



Application

10356 - 2018 Bridges

10992 - Replacement of Kellogg/3rd Street Bridge No. 62080 and 62080A

Regional Solicitation - Roadways Including Multimodal Elements

Status: Submitted
Submitted Date: 07/13/2018 9:11 AM

Primary Contact

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What Grant Programs are you most interested in?	Regional Solicitation - Roadways Including Multimodal Elements			

Organization Information

Name: ST PAUL, CITY OF

Jurisdictional Agency (if different):

Organization Type:

City

Organization Website:

Address:

DEPT OF PUBLIC WORKS-CITY HALL ANNEX
25 W 4TH ST #1500

*

ST PAUL	Minnesota	55101
City	State/Province	Postal Code/Zip

County:

Ramsey

Phone:*

651-266-9700
Ext.

Fax:

PeopleSoft Vendor Number

0000003222A22

Project Information

Project Name

Replacement of Kellogg/3rd Street Bridge Nos. 62080 and 62080A

Primary County where the Project is Located

Ramsey

Cities or Townships where the Project is Located:

Saint Paul

Jurisdictional Agency (If Different than the Applicant):

This project is to reconstruct Kellogg Boulevard / Third Street retaining walls, approach roadway and Bridge Nos. 62080 and 62080A over Ramsey County Regional Rail Authority (RCRRA), BNSF Railway, Bruce Vento Nature Sanctuary, Commercial Street, and Minnesota Department of Transportation (MnDOT) Trunk Highway I-94.

Project limits are from 635 feet west of Lafayette Street to 150 feet west of Maria Avenue (total project length of 3,563 feet, bridge project length of 2,112 feet).

The existing 2,116 foot bridge was constructed in 1982/83, has a sufficiency rating of 36.7 in the most recent MnDOT structural inventory report, and is structurally deficient. The bridge was designed as a four lane vehicular bridge, with a 10 foot combined use bicycle/pedestrian trail.

Brief Project Description (Include location, road name/functional class, type of improvement, etc.)

In 2014 a structural evaluation determined that the deteriorated pier cap cantilevers could not support any live load under the current MnDOT-approved analysis method. The bridge was temporarily closed to allow for installation of concrete barriers that restrict all modes of traffic to the center portion of the pier caps. The bridge reopened as a reconfigured three vehicular lane bridge (two inbound and one outbound) with substandard 1.75-foot shoulders and a substandard 6 foot bicycle/pedestrian walk. AADT of the project segment is estimated at 14,200 vehicles per day (A-minor reliever).

The City supports reconstruction as this would further allow for upgraded bicycle and pedestrian facilities as well as adequate accommodation for Gateway Bus Rapid Transit (BRT), Rush Line and Red Rock transitways, and future LRT.

(Limit 2,800 characters; approximately 400 words)

TIP Description Guidance (will be used in TIP if the project is selected for funding)

Project Length (Miles)

to the nearest one-tenth of a mile

MSAS 158 Kellogg/3rd, from 635' E of Lafayette St to 150' E of Maria Ave - Replace Bridge Nos. 62080 & 62080A with new bridge including approach roadways and intersections

0.7

Project Funding

Are you applying for competitive funds from another source(s) to implement this project?

Yes

If yes, please identify the source(s)

State Legislative Direct Appropriation

Federal Amount

\$7,000,000.00

Match Amount

\$47,145,000.00

Minimum of 20% of project total

Project Total

\$54,145,000.00

Match Percentage

87.07%

Minimum of 20%

Compute the match percentage by dividing the match amount by the project total

Source of Match Funds

State Bridge Bonds, Various Local Funds, Direct Appropriations

A minimum of 20% of the total project cost must come from non-federal sources; additional match funds over the 20% minimum can come from other federal sources

Preferred Program Year

Select one:

2022

Select 2020 or 2021 for TDM projects only. For all other applications, select 2022 or 2023.

Additional Program Years:

Select all years that are feasible if funding in an earlier year becomes available.

Project Information-Roadways

County, City, or Lead Agency

City of Saint Paul

Functional Class of Road

A Minor Reliever

Road System

MSAS

TH, CSAH, MSAS, CO. RD., TWP. RD., CITY STREET

Road/Route No.

158

i.e., 53 for CSAH 53

Name of Road

Kellogg Boulevard/3rd Street (MSAS 158)

Example: 1st ST., MAIN AVE

Zip Code where Majority of Work is Being Performed	55101
(Approximate) Begin Construction Date	09/15/2020
(Approximate) End Construction Date	12/31/2022

TERMINI:(Termini listed must be within 0.3 miles of any work)

From: (Intersection or Address)	635' west of Lafayette Street
To: (Intersection or Address)	150' west of Maria Avenue

DO NOT INCLUDE LEGAL DESCRIPTION

Or At

Primary Types of Work	Bridge, Retaining Walls, Ped Ramps, Aggregate Base, Bit Surface, Signals, Lighting, Guard Rail, Bike Path, Sidewalk.
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Examples: GRADE, AGG BASE, BIT BASE, BIT SURF, SIDEWALK, CURB AND GUTTER, STORM SEWER, SIGNALS, LIGHTING, GUARDRAIL, BIKE PATH, PED RAMPS, BRIDGE, PARK AND RIDE, ETC.

BRIDGE/CULVERT PROJECTS (IF APPLICABLE)

Old Bridge/Culvert No.:	62080 and 62080A
New Bridge/Culvert No.:	Not assigned
Structure is Over/Under (Bridge or culvert name):	Kellogg Blvd over Commercial St, I-94, RR, Parks

Requirements - All Projects

All Projects

1. The project must be consistent with the goals and policies in these adopted regional plans: Thrive MSP 2040 (2014), the 2040 Transportation Policy Plan (2015), the 2040 Regional Parks Policy Plan (2015), and the 2040 Water Resources Policy Plan (2015).

Check the box to indicate that the project meets this requirement. Yes

2. The project must be consistent with the 2040 Transportation Policy Plan. Reference the 2040 Transportation Plan goals, objectives, and strategies that relate to the project.

The proposed project fits the majority of goals and objectives identified in the 2040 Transportation Policy Plan.

"Transportation System Stewardship" Goal (p58)

- Efficiently preserves and maintains the regional transportation system in a state of good repair.

- Operates the system to efficiently connect people to destinations.

Bridge replacement provides the opportunity for improvement of the non-motorized system into and out of downtown. The existing bridge sidewalk is substandard, located only on the south side of the bridge, and lacks connections to key user destinations including Lowertown, CHS Field, and Metro State University. These system deficiencies will be corrected.

List the goals, objectives, strategies, and associated pages:

"Safety and Security" Goal (p60)

- Reduces crashes and improve safety and security for all modes of passenger travel.

- Reduces the system's vulnerability to man-made incidents.

The proposed project improves bike and ped facility safety, roadway lighting, and at-grade driveways/intersections where vehicles encounter bikes and peds.

Though often unreported, Public Works has made several minor repairs to bridge barriers since 2014, especially near Kellogg/Mounds. Errant vehicles are not uncommon; a new bridge design and signal improvements can lower the frequency of such occurrences.

"Access to Destinations" Goal (p62)

- Increases availability of multimodal travel options.
- Increases travel time reliability and predictability.

- Improves multimodal travel options for people to connect to jobs and other opportunities.

Congestion can be significant during peak hours or when there is a traffic incident at intersections on either side of the bridge. Travel time is often unreliable as backups build quickly over the bridge's 2,000+ foot length. This issue is worsened by the fact that there are limited downtown/freeway access alternatives, and only a single outbound lane.

"Competitive Economy" Goal (p64)

- Improves multimodal access to regional job concentrations identified in Thrive SP 4020

"Healthy Environment" Goal (p66)

- Increases availability and attractiveness of transit, bicycling, and walking.
- Provides a transportation system that promotes community cohesion and connectivity for people of all ages and abilities, particularly for historically under represented populations.

3. The project or the transportation problem/need that the project addresses must be in a local planning or programming document. Reference the name of the appropriate comprehensive plan, regional/statewide plan, capital improvement program, corridor study document [studies on trunk highway must be approved by the Minnesota Department of Transportation and the Metropolitan Council], or other official plan or program of the applicant agency [includes Safe Routes to School Plans] that the project is included in and/or a transportation problem/need that the project addresses.

List the applicable documents and pages:

This project is included in the 5-year prioritized bridge replacement plan approved by the Saint Paul City Council on December 19, 2014. [RES 14-2129]

4. The project must exclude costs for studies, preliminary engineering, design, or construction engineering. Right-of-way costs are only eligible as part of transit stations/stops, transit terminals, park-and-ride facilities, or pool-and-ride lots. Noise barriers, drainage projects, fences, landscaping, etc., are not eligible for funding as a standalone project, but can be included as part of the larger submitted project, which is otherwise eligible.

