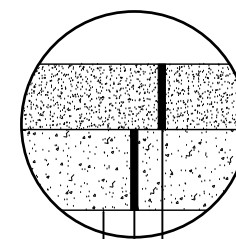
3.5" MILL BITUMINOUS SURFACE
1.5" TYPE SP 9.5 WEARING COURSE MIXTURE
MnDOT SPEC. 2360, SPWEA340C
2" TYPE SP 12.5 WEARING COURSE MIXTURE
MnDOT SPEC. 2360, SPWEA340C
EXISTING CONCRETE PAVEMENT (BELOW MILLING DEPTH)
TACK COAT MnDOT SPEC. 2357 (INCIDENTAL)

INSET B
BITUMINOUS PAVEMENT IN MILLED AREA



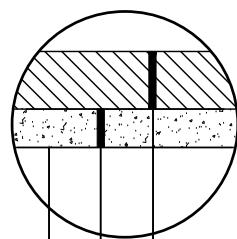
2.0" TYPE SP 12.5 WEARING COURSE MIXTURE
MnDOT SPEC. 2360, SPWEA340C
2" TYPE SP 12.5 WEARING COURSE MIXTURE
MnDOT SPEC. 2360, SPWEA340C
6" AGGREGATE BASE CLASS 5 (CV)
MNDOT SPEC. 2211
TACK COAT MnDOT SPEC. 2357 (INCIDENTAL)

INSET C
BITUMINOUS PAVEMENT (PARKING LOTS)



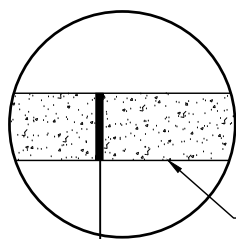
8" SIDEWALK CONCRETE (3S52)
SEE RETAINING WALL PLANS
MnDOT SPEC. 2301
10" AGGREGATE BASE CLASS 5 (CV)
MnDOT SPEC. 2211 (INCIDENTAL)
GRADING GRADE

INSET D
REINFORCED CONCRETE WALK



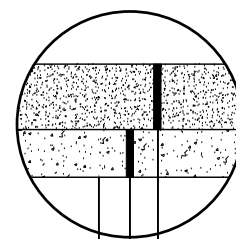
6" CONCRETE WALK
MNDOT SPEC. 2521
4" AGGREGATE BASE CLASS 5 (CV)
MnDOT SPEC. 2211 (INCIDENTAL)
GRADING GRADE

INSET E
6" CONCRETE WALK



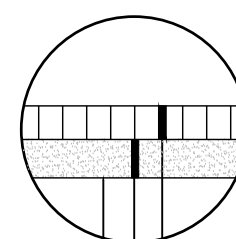
GRADING GRADE
8" AGGREGATE SURFACING

INSET F
GRAVEL DRIVEWAY



8" CONCRETE PAVEMENT
MnDOT SPEC. 2301
6" AGGREGATE BASE CLASS 5 (CV)
MnDOT SPEC. 2211 (INCIDENTAL)
GRADING GRADE

INSET G
CONCRETE PAVEMENT



INSTALL BRICK PAVERS
VAR SELECT GRANULAR BORROW (CV)
GRADING GRADE

INSET H
INSTALL BRICK PAVERS

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NO	DATE	BY	CKD	APPR

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DRAWN BY
BDB
 DESIGNED BY
ADG
 CHECKED BY
SAS
 COMM. NO. 11371



SAINT PAUL, MINNESOTA
 TYPICAL SECTIONS
 KELLOGG/3RD STREET BRIDGE REPLACEMENT

SHEET
55
OF
235

PARCEL NO. 1
 FEE OWNER SAINT PAUL PW SEWER LIFT STATION &
 VENT PIPES KELLOGG AT BROADWAY
 PID NO. 322922330038
 TEMPORARY ESMT = 397.75 SQ FT
 PERMANENT ESMT = 277.50 SQ FT
 DATE ACQUIRED

PARCEL NO. 9
 FEE OWNER CITY OF ST PAUL
 PID NO. 322922420042
 TEMPORARY ESMT = 5920.62 SQ FT
 DATE ACQUIRED

PARCEL NO. 18
 FEE OWNER CITY OF ST PAUL REAL ESTATE
 PID NO. 322922420019
 TEMPORARY ESMT = 27716.92 SQ FT
 DATE ACQUIRED

PARCEL NO. 2A
 FEE OWNER RAMSEY COUNTY REG RR AUTH
 PID NO. 322922330360
 TEMPORARY ESMT = 12563.41 SQ FT
 PERMANENT ESMT = 12332.63 SQ FT
 DATE ACQUIRED

PARCEL NO. 10
 FEE OWNER CHARLES CROTTY AND SONS INC
 PID NO. 322922420055
 TEMPORARY ESMT = 6747.42 SQ FT
 DATE ACQUIRED

PARCEL NO. 19
 FEE OWNER CITY OF ST PAUL
 PID NO. 322922420020
 TEMPORARY ESMT = 9999.08 SQ FT
 DATE ACQUIRED

PARCEL NO. 2B
 FEE OWNER RAMSEY COUNTY REG RR AUTH
 PID NO. 322922330360
 TEMPORARY ESMT = 19802.01 SQ FT
 PERMANENT ESMT = 17326.99 SQ FT
 DATE ACQUIRED

PARCEL NO. 11
 FEE OWNER DONERLY INC
 PID NO. 322922340022
 TEMPORARY ESMT = 315.35 SQ FT
 PERM ESMT = 14606.81 SQ FT
 DATE ACQUIRED

PARCEL NO. 20
 FEE OWNER CITY OF ST PAUL
 PID NO. 322922420021
 TEMPORARY ESMT = 7801.42 SQ FT
 DATE ACQUIRED

PARCEL NO. 3
 FEE OWNER RAMSEY COUNTY REG RR AUTH
 PID NO. 322922340011
 TEMPORARY ESMT = 83216.57 SQ FT
 DATE ACQUIRED

PARCEL NO. 12
 FEE OWNER D OREN B LLC
 PID NO. 322922340019
 TEMPORARY ESMT = 46702.64 SQ FT
 PERM ESMT = 10793.11 SQ FT
 DATE ACQUIRED

PARCEL NO. 21
 FEE OWNER CITY OF ST PAUL
 PID NO. 322922420023
 TEMPORARY ESMT = 26502.63 SQ FT
 DATE ACQUIRED

PARCEL NO. 4
 FEE OWNER RAMSEY COUNTY RR AUTH
 PID NO. 322922340018
 TEMPORARY ESMT = 2007.08 SQ FT
 DATE ACQUIRED

PARCEL NO. 13
 FEE OWNER BNSF RAILWAY CO & PROPERTY
 PID NO. 322922340007
 TEMPORARY ESMT = 9558.51 SQ FT
 DATE ACQUIRED

PARCEL NO. 22
 FEE OWNER MINDFRAME INC
 PID NO. 322922330012
 TEMPORARY ESMT = 2587.69 SQ FT
 DATE ACQUIRED

PARCEL NO. 5
 FEE OWNER UNION PACIFIC RAILROAD COMPANY & ATTN
 PROPERTY TAX
 PID NO. 322922340017
 TEMPORARY ESMT = 5401.08 SQ FT
 DATE ACQUIRED

PARCEL NO. 14
 FEE OWNER BNSF RAILWAY CO & PROPERTY
 PID NO. 322922310027
 TEMPORARY ESMT = 326.32 SQ FT
 DATE ACQUIRED

PARCEL NO. 6
 FEE OWNER BNSF RAILWAY CO & PROPERTY
 PID NO. 322922341106
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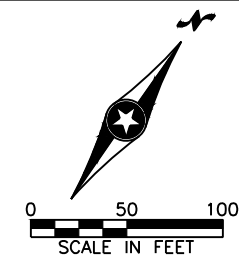
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 PID NO. 322922310051
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 FEE OWNER BN LEASING CORP
 PID NO. 322922430012
 TEMPORARY ESMT = 645.47 SQ FT
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PARCEL NO. 16
 FEE OWNER RAMSEY COUNTY REG RR AUTH
 PID NO. 322922310050
 TEMPORARY ESMT = 7767.92 SQ FT
 DATE ACQUIRED