Check the box to indicate that the project meets this requirement. Yes

5. Applicants that are not cities or counties in the seven-county metro area with populations over 5,000 must contact the MnDOT Metro State Aid Office prior to submitting their application to determine if a public agency sponsor is required.

Check the box to indicate that the project meets this requirement. Yes

6. Applicants must not submit an application for the same project elements in more than one funding application category.

Check the box to indicate that the project meets this requirement. Yes

7. The requested funding amount must be more than or equal to the minimum award and less than or equal to the maximum award. The cost of preparing a project for funding authorization can be substantial. For that reason, minimum federal amounts apply. Other federal funds may be combined with the requested funds for projects exceeding the maximum award, but the source(s) must be identified in the application. Funding amounts by application category are listed below.

Roadway Expansion: \$1,000,000 to \$7,000,000

Roadway Reconstruction/ Modernization Modernization and Spot Mobility: \$1,000,000 to \$7,000,000

Traffic Management Technologies (Roadway System Management): \$250,000 to \$7,000,000

Bridges Rehabilitation/ Replacement: \$1,000,000 to \$7,000,000

Check the box to indicate that the project meets this requirement. Yes

8. The project must comply with the Americans with Disabilities Act (ADA).

Check the box to indicate that the project meets this requirement. Yes

9. In order for a selected project to be included in the Transportation Improvement Program (TIP) and approved by USDOT, the public agency sponsor must either have, or be substantially working towards, completing a current Americans with Disabilities Act (ADA) self-evaluation or transition plan that covers the public right of way/transportation, as required under Title II of the ADA.

The applicant is a public agency that employs 50 or more people and has an adopted ADA transition plan that covers the public right of way/transportation. Yes

04/27/2010

Date plan adopted by governing body

The applicant is a public agency that employs 50 or more people and is currently working towards completing an ADA transition plan that covers the public rights of way/transportation.

Date process started

Date of anticipated plan completion/adoption

The applicant is a public agency that employs fewer than 50 people and has a completed ADA self-evaluation that covers the public rights of way/transportation.

Date self-evaluation completed

The applicant is a public agency that employs fewer than 50 people and is working towards completing an ADA self-evaluation that covers the public rights of way/transportation.

Date process started

Date of anticipated plan completion/adoption

(TDM Applicants Only) The applicant is not a public agency subject to the self-evaluation requirements in Title II of the ADA.

10. The project must be accessible and open to the general public.

Check the box to indicate that the project meets this requirement. Yes

11. The owner/operator of the facility must operate and maintain the project year-round for the useful life of the improvement, per FHWA direction established 8/27/2008 and updated 6/27/2017.

Check the box to indicate that the project meets this requirement. Yes

12. The project must represent a permanent improvement with independent utility. The term independent utility means the project provides benefits described in the application by itself and does not depend on any construction elements of the project being funded from other sources outside the regional solicitation, excluding the required non-federal match. Projects that include traffic management or transit operating funds as part of a construction project are exempt from this policy.

Check the box to indicate that the project meets this requirement. Yes

13. The project must not be a temporary construction project. A temporary construction project is defined as work that must be replaced within five years and is ineligible for funding. The project must also not be staged construction where the project will be replaced as part of future stages. Staged construction is eligible for funding as long as future stages build on, rather than replace, previous work.

Check the box to indicate that the project meets this requirement. Yes

14. The project applicant must send written notification regarding the proposed project to all affected state and local units of government prior to submitting the application.

Check the box to indicate that the project meets this requirement. Yes

Roadways Including Multimodal Elements

1. All roadway and bridge projects must be identified as a principal arterial (non-freeway facilities only) or A-minor arterial as shown on the latest TAB approved roadway functional classification map.

Check the box to indicate that the project meets this requirement. Yes

Roadway Expansion and Reconstruction/Modernization and Spot Mobility projects only:

2. The project must be designed to meet 10-ton load limit standards.

Check the box to indicate that the project meets this requirement. Yes

Bridge Rehabilitation/Replacement projects only:

3. Projects requiring a grade-separated crossing of a principal arterial freeway must be limited to the federal share of those project costs identified as local (non-MnDOT) cost responsibility using MnDOT's Cost Participation for Cooperative Construction Projects and Maintenance Responsibilities manual. In the case of a federally funded trunk highway project, the policy guidelines should be read as if the funded trunk highway route is under local jurisdiction.

Check the box to indicate that the project meets this requirement. Yes

4. The bridge must carry vehicular traffic. Bridges can carry traffic from multiple modes. However, bridges that are exclusively for bicycle or pedestrian traffic must apply under one of the Bicycle and Pedestrian Facilities application categories. Rail-only bridges are ineligible for funding.

Check the box to indicate that the project meets this requirement. Yes

5. The length of the bridge must equal or exceed 20 feet.

Check the box to indicate that the project meets this requirement. Yes

6. The bridge must have a sufficiency rating less than 80 for rehabilitation projects and less than 50 for replacement projects. Additionally, the bridge must also be classified as structurally deficient or functionally obsolete.

Check the box to indicate that the project meets this requirement. Yes

Roadway Expansion, Reconstruction/Modernization and Spot Mobility, and Bridge Rehabilitation/Replacement projects only:

7. All roadway projects that involve the construction of a new/expanded interchange or new interchange ramps must have approval by the Metropolitan Council/MnDOT Interchange Planning Review Committee prior to application submittal. Please contact Michael Corbett at MnDOT (Michael.J.Corbett@state.mn.us or 651-234-7793) to determine whether your project needs to go through this process.

Check the box to indicate that the project meets this requirement. Yes

Requirements - Roadways Including Multimodal Elements

Specific Roadway Elements

CONSTRUCTION PROJECT ELEMENTS/COST ESTIMATES	Cost
Mobilization (approx. 5% of total cost)	\$2,200,000.00
Removals (approx. 5% of total cost)	\$2,200,000.00
Roadway (grading, borrow, etc.)	\$200,000.00
Roadway (aggregates and paving)	\$1,500,000.00
Subgrade Correction (muck)	\$0.00
Storm Sewer	\$50,000.00
Ponds	\$15,000.00
Concrete Items (curb & gutter, sidewalks, median barriers)	\$50,000.00
Traffic Control	\$200,000.00
Striping	\$20,000.00
Signing	\$10,000.00
Lighting	\$150,000.00
Turf - Erosion & Landscaping	\$10,000.00
Bridge	\$36,000,000.00
Retaining Walls	\$1,500,000.00
Noise Wall (not calculated in cost effectiveness measure)	\$0.00
Traffic Signals	\$750,000.00
Wetland Mitigation	\$0.00
Other Natural and Cultural Resource Protection	\$0.00
RR Crossing	\$0.00
Roadway Contingencies	\$0.00
Other Roadway Elements	\$0.00
Totals	\$44,855,000.00

Specific Bicycle and Pedestrian Elements

CONSTRUCTION PROJECT ELEMENTS/COST ESTIMATES	Cost
Path/Trail Construction	\$100,000.00

Sidewalk Construction	\$9,000,000.00
On-Street Bicycle Facility Construction	\$0.00
Right-of-Way	\$0.00
Pedestrian Curb Ramps (ADA)	\$40,000.00
Crossing Aids (e.g., Audible Pedestrian Signals, HAWK)	\$0.00
Pedestrian-scale Lighting	\$50,000.00
Streetscaping	\$0.00
Wayfinding	\$0.00
Bicycle and Pedestrian Contingencies	\$0.00
Other Bicycle and Pedestrian Elements	\$0.00
Totals	\$9,190,000.00

Specific Transit and TDM Elements

CONSTRUCTION PROJECT ELEMENTS/COST ESTIMATES	Cost
Fixed Guideway Elements	\$0.00
Stations, Stops, and Terminals	\$0.00
Support Facilities	\$50,000.00
Transit Systems (e.g. communications, signals, controls, fare collection, etc.)	\$50,000.00
Vehicles	\$0.00
Contingencies	\$0.00
Right-of-Way	\$0.00
Other Transit and TDM Elements	\$0.00
Totals	\$100,000.00

Transit Operating Costs

Number of Platform hours	0
Cost Per Platform hour (full loaded Cost)	\$0.00
Subtotal	\$0.00
Other Costs - Administration, Overhead,etc.	\$0.00

Totals

Total Cost	\$54,145,000.00
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Construction Cost Total	\$54,145,000.00
Transit Operating Cost Total	\$0.00

Measure A: Distance to the nearest parallel bridge

RESPONSE:

Location of nearest parallel bridge crossing: 7th Street / T.H. 5 is located 0.27 miles northwest

Distance from one end of proposed project to nearest parallel crossing (that is an A-minor arterial or principal arterial) and then back to the other side of the proposed project (calculated by Council Staff): 0

Explanation: 7th Street / T.H. 5 spans provides the nearest alternative route connecting Downtown Saint Paul with the Dayton's Bluff neighborhood, and spanning over the underlying railroad corridor. Connection with I-94 may be a more substantial challenge during closure of the Kellogg-Third Bridge (Mounds Boulevard/I-94 access be be impacted) and result in increased congestion/travel time along T.H. 5.

(Limit 2,800 characters; approximately 400 words)

Measure B: Project Location Relative to Jobs, Manufacturing, and Education

Existing Employment within 1 Mile: 70343

Existing Manufacturing/Distribution-Related Employment within 1 Mile: 3728

Existing Post-Secondary Students within 1 Mile: 8424

Upload Map 1530639944248_kellogg-third-economy-map 2018-07-03.pdf

Please upload attachment in PDF form.

Measure C: Regional Truck Corridor Tiers

RESPONSE (Select one for your project, based on the Regional Truck Corridor Study):

The project is located on either a Tier 1, Tier 2, or Tier 3 corridor:

(65 Points)

The project provides a direct and immediate connection (i.e., intersects) with either a Tier 1, Tier 2, or Tier 3 corridor:

(10 Points)

The project is not located on a Tier 1, Tier 2, or Tier 3 corridor:

(0 Points)

Measure A: Current Daily Person Throughput

Location	Kellogg Blvd
Current AADT Volume	14200.0
Existing Transit Routes on the Project:	3, 16, 21, 54, 63, 70, 94, 262, 294, 350, 351, 353, 355, 361, 364, 365, 375, 417, 452, 480, 484, 489
Upload "Transit Connections" map	1530644186936_kellogg-third-transit-map 2018-07-03.pdf
<i>Please upload attachment in PDF form.</i>	

Response: Current Daily Person Throughput

Average Annual Daily Transit Ridership	6185.0
Current Daily Person Throughput	24645.0

Measure B: 2040 Forecast ADT

Use Metropolitan Council model to determine forecast (2040) ADT volume	Yes
If checked, METC Staff will provide Forecast (2040) ADT volume	16400
OR	
Identify the approved county or city travel demand model to determine forecast (2040) ADT volume	
Forecast (2040) ADT volume	

Measure A: Connection to disadvantaged populations and projects benefits, impacts, and mitigation

Select one:

Project located in Area of Concentrated Poverty with 50% or more of residents are people of color (ACP50): Yes

(up to 100% of maximum score)

Project located in Area of Concentrated Poverty:

(up to 80% of maximum score)

Projects census tracts are above the regional average for population in poverty or population of color:

(up to 60% of maximum score)

Project located in a census tract that is below the regional average for population in poverty or populations of color or includes children, people with disabilities, or the elderly:

(up to 40% of maximum score)

1.(0 to 3 points) A successful project is one that has actively engaged low-income populations, people of color, children, persons with disabilities, and the elderly during the project's development with the intent to limit negative impacts on them and, at the same time, provide the most benefits.

Describe how the project has encouraged or will engage the full cross-section of community in decision-making. Identify the communities to be engaged and where in the project development process engagement has occurred or will occur. Elements of quality engagement include: outreach to specific communities and populations that are likely to be directly impacted by the project; techniques to reach out to populations traditionally not involved in the community engagement related to transportation projects; residents or users identifying potential positive and negative elements of the project; and surveys, study recommendations, or plans that provide feedback from populations that may be impacted by the proposed project. If relevant, describe how NEPA or Title VI regulations will guide engagement activities.

This bridge project provides a physical link between Lowertown and Dayton's Bluff, two independently-diverse and vibrant communities. Dayton's Bluff has a high proportion of historically disadvantaged, under-represented, or low-income populations. Lowertown, known for its artists quarters, has seen a recent upswing with development of CHS Field, Union Depot, and food/arts/entertainment.

The project will openly engage members of both communities. The City's consultant SRF will facilitate the public participation process with the help of local artist Seitu Jones. SRF and Mr. Jones have partnered on several past projects to deliver community-driven public art that enhances community and a sense of ownership in the project. Art is only one conduit for a larger connection that a bridge can offer to its users and neighbors. SRF will conduct public meetings, present alternatives and solicit community feedback. In addition to traditional flyers/mailings alternative options may include social media, and the City has had past success with hosting on-site "pop-up" meetings to capture interest and input of passersby, offering popsicles in exchange for participation.

Connectivity to Dayton's Bluff and its low-income populations is of special importance and will be a project focus. There is an opportunity to improve job access for individuals who rely on non-motorized or bus transit.

(Limit 1,400 characters; approximately 200 words)

2.(0 to 7 points) Describe the projects benefits to low-income populations, people of color, children, people with disabilities, and the elderly. Benefits could relate to safety; public health; access to destinations; travel time; gap closure; leveraging of other beneficial projects and investments; and/or community cohesion. Note that this is not an exhaustive list.

Response:

The low income population (which consists primarily of people of color) will benefit from the proposed upgraded shared-use off-street trail, which serves as a link between the east side of Saint Paul and the job concentration center on the west end of the bridge. The bridge will also serve as a direct route to the Union Depot transit facility, which serves multiple bus lines, Green Line LRT and Amtrack. In the future it will serve the Riverview, Red Rock, Robert Street, Gateway and Rushline Transit Corridors.

Response:

Elderly and disabled populations will benefit from the addition of ADA-accessible pedestrian curb ramps at all intersections. At signalized intersections, audible pedestrian signals will be included. Additionally, bikes and pedestrians could benefit from reconstruction of the Mounds Boulevard intersection, which may involve relocation of the off-street trail from the south side of the bridge to the north side, or other connectivity improvements that would eliminate the need for an at-grade crossing of Kellogg Boulevard to access downtown destinations at the other end of the bridge: Capital City Bikeway, CHS Field and Lowertown businesses.

(Limit 2,800 characters; approximately 400 words)

3. (-3 to 0 points) Describe any negative externalities created by the project along with measures that will be taken to mitigate them. Negative externalities can result in a reduction in points, but mitigation of externalities can offset reductions.

Below is a list of negative impacts. Note that this is not an exhaustive list.

Increased difficulty in street crossing caused by increased roadway width, increased traffic speed, wider turning radii, or other elements that negatively impact pedestrian access.

Increased noise.

Decreased pedestrian access through sidewalk removal / narrowing, placement of barriers along the walking path, increase in auto-oriented curb cuts, etc.

Project elements that are detrimental to location-based air quality by increasing stop/start activity at intersections, creating vehicle idling areas, directing an increased number of vehicles to a particular point, etc.

Increased speed and/or cut-through traffic.

Removed or diminished safe bicycle access.

Inclusion of some other barrier to access to jobs and other destinations.

Displacement of residents and businesses.

Construction/implementation impacts such as dust; noise; reduced access for travelers and to businesses; disruption of utilities; and eliminated street crossings. These tend to be temporary.

Other

Response:

During construction, ped/bike and bus facilities will be negatively impacted. These impacts are largely with respect to travel times or access, and will be mitigated by temporarily relocating bus service to other non-impacted streets and by providing a fully ADA-compliant Temporary Pedestrian Access Route (TPAR). Temporary construction impacts such as noise or dust are anticipated to be low, as the bridge has few neighbors and other existing facilities such as the freeway and railways produce similar inconveniences during normal operation.

Once completed, this project will have no negative impacts on low-income populations, people of color, children, people with disabilities, or the elderly. No residents or businesses will be displaced.

(Limit 2,800 characters; approximately 400 words)

Upload Map

1530644386092_kellogg-third-socioeconomic-map 2018-07-03.pdf

Measure B: Affordable Housing

City	Segment Length (For stand-alone projects, enter population from Regional Economy map) within each City/Township	Segment Length/Total Project Length	Score	Housing Score Multiplied by Segment percent
St. Paul	25737.0	1.0	100.0	100.0

Total Project Length

Total Project Length (as entered in the "Project Information" form) 0.7

Affordable Housing Scoring

Total Project Length (Miles) or Population 25737.0

Total Housing Score	100.0
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Affordable Housing Scoring

Measure A: Bridge Condition

Bridge Sufficiency Rating	36.7
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Upload Structure Inventory Report	1530817522686_2017-Routine-Inspection-62080.pdf
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Please upload attachment in PDF form.