PARCEL NO. 8
 FEE OWNER CITY OF ST PAUL
 PID NO. 322922430013
 TEMPORARY ESMT = 21656.16 SQ FT
 DATE ACQUIRED

PARCEL NO. 17
 FEE OWNER RAMSEY COUNTY REG RR AUTH
 PID NO. 322922420057
 TEMPORARY ESMT = 4943.82
 DATE ACQUIRED



LEGEND

TEMPORARY EASEMENT

PERMANENT EASEMENT

XXXX PARCEL NUMBER

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NO	DATE	BY	CKD	APPR

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.
 Print Name: MARK REDINGER
 Date --/-- License # 59198




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164-158-028
 STATE PROJECT NO.
6283-261 (TH 94)
 COUNTY PROJECT NO.
 CITY PROJECT NO. B-1202

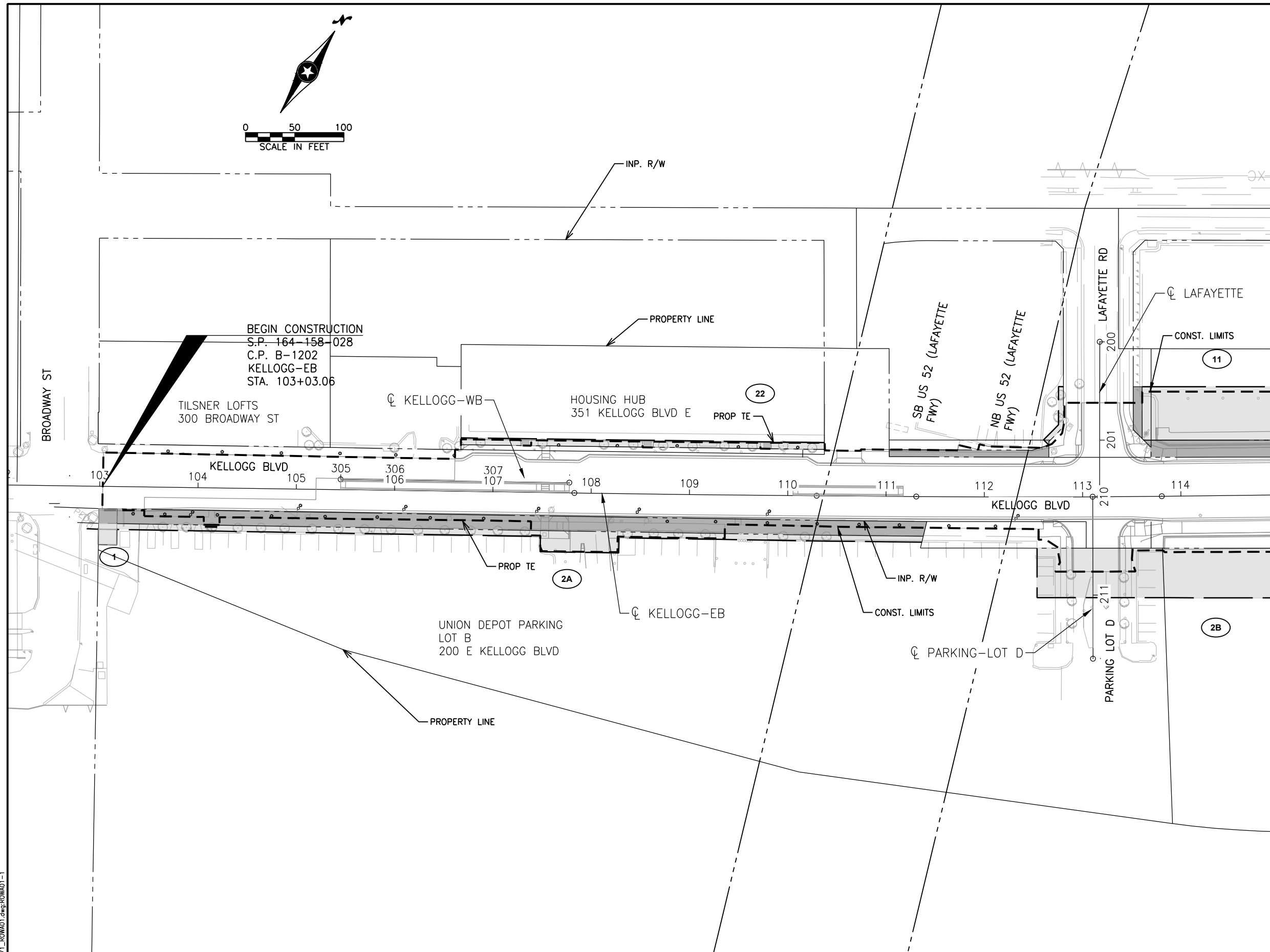
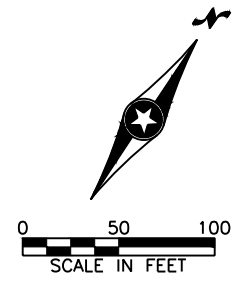
DRAWN BY
BDB
 DESIGNED BY
ADG
 CHECKED BY
SAS
 COMM. NO. 11371



SAINT PAUL, MINNESOTA
 RIGHT-OF-WAY PLANS
 KELLOGG/3RD STREET BRIDGE REPLACEMENT

SHEET
 90
 OF
 235

LEGEND	
	TEMPORARY EASEMENT
	PERMANENT EASEMENT
	PARCEL NUMBER

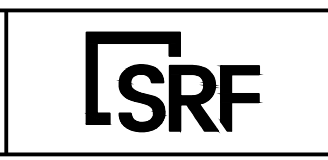


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NO	DATE	BY	CKD	APPR




I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.
 Print Name: MARK REDINGER
 Date: / / License # 59198

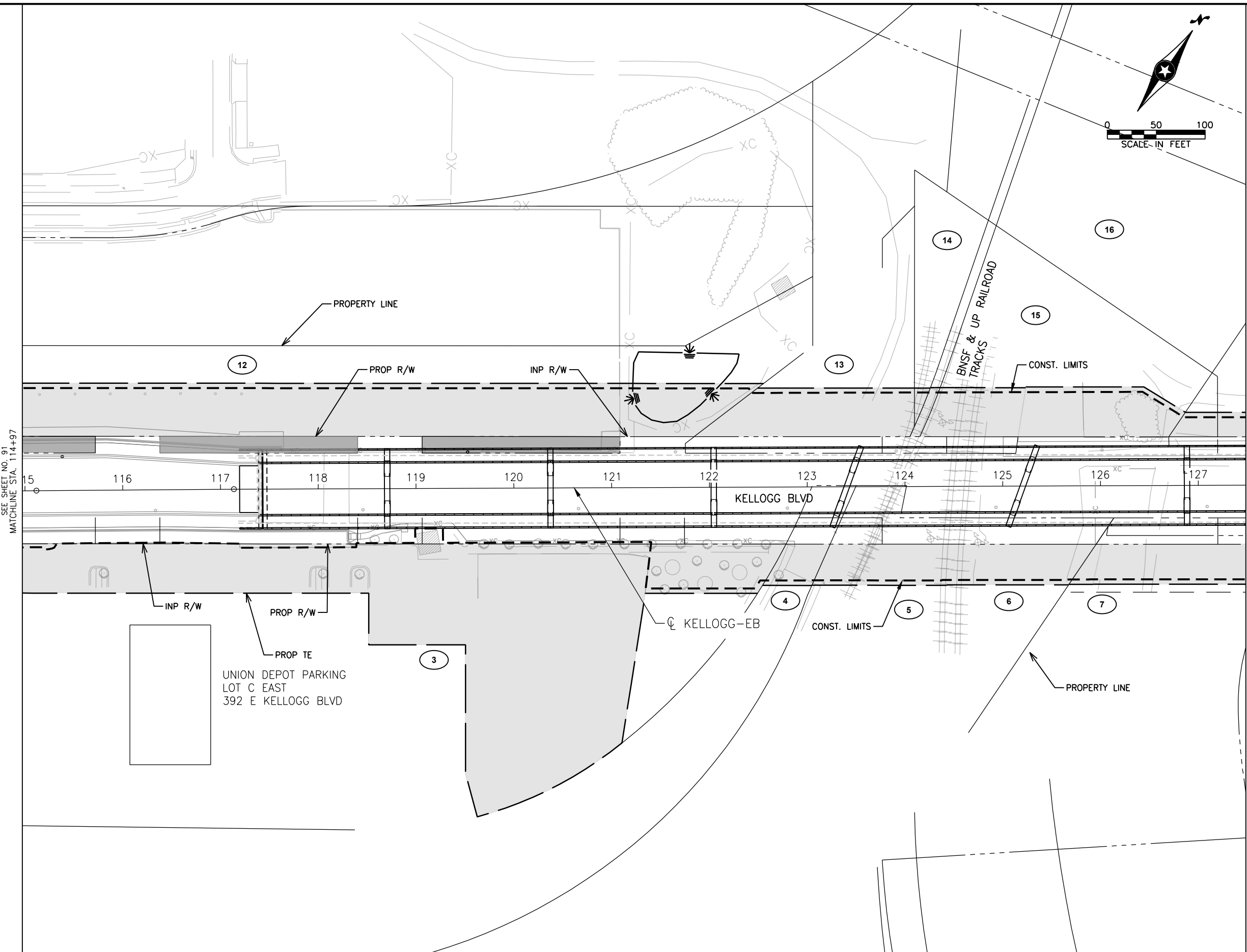
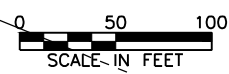
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STATE PROJECT NO. 6283-261 (TH 94)	DESIGNED BY ADG
COUNTY PROJECT NO.	CHECKED BY SAS
CITY PROJECT NO. B-1202	COMM. NO. 11371



SAINT PAUL, MINNESOTA
 RIGHT-OF-WAY PLANS
 KELLOGG/3RD STREET BRIDGE REPLACEMENT

SHEET
 91
 OF
 235

LEGEND	
	TEMPORARY EASEMENT
	PERMANENT EASEMENT
	PARCEL NUMBER



SEE SHEET NO. 91
MATCHLINE STA. 114+97

MATCHLINE STA. 127+49
SEE SHEET NO. 93

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NO	DATE	BY	CKD	APPR

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.
Print Name: MARK REDINGER
Date - - License # 59198




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STATE PROJECT NO. 6283-261 (TH 94)
COUNTY PROJECT NO.
CITY PROJECT NO. B-1202

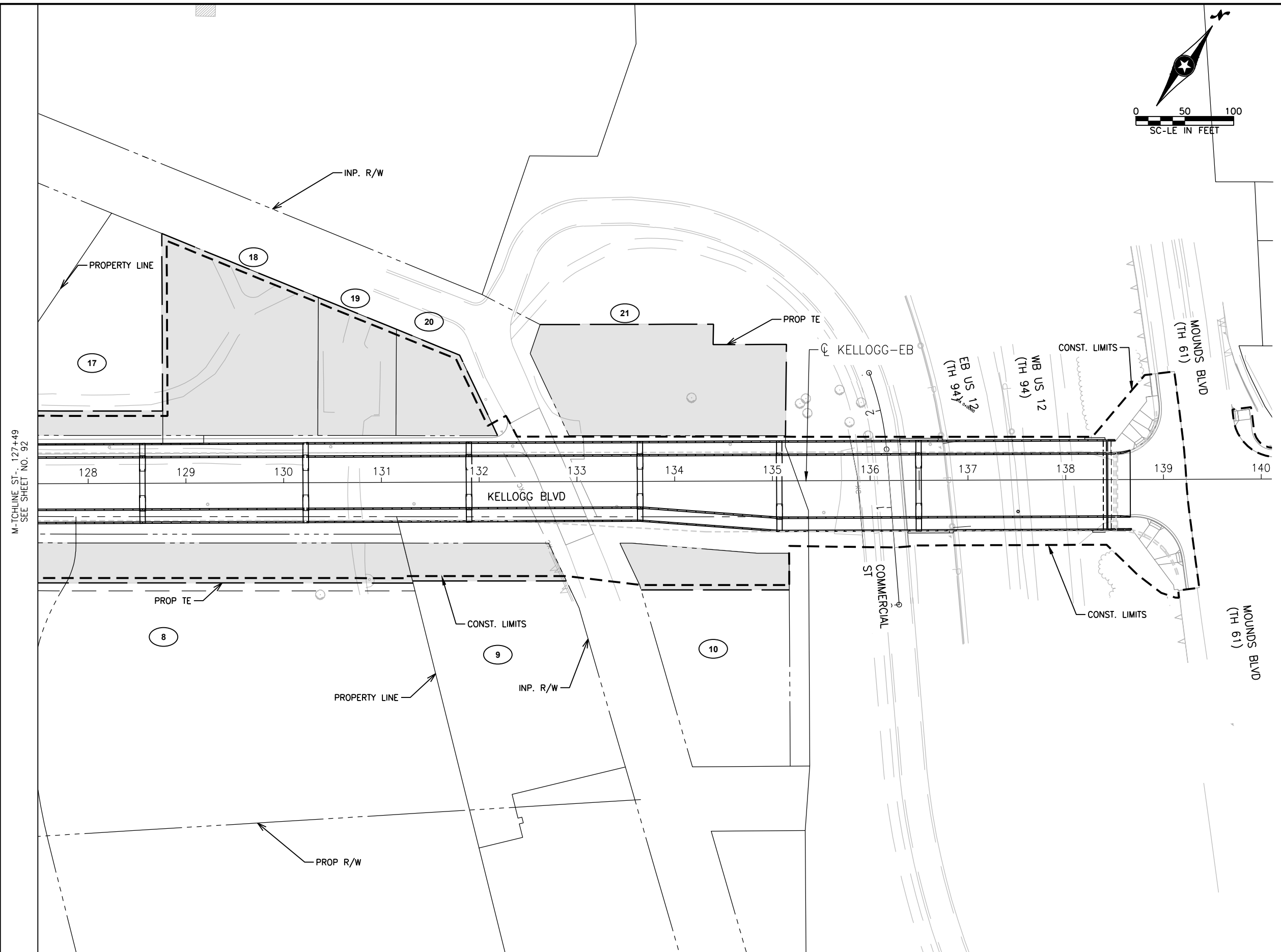
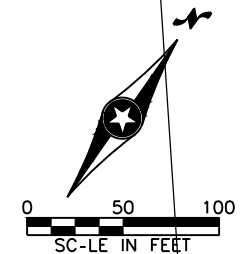
DRAWN BY BDB
DESIGNED BY ADG
CHECKED BY SAS
COMM. NO. 11371



SAINT PAUL, MINNESOTA
RIGHT-OF-WAY PLANS
KELLOGG/3RD STREET BRIDGE REPLACEMENT

SHEET 92 OF 235

LEGEND	
	TEMPOR-RY E-SEMENT
	PERM-NENT E-SEMENT
	P-RCEL NUMBER



M-TCHLINE ST., 127+49
SEE SHEET NO. 92

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NO	D-TE	BY	CKD	-PPR

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.
 Print Name: M-RK REDINGER
 Date: / / License # 59198