Measure B: Load-Posting

Load Posted (Check box if the bridge is load-posted):	Yes
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Measure A: Multimodal Elements and Existing Connections

Multimodal elements included as part of the project include new/improved bike and ped facilities, support infrastructure for bus rapid transit, and at-grade intersection improvements.

- 20%-30% of the overall bridge width will support a combined bike/ped facility.

- Possible new bike/ped segments include sidewalk on the west side of Mounds Blvd, trail spur connection(s) to Union Depot and CHS Field/Prince St.

- Support infrastructure for bus rapid transit includes modification of the approach at Kellogg/Mounds to accommodate a boarding station on Mounds Blvd, and designated signal phasing for buses to turn onto Kellogg Blvd bridge en route to Union Depot.

- At-grade intersection improvements include ADA upgrades to signals and curb ramps, primarily at major intersections at Mounds Blvd and Lafayette St, with other improvements also anticipated at minor intersections at John St and Union Depot entrances.

- All facilities will be designed in accordance with state and local requirements and best practices, including guidance manuals on the development of bikeways.

The Kellogg-Third bridge provides a direct connection for pedestrians and bicycles between Dayton's Bluff and downtown Saint Paul. Kellogg Blvd connects to the high pedestrian traffic areas around the Union Depot, the Green Line LRT, and the bustling Lowertown neighborhood. The Lowertown Master Plan identifies that the proposed bridge will connect East Side residents to the food/arts cluster near the Farmers Market and an entertainment district near Mears Park. The bridge route is an identified Planned Regional Bikeway in the Regional Bicycle Transportation Network

Response:

(RBTN) and connects to other RTBN trails, such as the Bruce Vento RT, Indian Mounds Park RT, and the Sam Morgan RT. The bridge will also improve connections to the Gateway State Trail and newly-constructed Capital City Bikeway, a network that is eventually planned to loop throughout downtown. The bridge is further identified in the City's draft Bicycle Plan, adopted in March 2015.

The proposed bridge will restore the existing substandard walk that was reduced to a 6' combined use trail when the City was forced to remove all traffic from the pier cantilevers. The existing vehicular lanes consist of one 11' outbound and two 11' inbound lanes. The vehicular shoulders are presently a substandard 1.75'. Mass transit and coach bus traffic use the bridge's common vehicular lanes, reducing travel time reliability.

The proposed bridge will include a functional shared-use trail facility (the preliminary layout identifies 12' barrier-protected sidewalks on each side of the bridge), four 12' vehicular lanes, and compliant-width shoulders, which will provide added safety to bicyclists that choose to travel in the vehicular area.

(Limit 2,800 characters; approximately 400 words)

Transit Projects Not Requiring Construction

If the applicant is completing a transit application that is operations only, check the box and do not complete the remainder of the form. These projects will receive full points for the Risk Assessment.

Park-and-Ride and other transit construction projects require completion of the Risk Assessment below.

[Check Here if Your Transit Project Does Not Require Construction](#)

Measure A: Risk Assessment - Construction Projects

1)Layout (30 Percent of Points)

Layout should include proposed geometrics and existing and proposed right-of-way boundaries.

Layout approved by the applicant and all impacted jurisdictions (i.e., cities/counties that the project goes through or agencies that maintain the roadway(s)). A PDF of the layout must be attached along with letters from each jurisdiction to receive points.

100%

Attach Layout

Please upload attachment in PDF form.

Layout completed but not approved by all jurisdictions. A PDF of the layout must be attached to receive points. Yes

50%

Attach Layout

1530821714686_prelim-layout-srf.pdf

Please upload attachment in PDF form.

Layout has not been started

0%

Anticipated date or date of completion 02/15/2019

2)Review of Section 106 Historic Resources (20 Percent of Points)

No known historic properties eligible for or listed in the National Register of Historic Places are located in the project area, and project is not located on an identified historic bridge

100%

There are historical/archeological properties present but determination of no historic properties affected is anticipated. Yes

100%

Historic/archeological property impacted; determination of no adverse effect anticipated

80%

Historic/archeological property impacted; determination of adverse effect anticipated

40%

Unsure if there are any historic/archaeological properties in the project area.

0%

Project is located on an identified historic bridge

3)Right-of-Way (30 Percent of Points)

Right-of-way, permanent or temporary easements either not required or all have been acquired

100%

Right-of-way, permanent or temporary easements required, plat, legal descriptions, or official map complete

50%

Right-of-way, permanent or temporary easements required, parcels identified Yes

25%

Right-of-way, permanent or temporary easements required,
parcels not all identified

0%

Anticipated date or date of acquisition

11/15/2019

4)Railroad Involvement (20 Percent of Points)

No railroad involvement on project or railroad Right-of-Way
agreement is executed (include signature page, if applicable)

100%

Signature Page

Please upload attachment in PDF form.

Railroad Right-of-Way Agreement required; negotiations have
begun

Yes

50%

Railroad Right-of-Way Agreement required; negotiations have not
begun.

0%

Anticipated date or date of executed Agreement

11/15/2019

Measure A: Cost Effectiveness

Total Project Cost (entered in Project Cost Form):

\$54,145,000.00

Enter Amount of the Noise Walls:

\$0.00

Total Project Cost subtract the amount of the noise walls:

\$54,145,000.00

Points Awarded in Previous Criteria

Cost Effectiveness

\$0.00

Other Attachments

File Name	Description	File Size
1 One-page-project-summary.pdf	One-page project summary.	687 KB
2 Before-photos-with-captions.pdf	Before photographs, with captions	12.9 MB
2 Before-photos-with-captions.pdf	Before photographs, with captions	12.9 MB
3 Prelim-plans-including-modal-layouts.pdf	Preliminary plans and layouts, including geometry and cross sections showing bicycle/pedestrian/transit components	9.6 MB
4 Project-Area-Maps.pdf	Project area maps, state-wide and local level	270 KB
5 MetC-mapping-application.pdf	Project information maps, generated through the Metropolitan Council Make-A-Map web-based application	17.5 MB
6 City-resolution-RES-18-803.pdf	City Council resolution, showing agency commitment to projects submitted for federal funding in the 2018 regional solicitation program	65 KB
7 City-resoltion-RES-14-2129 5yr.pdf	City Council resolution, showing initial 2014 designation of the Kellogg-Third bridge on the city's 5-year bridge prioritization plan (bridge replacement remains city's top bonding priority)	129 KB
8 2018-notification-letters.pdf	Notification letters, sent to external agencies affected by proposed replacement of the Kellogg-Third bridge	588 KB

Regional Economy

Bridges Project: Replacement of Kellogg/3rd Street Bridge | Map ID: 1530639399475

Results

WITHIN ONE MI of project:
Postsecondary Students: 8424

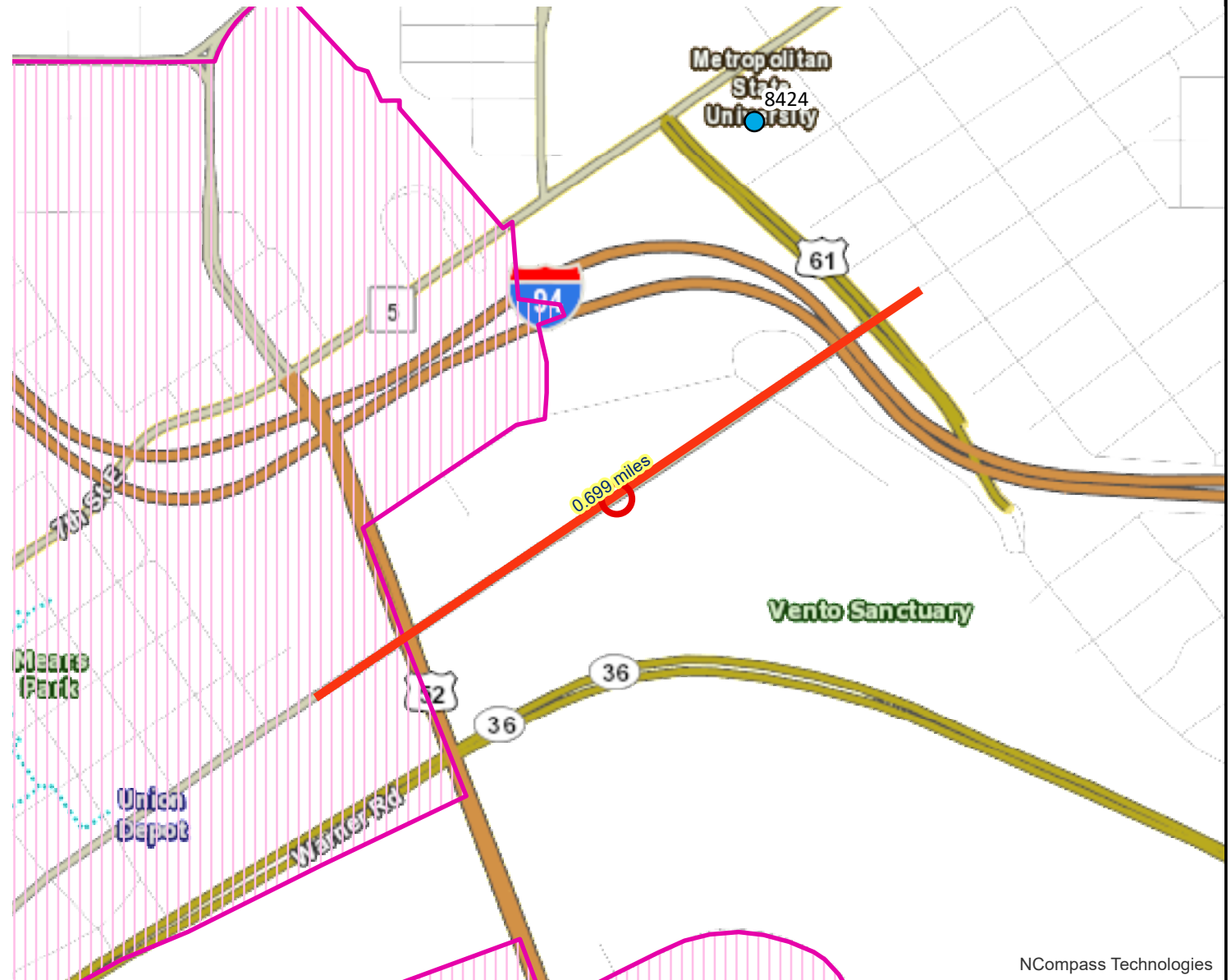
Totals by City:

St. Paul






Population: 25737

Employment: 70343

Mfg and Dist Employment: 3728



NCompass Technologies

-  Project Points
-  Postsecondary Education Centers
-  Job Concentration Centers
-  Project
-  Manufacturing/Distribution Centers

0 0.075 0.15 0.3 0.45 0.6 Miles

Created: 7/3/2018
LandscapeRSA5

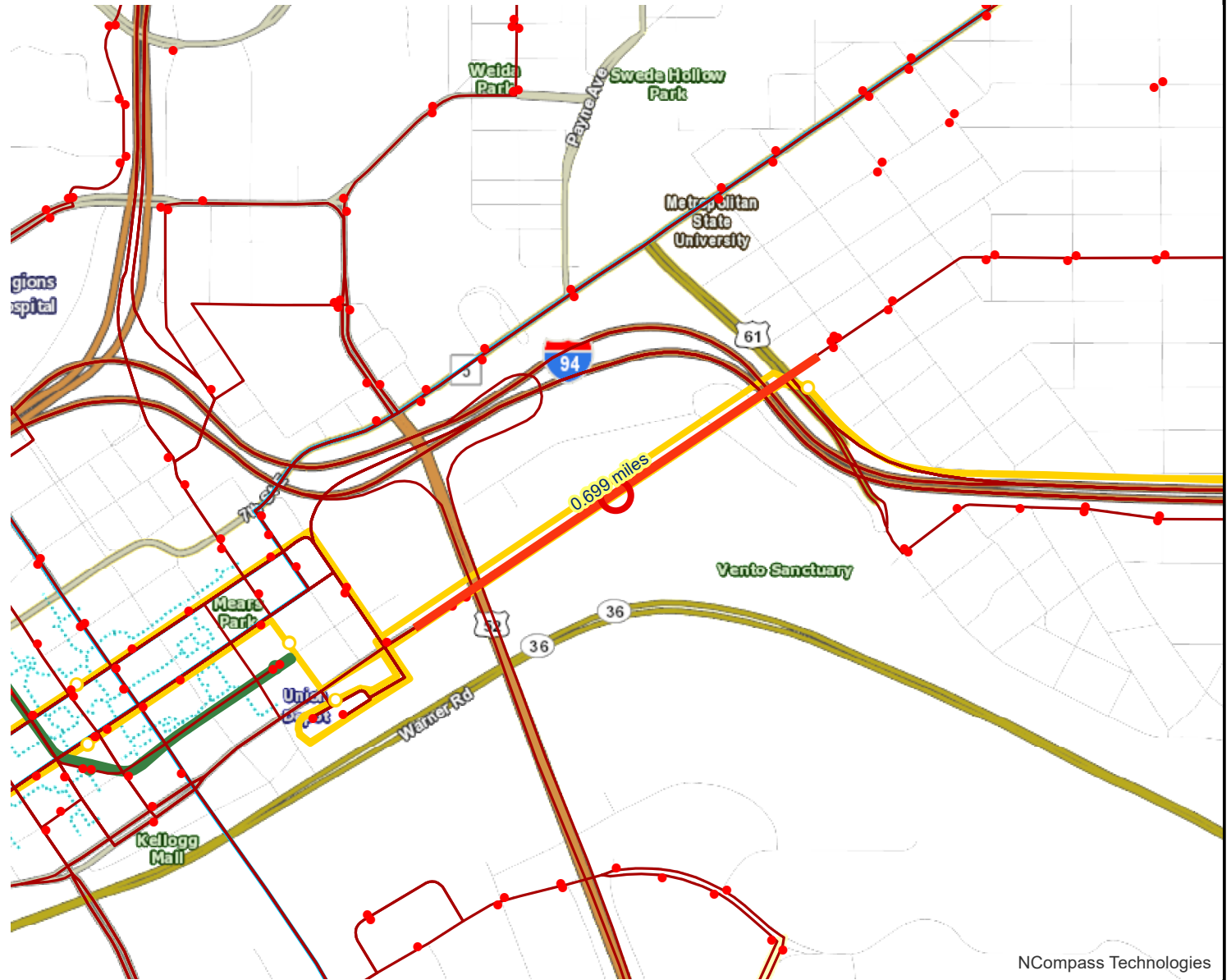


For complete disclaimer of accuracy, please visit
<http://giswebsite.metc.state.mn.us/gissitenew/notice.aspx>



Transit Connections

Bridges Project: Replacement of Kellogg/3rd Street Bridge | Map ID: 1530639399475



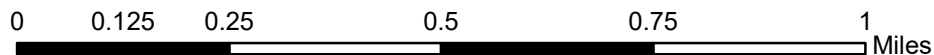
Results

Transit with a Direct Connection to project:
 16 21 262 294 3 350 351 353 355 361 364
 365 375 417 452 480 484 489 54 63 70 94

*Gold Line

*Gold Line

*indicates Planned Alignments



Created: 7/3/2018
 LandscapeRSA3



For complete disclaimer of accuracy, please visit
<http://giswebsite.metc.state.mn.us/gissitenew/notice.aspx>



NCompass Technologies

Socio-Economic Conditions

Bridges Project: Replacement of Kellogg/3rd Street Bridge | Map ID: 1530639399475

Results

Project located **IN**
Area of Concentrated Poverty
with 50% or more of residents
are people of color (ACP50):
(0 to 30 Points)



NCompass Technologies



Project Points



Project



Area of Concentrated Poverty > 50% residents of color



Area of Concentrated Poverty



Above reg'l avg conc of race/poverty

0 0.075 0.15 0.3 0.45 0.6 Miles

Created: 7/3/2018
LandscapeRSA2



For complete disclaimer of accuracy, please visit
<http://giswebsite.metc.state.mn.us/gissitenew/notice.aspx>