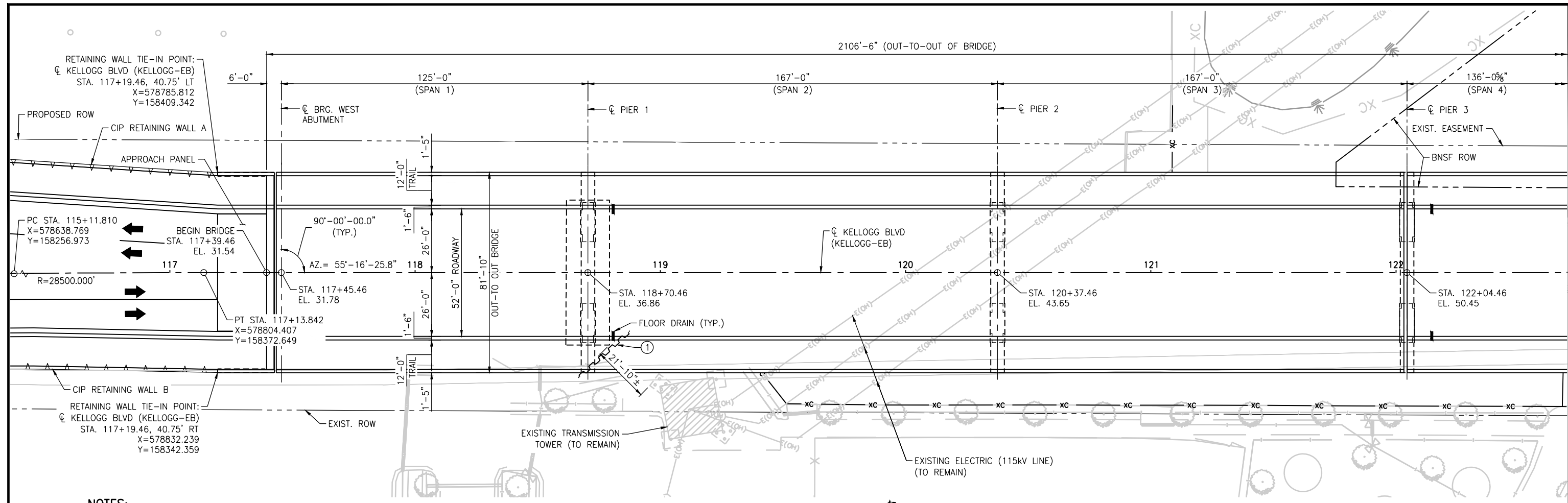
ST-TE -ID PROJECT NO. 164-158-028
 ST-TE PROJECT NO. 6283-261 (TH 94)
 COUNTY PROJECT NO.
 CITY PROJECT NO. B-1202

DR-WN BY BDB
 DESIGNED BY -DG
 CHECKED BY S-S
 COMM. NO. 11371



S-INT P-UL, MINNESOT-
 RIGHT-OF-W-Y PL-NS
 KELLOGG/3RD STREET BRIDGE REPL-CEMENT

SHEET 93
 OF 235



NOTES:

SEE BORINGS - PLAN & PROFILE SHEETS FOR INPLACE UTILITIES.

AN APPROACH SETTLEMENT PERIOD OF 2 MONTHS IS REQUIRED BEFORE CONSTRUCTION OF THE WEST ABUTMENT MAY BEGIN.

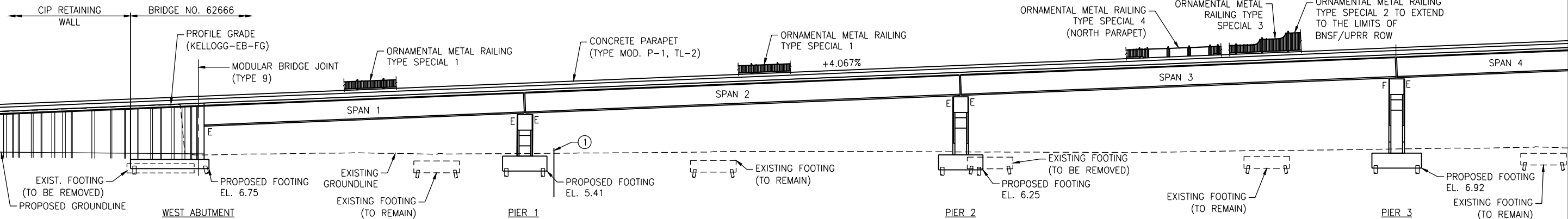
① NO EXCAVATION IS PERMITTED WITHIN 20'-0" OF EXISTING TRANSMISSION TOWER FOOTING. PROVIDE TEMPORARY SHORING AS REQUIRED FOR FOOTING CONSTRUCTION.

GENERAL PLAN



EL. 120
EL. 110
EL. 100
EL. 90
EL. 80
EL. 70
EL. 60
EL. 50
EL. 40
EL. 30
EL. 20
EL. 10
EL. 0

VPT STA. 113+71.000
VPT EL. 16.552



GENERAL ELEVATION

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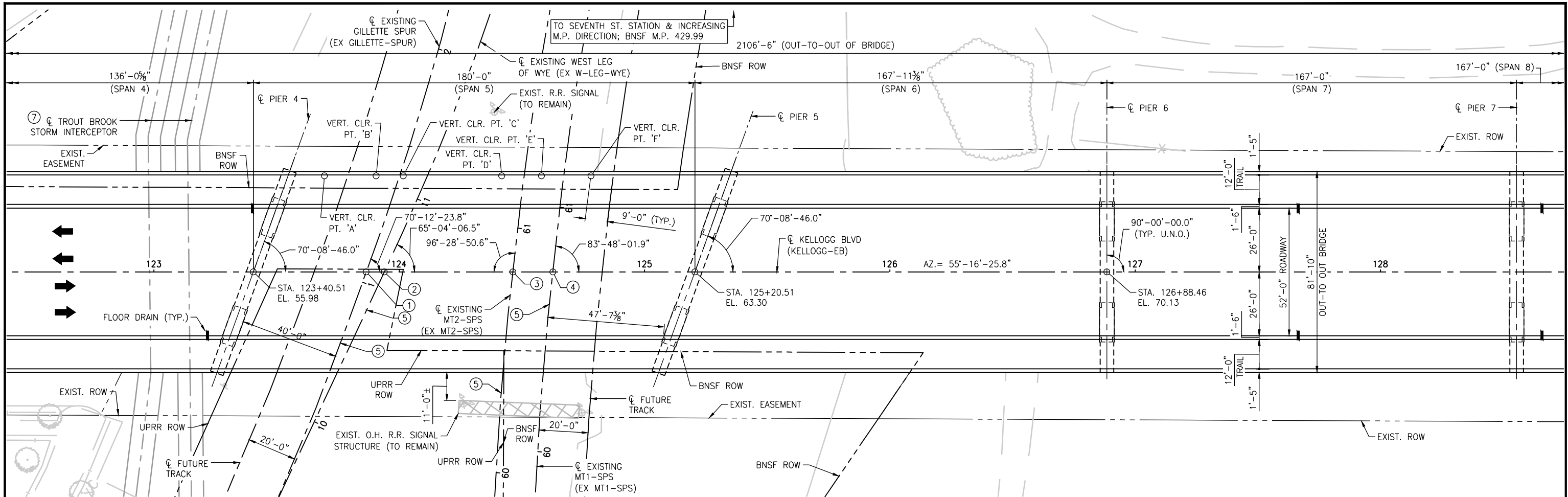
DESIGNED BY	RMS	I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA SIGNED: _____ DATE: XX/XX/XXXX LIC. NO. 44648 JAMISON M. BEISSWENGER
DESIGN CHECK	JMB	
DRAWN BY	HMF	
DWG CHECK	JMB	



KELLOGG BLVD/3RD ST BRIDGE REPLACEMENT
GENERAL PLAN & ELEVATION
(SHEET 1 OF 4)

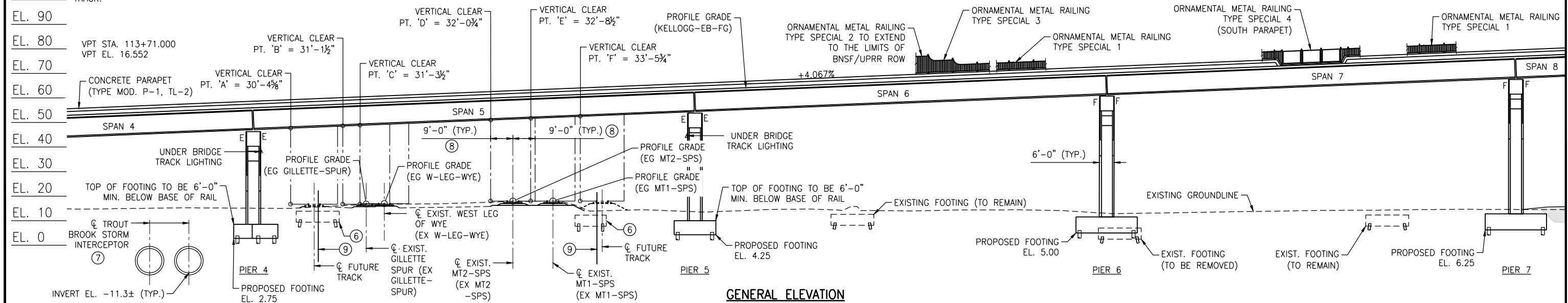
STATE PROJECT NUMBER:	S.P. 164-158-028
CAD DRAWING FOLDER:	B1202 KELLOGG THIRD \ PLANS
APPROVED:	SHEET NO. B3 OF B207 SHEETS