2017 ROUTINE BRIDGE INSPECTION REPORT



BRIDGE # 62080 KELLOGG Blvd over RR; I 94; Comm Fox St

DISTRICT: Metro

COUNTY: Ramsey

CITY/TOWNSHIP: St Paul

STATE: Minnesota

Date of Inspection: 07/12/2017

Equipment Used: Other

Owner: City or Municipal Highway Agency

Inspected By: Engel, Michael; Sanders, Rick; Schaaf, Jerry

Report Written By: Jerry Schaaf

Report Reviewed By: Glenn Pagel

Final Report Date: 12/18/2017

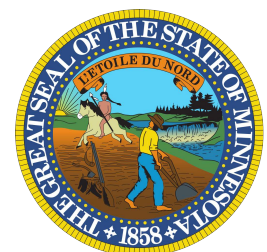


Table of Contents

<u>SECTION</u>	<u>PAGE</u>
COVER	1
STRUCTURE INVENTORY	2
ELEMENTS	4
PICTURES - THUMBNAILS	9

Minnesota Structure Inventory Report

Bridge ID: 62080

KELLOGG Blvd over RR; I 94; Comm Fox St

Date: 07/12/2017

+ G E N E R A L +				+ R O A D W A Y +				+ I N S P E C T I O N +				
Agency Br. No.	Crew	7639		Bridge Match ID (TIS)	1			Userkey	199			
District	05	Maint. Area	5B	Roadway O/U Key	Route On Structure			Structurally Deficient	Y			
County	062 - Ramsey			Route Sys	05 - MSAS	Number 158		Functionally Obsolete	N			
City	St Paul			Roadway Name or Description				Sufficiency Rating	36.7			
Township				Kellogg Blvd (MSAS 158)				Routine Inspection Date	07/12/2017			
Desc. Loc.	0.5 MI E OF JCT TH 52			Level of Service	1 - MAINLINE			Routine Inspection Frequency	12			
Sect., Twp., Range	32 - 029N - 22W			Roadway Type	2 - 2-way traffic			Inspector Name	Schaaf, Jerry			
Latitude	44 ° 57 ' 07.10 "			Control Section (TH Only)				Status	A - Open			
Longitude	-93 ° 04 ' 35.89 "			Reference Point				+ N B I C O N D I T I O N R A T I N G S +				
Custodian	04 - City or Municipal Highway			Detour Length	1.0	mi.		Deck	6	Unsound Deck %		
Owner	04 - City or Municipal Highway			Lanes	ON 4	UNDER 6		Superstructure	6			
BMU Agreement				ADT	10300	YEAR	2005	Substructure	3			
Year Built	1982			HCA DT	ADTT %			Channel	N			
MN Year Reconstructed				Functional Class	16 - Urban - Minor Arterial			Culvert	N			
FHWA Year Reconstructed				+ R D W Y D I M E N S I O N S +				+ N B I A P P R A I S A L R A T I N G S +				
MN Temporary Status				If Divided	NB-EB		SB-WB	Structure Evaluation	3			
Bridge Plan Location	1 - CENTRAL			Roadway Width	54.80	ft.	ft.	Deck Geometry	5			
Date Opened to Traffic	9/1/1983			Vertical Clearance		ft.	ft.	Underclearances	9			
On - Off System	1 - ON			Max. Vert. Clear.		ft.	ft.	Waterway Adequacy	N			
Legislative District	65B			Horizontal Clear.	54.7	ft.	ft.	Approach Alignment	8			
Potential ABC	2 - N/A			Lateral Clearance		ft.	ft.	+ S A F E T Y F E A T U R E S +				
+ S T R U C T U R E +				Appr. Surface Width	48.0	ft.		Bridge Railing	1 - MEETS STANDARDS			
Service On	5 - Highway-pedestrian			Bridge Roadway Width	54.8	ft.		GR Transition	N - NOT REQUIRED			
Service Under	4 - Highway - railroad			Median Width On Bridge	50.00	ft.		Appr. Guardrail	N - NOT REQUIRED			
Main Span Type	5 - Prestress or Precast			+ M I S C. B R I D G E D A T A +				GR Termini	N - NOT REQUIRED			
Main Span Design	01 - Beam Span			Structure Flared	1 - Flared			+ I N D E P T H I N S P. +				
Main Span Detail				Parallel Structure	N - No parallel structure			Y/N	Freq	Date		
Appr. Span Type				Field Conn. ID				Frac. Critical				
Appr. Span Design				Abutment Foundation (Material/Type)	1 - CONC 3 - FTG PILE			Underwater				
Appr. Span Detail				Pier Foundation (Material/Type)	1 - CONC 3 - FTG PILE			Pinned Asbly.				
Skew	0			Historic Status	5 - Not eligible			Spec. Feat.				
Culvert Type				+ P A I N T +				+ W A T E R W A Y +				
Barrel Length				Year Painted				Drainage Area (sq. mi.)				
Cantilever ID				Unsound Paint %				Waterway Opening (sf.)				
Number of Spans				Painted Area	sq. ft.			Navigation Control	N - Not applicable, no			
MAIN: 18	APPR: 0	TOTAL:		Primer Type				Pier Protection				
Main Span Length	109.0 ft.			Finish Type				Nav. Clr. (ft.)	Vert.	0.0	Horiz.	0.0
Structure Length	1914.0 ft.			+ B R I D G E S I G N S +				Nav. Vert. Lift Bridge Clear. (ft.)				
Deck Width (Out-to-Out)	69.3 ft.			Posted Load	2 - Vehicle & Semi (Type R12-5)			MN Scour Code	A - NON	Year		
Deck Material	1 - Concrete Cast-in-Place			Traffic	0 - Not Required			+ C A P A C I T Y R A T I N G S +				
Wear Surf Type	4 - Low Slump Concrete			Horizontal	0 - Not Required			Design Load	5 - HS 20			
Wear Surf Install Year	1983			Vertical	0 - Not Required			Operating Rating	2 - HS TRUCK	39.4		
Wear Course/Fill Depth	0.17 ft.							Inventory Rating	2 - HS TRUCK	23.6		
Deck Membrane	0 - None							Rating Date	09/04/2014			Posting VEH: 13
Deck Rebars	1 - Epoxy Coated Reinforcing							Overweight Permit Codes				
Deck Rebars Install Year	1983							A	N - N/A	B	N - N/A	C
Structure Area (Out-to-Out)	131129	sq. ft.										
Roadway Area (Curb-to-Curb)	104750	sq. ft.										
Sidewalk Width	50A. Lt 0.00 ft.	50B. Rt 10.30 ft.										
Curb Height	Lt 0.00 ft.	Rt 0.00 ft.										
Rail Type	Lt 28	Rt 22										

Minnesota Structure Inventory Report

Additional Roadways

Bridge ID: 62080

KELLOGG Blvd over RR; I 94; Comm Fox St

Date: 12/18/2017

ROADWAY		
Bridge Match ID (TIS):	3	
5A. Roadway On/Under:	A - UNDERRECORD A TYPE (IF MORE	
Bridge Route System:	5 - CITY STREET	
5D. Route Number:	926	
Roadway Name or Description		
FOX ROAD		
5C. Level of Service:	1 - MAINLINE	
102. Direction of Traffic:	2 - 2-way traffic	
Control Section (TH Only):		
Reference Point:		
19. Detour Length (mi):	1.0	
Lanes:	2	
29. ADT:	50	
30. Year:	1980	
26. Functional Class:	19	
Traffic Sequence Number:		
InterRegional Corridor (TH Only):		
ROADWAY DIMENSIONS		
	NB-EB	SB-WB
Roadway Width (ft):	22.0	
Vertical Clearance (ft):	54.9	
Max. Vert. Clear. (ft):	54.9	
Horizontal Clear. (ft):	94.9	
	Left	Right
Lateral Clearance (ft):		29.4
32. Appr. Roadway Width (ft):	22.0	
51. Brdg Roadway Width (ft):	22.0	
Median Width (ft):		
10. Vertical Clearance (ft):	54.9	
47. Horizontal Clearance	94.9	

ROADWAY		
Bridge Match ID (TIS):	4	
5A. Roadway On/Under:	2 - UNDERRECORD 2 TYPE (IF ONLY 1	
Bridge Route System:	5 - CITY STREET	
5D. Route Number:	1101	
Roadway Name or Description		
Frontage Road		
5C. Level of Service:	1 - MAINLINE	
102. Direction of Traffic:	2 - 2-way traffic	
Control Section (TH Only):		
Reference Point:		
19. Detour Length (mi):	1.0	
Lanes:	2	
29. ADT:	500	
30. Year:	1980	
26. Functional Class:	19	
Traffic Sequence Number:		
InterRegional Corridor (TH Only):		
ROADWAY DIMENSIONS		
	NB-EB	SB-WB
Roadway Width (ft):	28.00	
Vertical Clearance (ft):	34.9	
Max. Vert. Clear. (ft):	34.9	
Horizontal Clear. (ft):	95.9	
	Left	Right
Lateral Clearance (ft):		9.7
32. Appr. Roadway Width (ft): 28.0		
51. Brdg Roadway Width (ft): 28.0		
Median Width (ft):		
10. Vertical Clearance (ft):	34.9	
47. Horizontal Clearance	95.9	