PROJECT: B-1202	
DRAWER: 143	
BRIDGE NO. 62666	



NOTES:

- ① SEE BORINGS - PLAN & PROFILE SHEETS FOR INPLACE UTILITIES.
- ② SEE BORINGS - PLAN & PROFILE FOR RAILROAD ALIGNMENT DATA.
- ③ SEE BORINGS - PLAN & PROFILE FOR RAILROAD ALIGNMENT DATA.
- ④ SEE BORINGS - PLAN & PROFILE FOR RAILROAD ALIGNMENT DATA.
- ⑤ SEE BORINGS - PLAN & PROFILE FOR RAILROAD ALIGNMENT DATA.
- ⑥ EXISTING FOOTING ADJACENT TO ACTIVE R.R. TRACKS TO BE REMOVED TO THE LIMITS AS DIRECTED BY BNSF RAILWAY & UP RAILROAD DURING CONSTRUCTION. SEE SHEET B14.
- ⑦ STOCKPILES OF SOIL AND CONSTRUCTION MATERIAL AND ANY IMPOSITION OF LOADING THAT EXCEEDS THE LIMITS DEFINED IN THE SPECIAL PROVISIONS IS NOT PERMITTED WITHIN 20 FEET FROM THE OUTERMOST EDGES OF THE INPLACE SEWER.
- ⑧ MEASURED PERPENDICULAR TO ALIGNMENT.
- ⑨ TEMPORARY SHORING AS REQUIRED TO REMOVE EXISTING FOUNDATION AND/OR CONSTRUCT NEW FOUNDATIONS. SEE SHEET B2 FOR REQUIREMENTS.



DESIGNED BY	RMS
DESIGN CHECK	JMB
DRAWN BY	HMF
DWG CHECK	JMB

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA

SIGNED: JAMISON M. BEISSWENGER DATE: XX/XX/XXXX LIC. NO. 44648



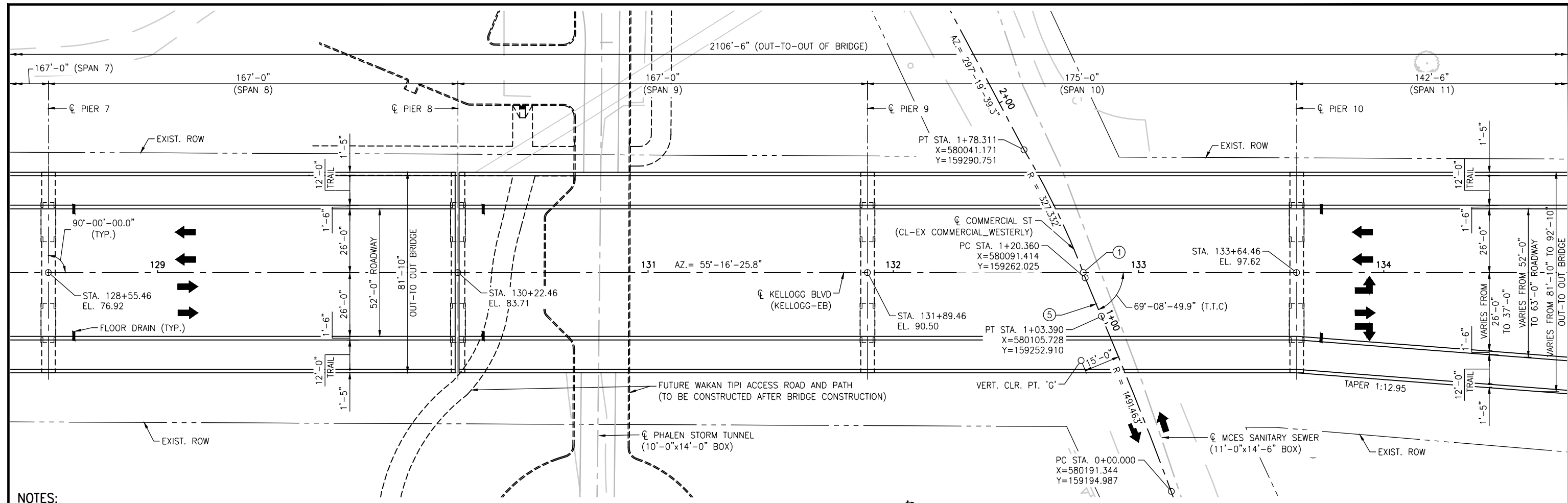
KELLOGG BLVD/3RD ST BRIDGE REPLACEMENT
GENERAL PLAN & ELEVATION
 (SHEET 2 OF 4)

STATE PROJECT NUMBER:	S.P. 164-158-028
CAD DRAWING FOLDER:	B1202 KELLOGG THIRD \ PLANS
APPROVED:	SHEET NO. B4 OFB207 SHEETS

PROJECT:	B-1202
DRAWER:	143
BRIDGE NO.	62666

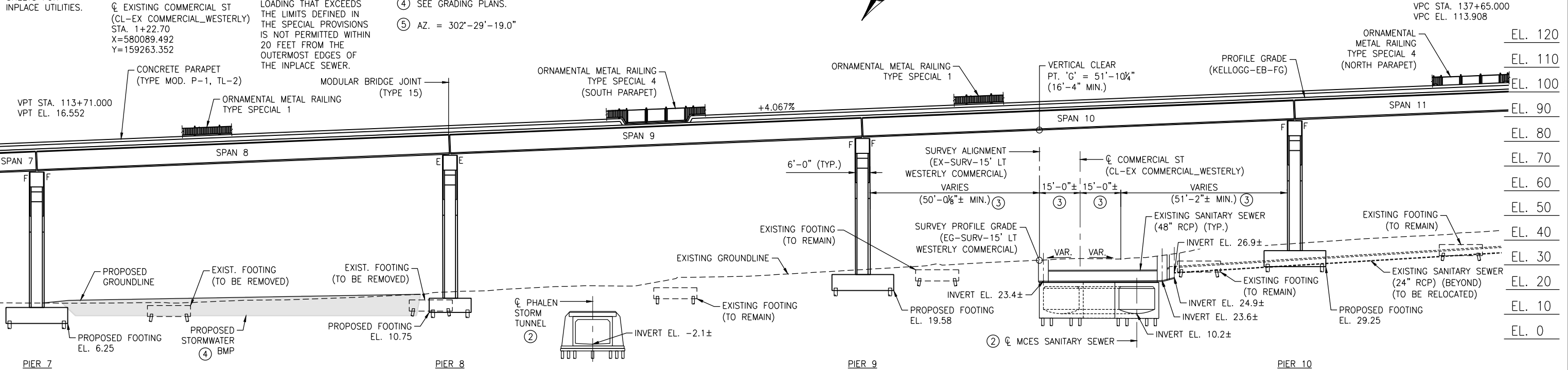


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- NOTES:**
- ① SEE BORINGS - PLAN & PROFILE SHEETS FOR INPLACE UTILITIES.
 - ② STOCKPILES OF SOIL AND CONSTRUCTION MATERIAL AND ANY IMPOSITION OF LOADING THAT EXCEEDS THE LIMITS DEFINED IN THE SPECIAL PROVISIONS IS NOT PERMITTED WITHIN 20 FEET FROM THE OUTERMOST EDGES OF THE INPLACE SEWER.
 - ③ MEASURED PERPENDICULAR TO ALIGNMENT.
 - ④ SEE GRADING PLANS.
 - ⑤ AZ. = 302°-29'-19.0"