ROADWAY		
Bridge Match ID (TIS):	2	
5A. Roadway On/Under:	2 - UNDERRECORD 2 TYPE (IF ONLY 1	
Bridge Route System:	5 - CITY STREET	
5D. Route Number:	924	
Roadway Name or Description		
Commercial St		
5C. Level of Service:	1 - MAINLINE	
102. Direction of Traffic:	2 - 2-way traffic	
Control Section (TH Only):		
Reference Point:		
19. Detour Length (mi):	1.0	
Lanes:	2	
29. ADT:	250	
30. Year:	1980	
26. Functional Class:	19	
Traffic Sequence Number:		
InterRegional Corridor (TH Only):		
ROADWAY DIMENSIONS		
	NB-EB	SB-WB
Roadway Width (ft):	32.70	
Vertical Clearance (ft):	51.9	
Max. Vert. Clear. (ft):	51.9	
Horizontal Clear. (ft):	59.9	
	Left	Right
Lateral Clearance (ft):		15.4
32. Appr. Roadway Width (ft):	32.0	
51. Brdg Roadway Width (ft):	32.7	
Median Width (ft):		
10. Vertical Clearance (ft):	51.9	
47. Horizontal Clearance	59.9	

12/18/2017

County:	Ramsey	Location:	0.5 MI E OF JCT TH 52		Length:	1914.0 ft.	
City:	St Paul	Route:	05 - MSAS 158	Ref. Pt.: 002+00.103	Deck Width:	69.3 ft.	
Township:		Control Section:			Rdwy. Area/ Pct. Unsnd:	104750 sq. ft. / %	
Section: 32	Township: 029N	Range: 22W	Maint. Area: 5B		Paint Area/ Pct. Unsnd:	sq. ft. / %	
Span Type: 5 - Prestressed Concrete 2 -			Local Agency Bridge Nbr.:		Culvert:	N/A	
List:	Stringer/Multi-beam or Girder				Postings:	13	30
NBI Deck: 6	Super: 6	Sub: 3	Chan: N	Culv: N			
				Open, Posted, Closed:	A - Open		
				MN Scour Code:	A - NON WATERWAY		
Appraisal Ratings - Approach:		8	Waterway:	N	Unofficial Structurally Deficient	Y	
Required Bridge Signs - Load Posting:		2 - Vehicle & Semi (Type R12-5)	Traffic:	0 - Not Required	Unofficial Functionally Obsolete	N	
		Horizontal: 0 - Not Required	Vertical:	0 - Not Required	Unofficial Sufficiency Rating	36.7	

ELEM NBR	ELEMENT NAME	REPORT TYPE	INSP. DATE	QUANTITY	QTY CS 1	QTY CS 2	QTY CS 3	QTY CS 4
12	Reinforced Concrete Deck	Routine	07/12/2017	131129 SF	128506	0	2623	0
		Update	02/28/2017	131129 SF	128506	0	2623	0
	Notes: 1 SF OF DELAM AT PIER 3. 2016-17 20 SF OF DELAM AT PIER 7 2016-17 2 SF of delam at pier 4. 2016-17 6 SF of delam at pier 5. West side of gland. 2016-17 8 SF of delam at pier 5. East side of gland. 2016-17							
	510 - Wearing Surfaces	Routine	07/12/2017	104750 SF	102655	0	2095	0
		Update	02/28/2017	104750 SF	102655	0	2095	0
	Notes: Low Slump Overlay with Epoxy Rebar Notes: [1996] 8 SF PATCH N GUTTER PIER 3. 10 SF PATCH N GUTTER PIER 9. PIER 19 - 3 sf SPALL. 2015 Unsealed cracks on deck. 2017							
	109	Prestressed Concrete Open Girder/Beam	Routine	07/12/2017	13829 LF	13691	138	0
Update			02/28/2017	13829 LF	13691	138	0	0
Notes: 9) 72" Deep prestressed beams. [1985] Concrete beams stained by Corten steel diaphragms. [1997] BEAM 5 AT WEST END OF SPAN 2 HAS A 2 LF CRACK AT BOTTOM FLANGE. Most of the crack is at the edge of the sole plate. 2005 Minor cracks at a few of the beam ends. 2013-17								
205		Reinforced Concrete Column	Routine	07/12/2017	34 EA	28	6	0
	Update		02/28/2017	34 EA	28	6	0	0
	Notes: Staining at odd # piers from glands leaking. 2011-17 Pier 3 - N. side column - 3' crack. 2015-17 Pier 5 - S. side column - 20' of cracking with corner spalls. 2015-17 - also, rust staining and exposed rebar. 2016-17 Pier 6 - N. side column - insignificant impact damage at RR service road. 2015-17 Pier 9 - N. side column - 4 SF of spalls. S. side column - crack with spall. 2015-17 - also, 3 SF delam, N. side. 2016-17 Pier 13 - Aggregate pop outs present. - SE. side column - 3' crack. 2015-17 Pier 15 - S. side column - 4 SF of spalls. 2015-17							
215	Reinforced Concrete Abutment	Routine	07/12/2017	169 LF	169	0	0	0
		Update	02/28/2017	169 LF	169	0	0	0
Notes: Crack at W. side abut. - S. end. 2011 50 LF light cracks in West abut wing walls. 30 LF light cracks in East abut wing walls. [1995] NE WING WALL HAS 30" CRACKS, OVERHANG HAS 1 SF SPALL. MINOR IMPACT DAMAGE ALONG SW WING WALL. 2005								

ELEM NBR	ELEMENT NAME	REPORT TYPE	INSP. DATE	QUANTITY	QTY CS 1	QTY CS 2	QTY CS 3	QTY CS 4
36B.	Transitions NBI:	Posted speed does not exceed 40 MPH. 2014						
		Bridge roadway is the same width as the approach roadway. 2014						
36C.	Appr Guardrail NBI:							
36D.	Appr Guardrail							
	Terminal NBI:							
59.	Superstructure NBI:	Minor to moderate isolated non-structural cracking present. 2014-2015 (beam ends and sole plates)						
60.	Substructure NBI:	Concrete shear cracking present at pier cap cantilevers. 2011-16						
		Extensive delamination and spalling of pier # 5. 2011-16						
		Severe spalling, delamination and cracking present at the odd # piers. 2014-16						
		Odd # piers have stripseals on them. 2014						
61.	Channel NBI:							
62.	Culvert NBI:							
71.	Waterway Adeq NBI:							
72.	Appr Roadway							
	Alignment NBI:							