GENERAL PLAN



GENERAL ELEVATION

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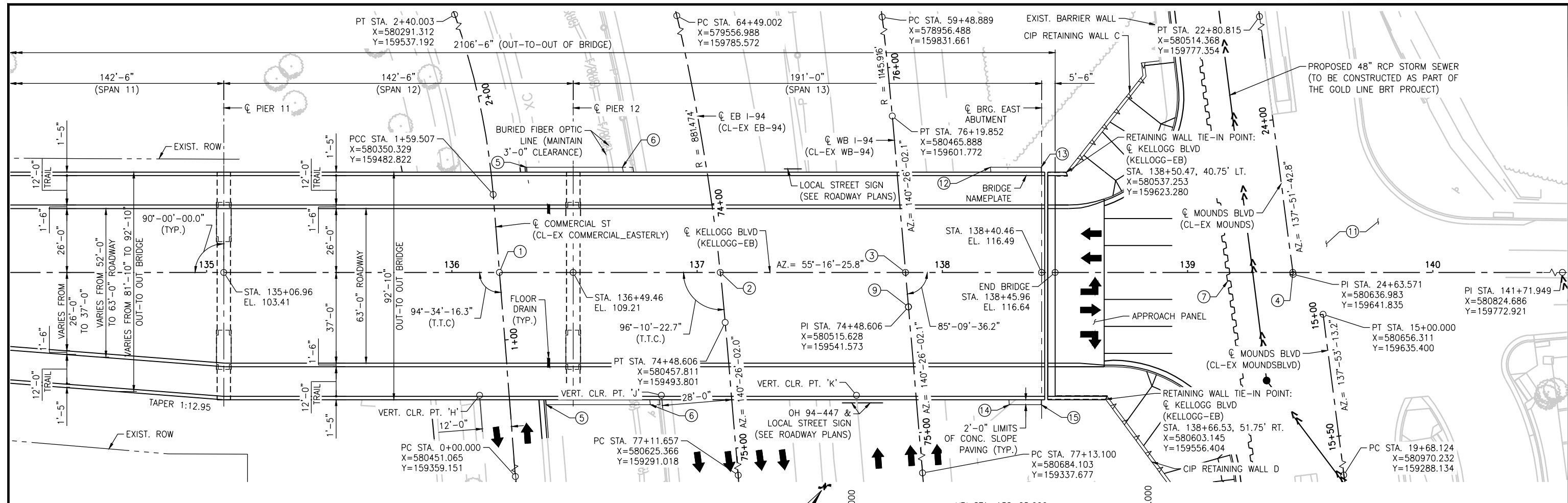
DESIGNED BY	RMS	I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA
DESIGN CHECK	JMB	
DRAWN BY	HMF	
DWG CHECK	JMB	
SIGNED: _____ DATE: XX/XX/XXXX LIC. NO. 44648		
JAMISON M. BEISSWENGER		



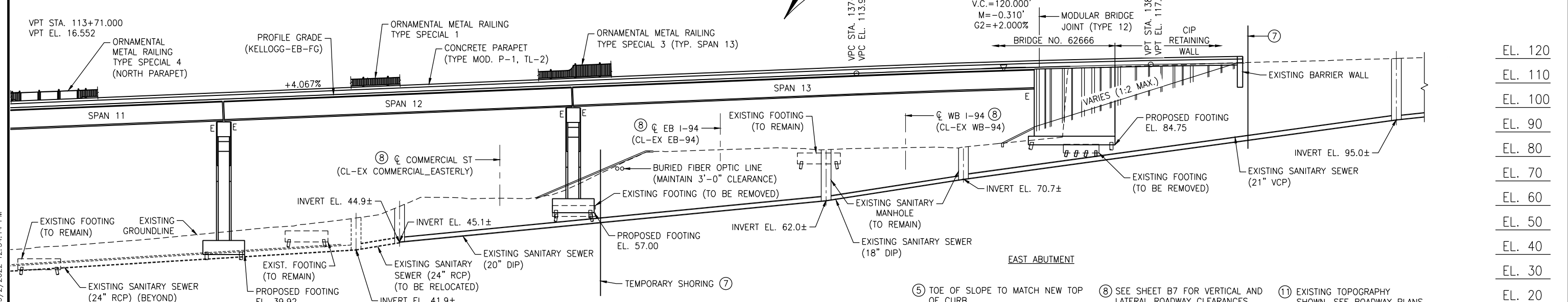
KELLOGG BLVD/3RD ST BRIDGE REPLACEMENT
GENERAL PLAN & ELEVATION
 (SHEET 3 OF 4)

STATE PROJECT NUMBER:	S.P. 164-158-028
CAD DRAWING FOLDER:	B1202 KELLOGG THIRD \ PLANS
APPROVED:	SHEET NO. B5 OF B207 SHEETS

PROJECT: B-1202	
DRAWER: 143	
BRIDGE NO. 62666	



GENERAL PLAN



GENERAL ELEVATION

NOTES:
SEE BORINGS - PLAN & PROFILE SHEETS FOR INPLACE UTILITIES.

- ① CL KELLOGG BLVD (KELLOGG-EB) STA. 136+19.39 CL COMMERCIAL ST (CL-EX COMMERCIAL_EASTERLY) STA. 1+27.61 X=580370.549 Y=159458.155
- ② CONTROL POINT: CL KELLOGG BLVD (KELLOGG-EB) STA. 137+09.45 CL EB I-94 (CL-EX EB-94) STA. 74+28.10 X=580444.566 Y=159509.457
- ③ CL KELLOGG BLVD (KELLOGG-EB) STA. 137+84.96 CL WB I-94 (CL-EX WB-94) STA. 76+83.81 X=580506.624 Y=159552.470
- ④ CL KELLOGG BLVD (KELLOGG-EB) STA. 139+42.90 CL WB I-94 (CL-EX WB-94) STA. 24+62.75 X=580636.434 Y=159642.442

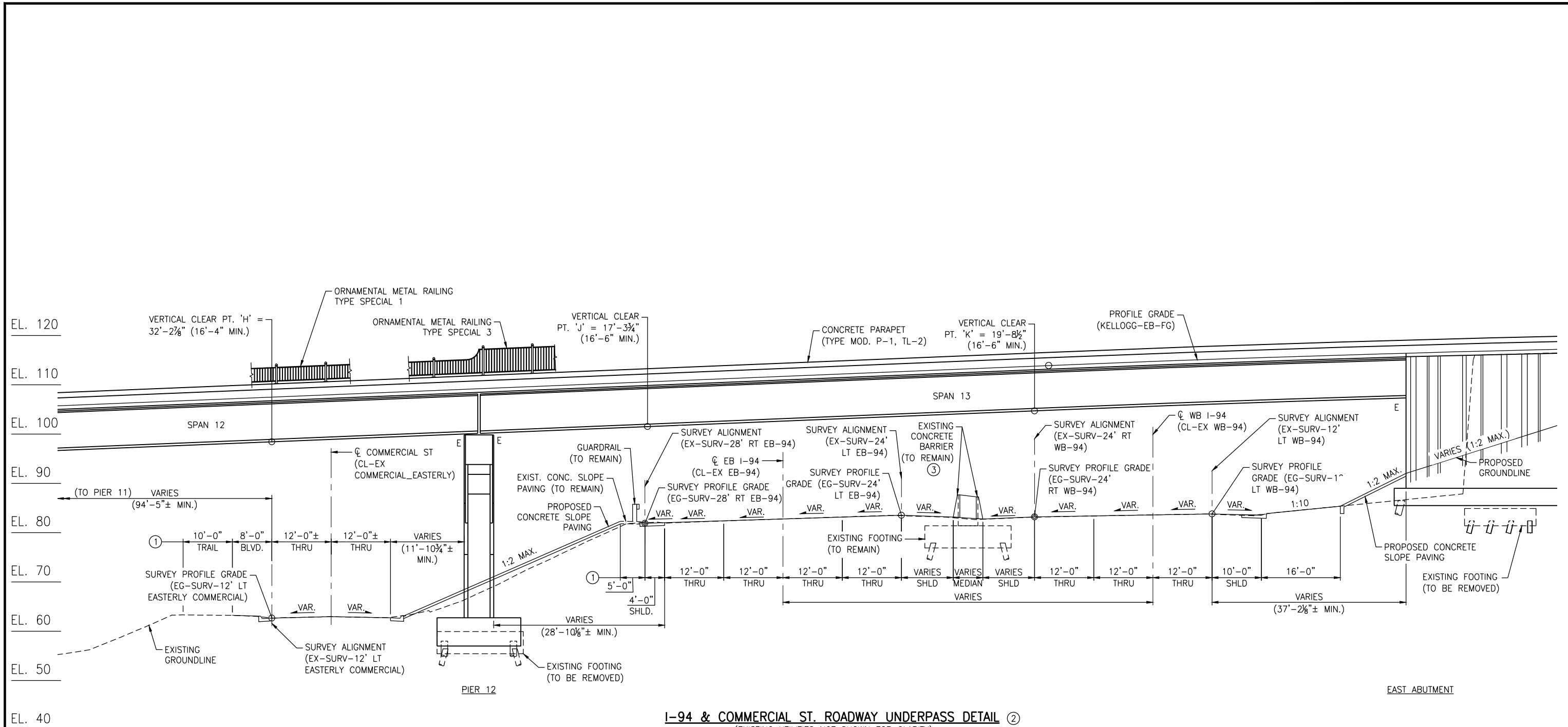
- ⑤ TOE OF SLOPE TO MATCH NEW TOP OF CURB.
- ⑥ TOP OF SLOPE TO MATCH EXISTING CONCRETE SLOPE PAVING ADJACENT TO I-94.
- ⑦ TEMPORARY SHORING MAY BE REQUIRED TO SUPPORT EXISTING OR PROPOSED UTILITY AND CONSTRUCT PIER OR TO CONSTRUCT ABUTMENT AND RETAINING WALLS.
- ⑧ SEE SHEET B7 FOR VERTICAL AND LATERAL ROADWAY CLEARANCES.
- ⑨ STA. EQUATION 76+97.941 BK = 74+48.606 AH.
- ⑩ SEE SHEET B7 FOR ADDITIONAL INFORMATION FOR DEFINITION OF ROADWAY SECTION BELOW BRIDGE.
- ⑪ EXISTING TOPOGRAPHY SHOWN. SEE ROADWAY PLANS FOR PROPOSED GOLD LINE BRT PROJECT GEOMETRY.
- ⑫ TOE OF SLOPE EL. 84.60
- ⑬ TOP OF SLOPE EL. 94.63
- ⑭ TOE OF SLOPE EL. 86.07
- ⑮ TOP OF SLOPE EL. 91.99

DESIGNED BY	RMS	I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA
DESIGN CHECK	JMB	
DRAWN BY	HMF	
DWG CHECK	JMB	
SIGNED: _____ DATE: XX/XX/XXXX LIC. NO. 44648		
JAMISON M. BEISSWENGER		

KELLOGG BLVD/3RD ST BRIDGE REPLACEMENT
GENERAL PLAN & ELEVATION
(SHEET 4 OF 4)

STATE PROJECT NUMBER:	S.P. 164-158-028	PROJECT: B-1202
CAD DRAWING FOLDER:	B1202 KELLOGG THIRD \ PLANS	DRAWER: 143
APPROVED:	SHEET NO. B6 OF B207 SHEETS	BRIDGE NO. 62666

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I-94 & COMMERCIAL ST. ROADWAY UNDERPASS DETAIL ②
 (EXISTING UTILITIES NOT SHOWN FOR CLARITY)

- NOTES:**
- ① MEASURED PERPENDICULAR TO ALIGNMENT.
 - ② SEE SHEET B6 FOR ADDITIONAL INFORMATION AND EXISTING UTILITIES.
 - ③ SEE SHEET B15 FOR REMOVAL DETAILS.

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	DESIGNED BY	RMS	I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA SIGNED: _____ DATE: XX/XX/XXXX LIC. NO. 44648 JAMISON M. BEISSWENGER
	DESIGN CHECK	JMB	
	DRAWN BY	HMF	
	DWG CHECK	JMB	



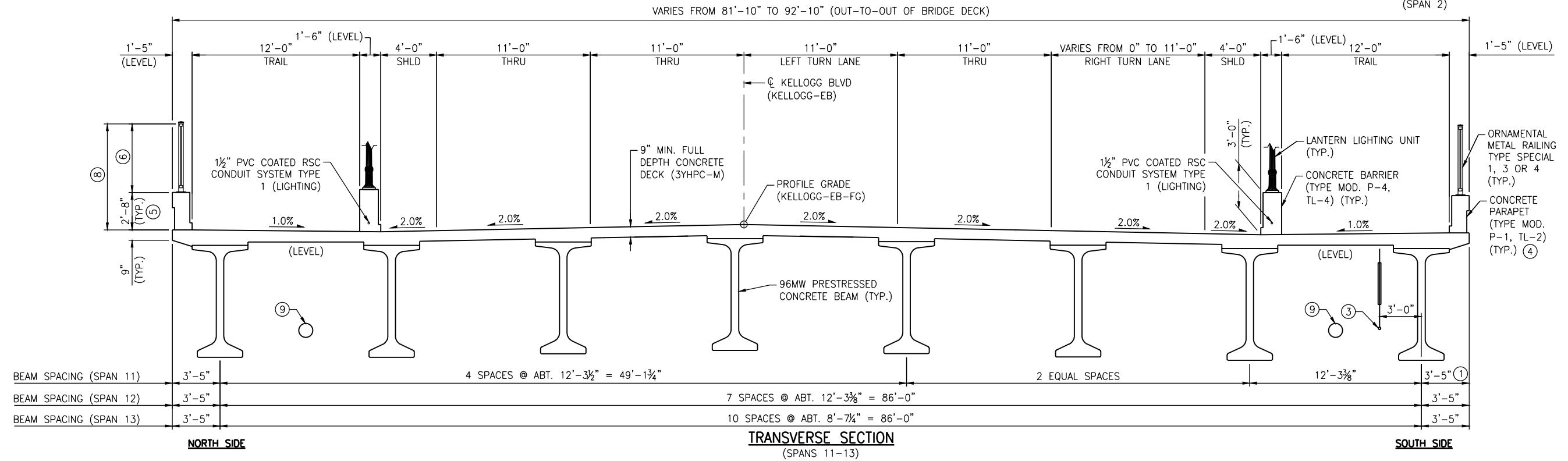
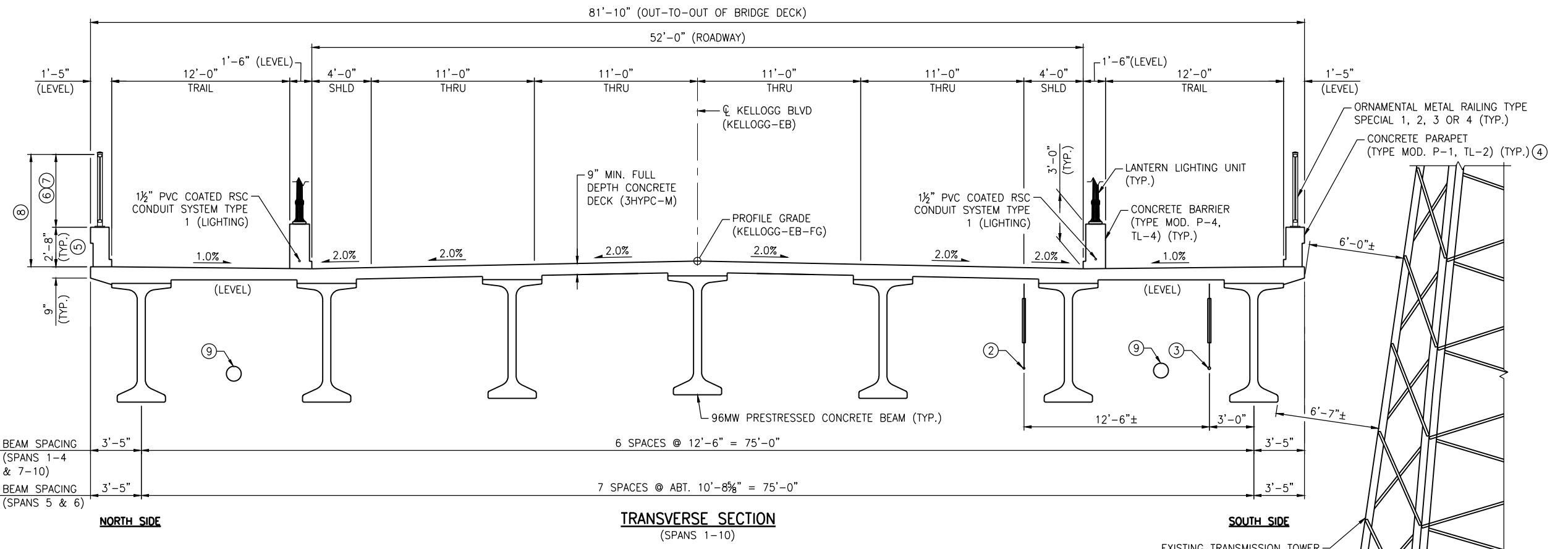
KELLOGG BLVD/3RD ST BRIDGE REPLACEMENT
ROADWAY UNDERPASS DETAIL

STATE PROJECT NUMBER:	S.P. 164-158-028	PROJECT: B-1202
CAD DRAWING FOLDER:	B1202 KELLOGG THIRD \ PLANS	DRAWER: 143
APPROVED:	SHEET NO. B7 OF B207 SHEETS	BRIDGE NO. 62666



NOTES:

- ① MEASURED PERPENDICULAR TO SOUTH EDGE OF DECK.
- ② 1½" DIA. PVC COATED RSC CONDUIT SYSTEM TYPE 2 (RR LIGHTING) - (SPANS 1-5).
- ③ 2" DIA. PVC COATED RSC CONDUIT SYSTEM TYPE 3 (SIGNALS).
- ④ BARRIER TRANSITION TO CONCRETE CURB WHERE ORNAMENTAL METAL RAILING TYPE SPECIAL 4 IS USED.
- ⑤ VARIES 6" TO 2'-8" WHERE ORNAMENTAL METAL RAILING TYPE SPECIAL 4 IS USED.
- ⑥ 3'-4" TYP. (ORNAMENTAL METAL RAILING TYPE SPECIAL 1), 7'-4" OVER RAILROAD (ORNAMENTAL METAL RAILING TYPE SPECIAL 2), 5'-4" OVER I-94 (ORNAMENTAL METAL RAILING TYPE SPECIAL 3), VARIES 3'-4" TO 5'-6" WHERE ORNAMENTAL METAL RAILING TYPE SPECIAL 4 IS USED.
- ⑦ ORNAMENTAL METAL RAILING TYPE SPECIAL 2 (OVER RAILROAD) TO INCLUDE VINYL COATED CHAIN LINK FENCE TO COMPLY WITH BNSF GUIDELINES FOR RAILROAD GRADE SEPARATION PROJECTS SECTION 4.7.B.1.
- ⑧ 6'-0" TYP. (ORNAMENTAL METAL RAILING TYPE SPECIAL 1 AND TYPE SPECIAL 4), 10'-0" OVER RAILROAD (ORNAMENTAL METAL RAILING TYPE SPECIAL 2), 8'-0" OVER I-94, (ORNAMENTAL METAL RAILING TYPE SPECIAL 3).
- ⑨ 12" DIA. MAINLINE DRAIN PIPE (SPANS 3-4 & 7-12) OR 8" FEED PIPE (SPAN 2); SEE DECK DRAINAGE DETAILS.



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DESIGNED BY	RMS	I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA
DESIGN CHECK	JMB	
DRAWN BY	HMF	
DWG CHECK	JMB	
SIGNED:	RICHARD M. SNYDER	



**KELLOGG BLVD/3RD ST BRIDGE REPLACEMENT
TRANSVERSE SECTION**

STATE PROJECT NUMBER:	S.P. 164-158-028	PROJECT: B-1202
CAD DRAWING FOLDER:	B1202 KELLOGG THIRD \ PLANS	DRAWER: 143
APPROVED:	SHEET NO. B9 OF B207 SHEETS	BRIDGE NO. 62666



Regional Program Year Policy

The Regional Program Year Policy is intended to manage the development and timely delivery of transportation projects awarded federal funds through the TAB's Regional Solicitation Process.

Project sponsors awarded federal funds through the regional solicitation process are expected to get their project ready for authorization in their program year.

The program year is July 1 to June 30 of the year in which the project is originally programmed in the Transportation Improvement Program (TIP).

By April 1 of the program year, the project must meet the criteria on the attached sheet.

Additionally, if a regionally selected project is not ready to request authorization by June 15 of its program year, the project will not be carried over into the new TIP unless the project sponsor receives a program year extension from the TAB.

Project sponsors that have made significant progress but are delayed by circumstances that prevent them from delivering their projects on time must submit a request for a program year extension to the TAB Coordinator by December 31 of the project's program year.

The maximum length of a program year extension is one year. Projects are eligible for only one program year extension request.

If a program year extension is granted, funding the project will be contingent on the availability of federal funds. A project sponsor is responsible for funding the project until federal funding becomes available.

Projects receiving program year extensions will not receive an inflationary cost increase in their federal cost caps.

"Procedure to Request a Program Year Extension" is provided as Attachment 1.

Criteria for Meeting Program Year

Construction Projects through the FHWA Process:

- Environmental document approved – April 1
 - *Environmental Documentation draft submittal due December 1*
- Right of way certificate approved – April 1
 - *Condemnation proceedings formally initiated by February 28 with title and possession by June 1.*
- Final construction plans submitted and reviewed for standards, eligibility, funding and structural design – April 1
- Engineer's estimate – April 1
- Utility relocation certificate – April 1
- Permit applications submitted – April 1

Construction Projects through the FTA Process

- Environmental document completed; project plans complete and reflect the project that was selected
- Letting date can be set within 90 days
- FTA notification that grant approval imminent

Right of Way Only Projects through FHWA Process

- Environmental document approved – April 1
- OCPPM/SALT authorization to proceed – June 1

Right of Way Only Projects through FTA Process

- Environmental document completed
- Appraisals over \$250,000 approved by FTA; under \$250,000 reviewed by Right of Way Section
- FTA notifies that grant approval is imminent
- OCPPM transfers funds
- Offers made/condemnation initiated if offers refused

Program Project

- Grant application submitted to FTA; includes work plan
- Notification from FTA that grant approval is imminent
- Work will begin within 90 days after grant approval
- Agreement executed between MnDOT and proposer once funds are transferred

PROCEDURE TO REQUEST A PROGRAM YEAR EXTENSION

If it appears that a project cannot meet the deadline for authorization within its program year and a program year extension is necessary, the project sponsor must demonstrate to the Funding and Programming Committee that significant progress has been made on the project and the program year criteria can be met within the requested one-year time extension. Projects may be granted only one program year extension. Requests for a program year extension must be submitted by December 31 of the project's program year.

The answers provided on the Progress Schedule for Program Year Extension on Attachment 1 will determine whether a project is **eligible** for a one-year extension. In addition to responding to the Progress Schedule for Program Year Extension, the project sponsor must submit the following materials to the Funding and Programming Committee so it can determine if a program year extension is reasonable:

- 1) Project Background (will be provided by TAB Coordinator).
- 2) Project Progress:
 - a) Complete attached progress schedule with actual dates.
 - b) Right of way acquisition - provide map showing status of individual parcels.
 - c) Plans - Provide layout and discussion on percent of plan completion.
 - d) Permits - provide a list of permitting agencies, permits needed and status.
 - e) Approvals - provide a list of agencies with approval authority and approval status.
 - f) Identify funds and other resources spent to date on project.
- 3) Justification for Extension Request:
 - a) What is unique about this project that requires an extension of the program year?
 - b) What are the financial impacts if this project does not meet its current program year?
 - c) What are the implications if the project does not obtain the requested extension?
 - d) What actions will the agency take to resolve the problems facing the project in the next three to six months?

PROCESS AND ROLES

The Funding and Programming Committee will hear all requests for extensions. The Committee's recommendation will be forwarded to the TAC and TAB for action. The requests will be presented to the TAB for action on its consent agenda. Staff for the Funding and Programming Committee will notify the applicant of the committee's decision.