Jerry Schaaf

Inspector's Signature

Glenn Pagel

Reviewer's Signature



1. P1-NE.JPG



2. P1-SE.JPG



3. P3-NE.JPG



4. P3-NW.JPG



5. P3-SE.JPG



6. P3-SW.JPG



7. P5-NE_1.JPG



8. P5-NE_2.JPG



9. P5-NW.JPG



10. P5-SE.JPG



11. P5-SW_1.JPG



12. P5-SW_2.JPG



13. P7-NE.JPG



14. P7-NW.JPG



15. P7-SE_1.JPG



16. P7-SE_2.JPG



17. P7-SE_3.JPG



18. P7-SW.JPG



19. P9-NE_1.JPG



20. P9-NE_2.JPG



21. P9-NW.JPG



22. P9-SE.JPG



23. P9-SW.JPG



24. P11-NE.JPG



25. P11-NW.JPG



26. P11-SE.JPG



27. P11-SW.JPG



28. P13-NE.JPG



29. P13-NW.JPG



30. P13-SE.JPG



31. P13-SW.JPG



32. P15-NE.JPG



33. P15-NW.JPG



34. P15-SE.JPG



35. P15-SW.JPG



36. P17-NE.JPG



37. P17-NW.JPG



38. P17-SE.JPG

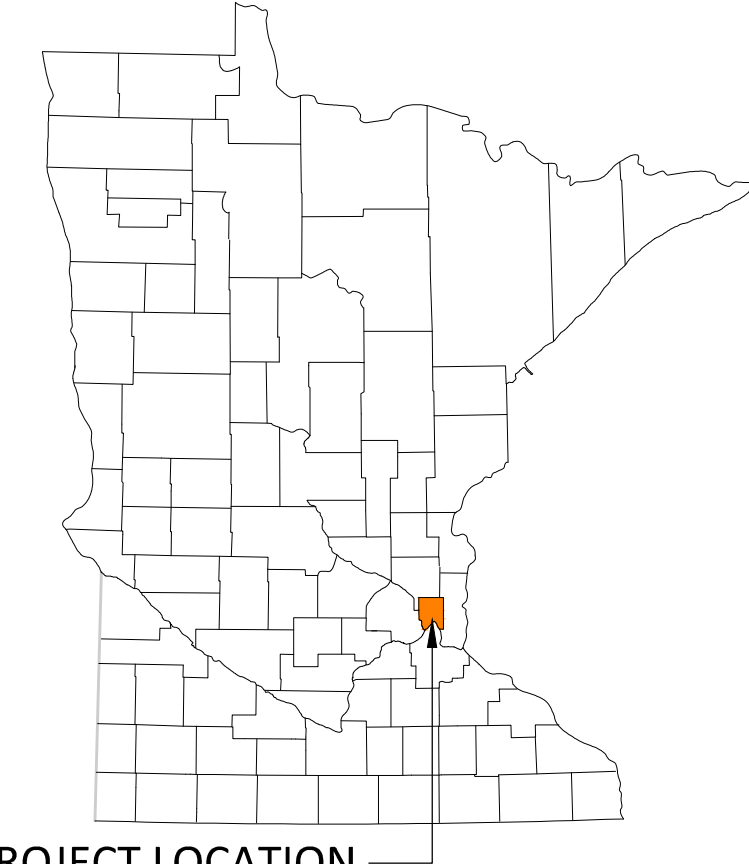


39. P17-SW.JPG

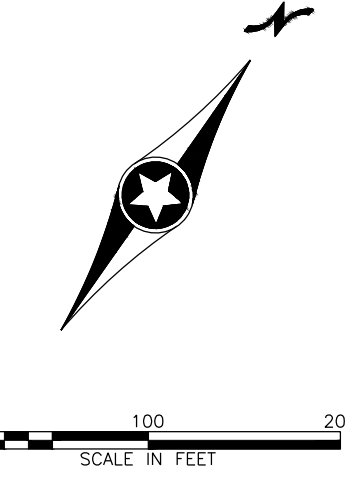
LAYOUT HISTORY		
NUMBER	DATE	DESCRIPTION
①	DATE	
②	DATE	
③	DATE	
④	DATE	

PRELIMINARY
SUBJECT TO CHANGE
10/12/2015
CSN CONSULTING GROUP, INC.

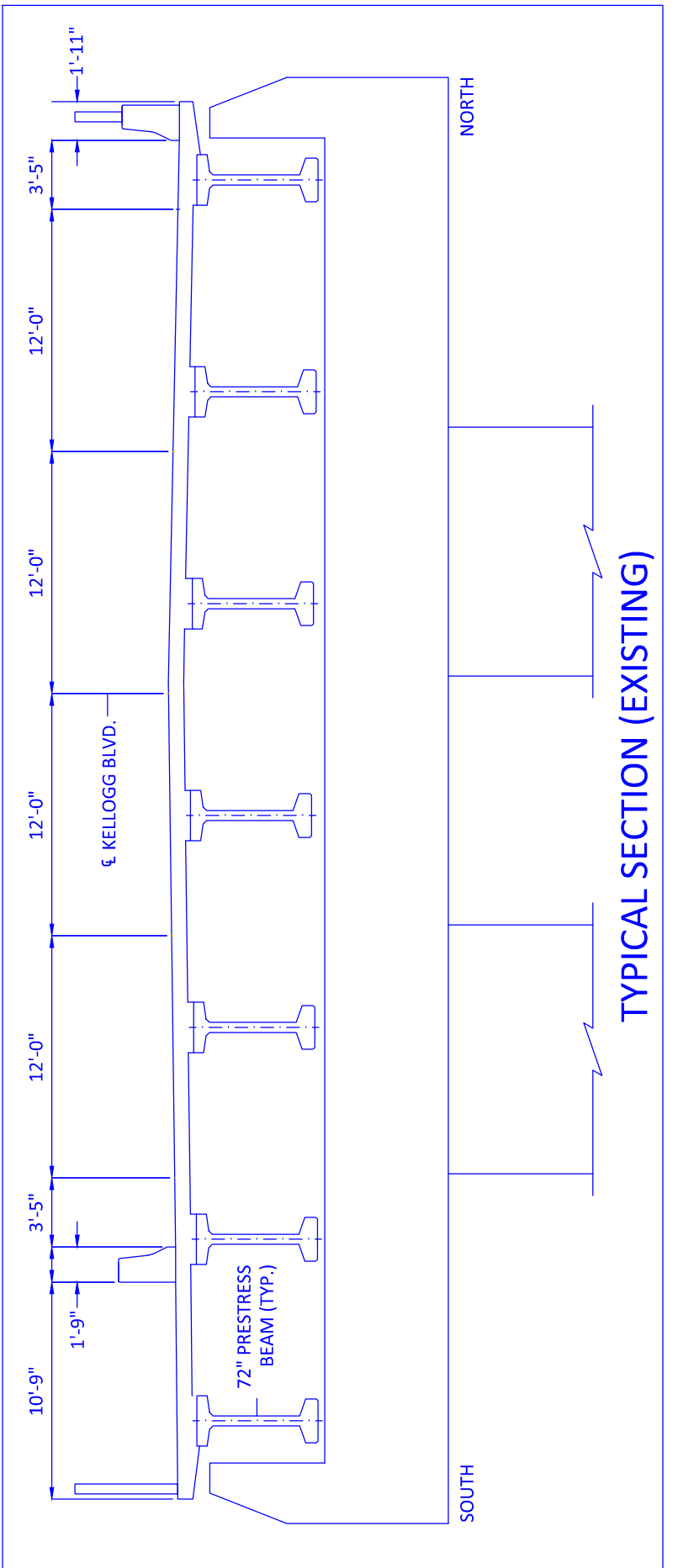
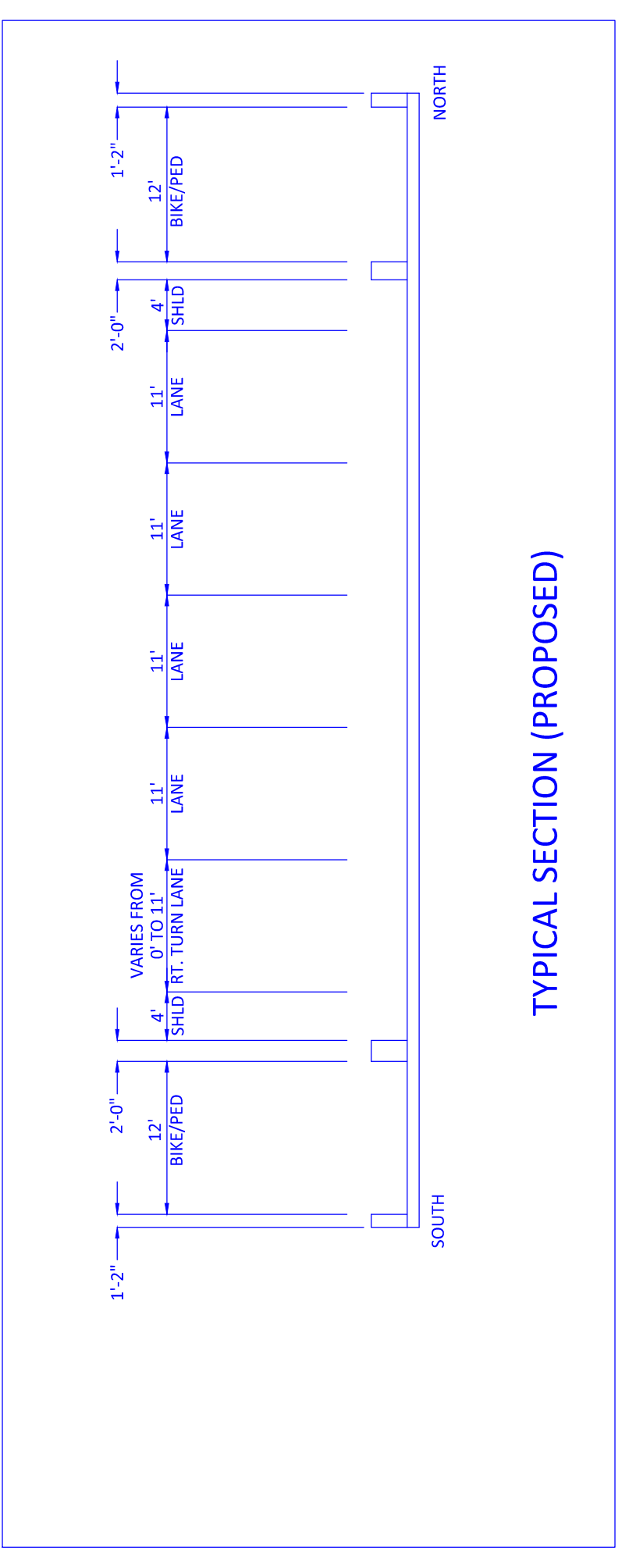
- LEGEND
- PAVED ROADWAY
 - BRIDGE & RETAINING WALLS
 - RAISED MEDIANS & CURBS
 - WALKS / TRAILS
 - EXISTING PONDING LOCATION
 - FUTURE CONSTRUCTION "BY OTHERS"
 - WETLAND LIMITS
 - EXISTING TRAFFIC SIGNAL
 - PROPOSED TRAFFIC SIGNAL TO BE REPLACED OR REVISED
 - EXISTING RAMP METER
 - EXISTING RIGHT OF WAY



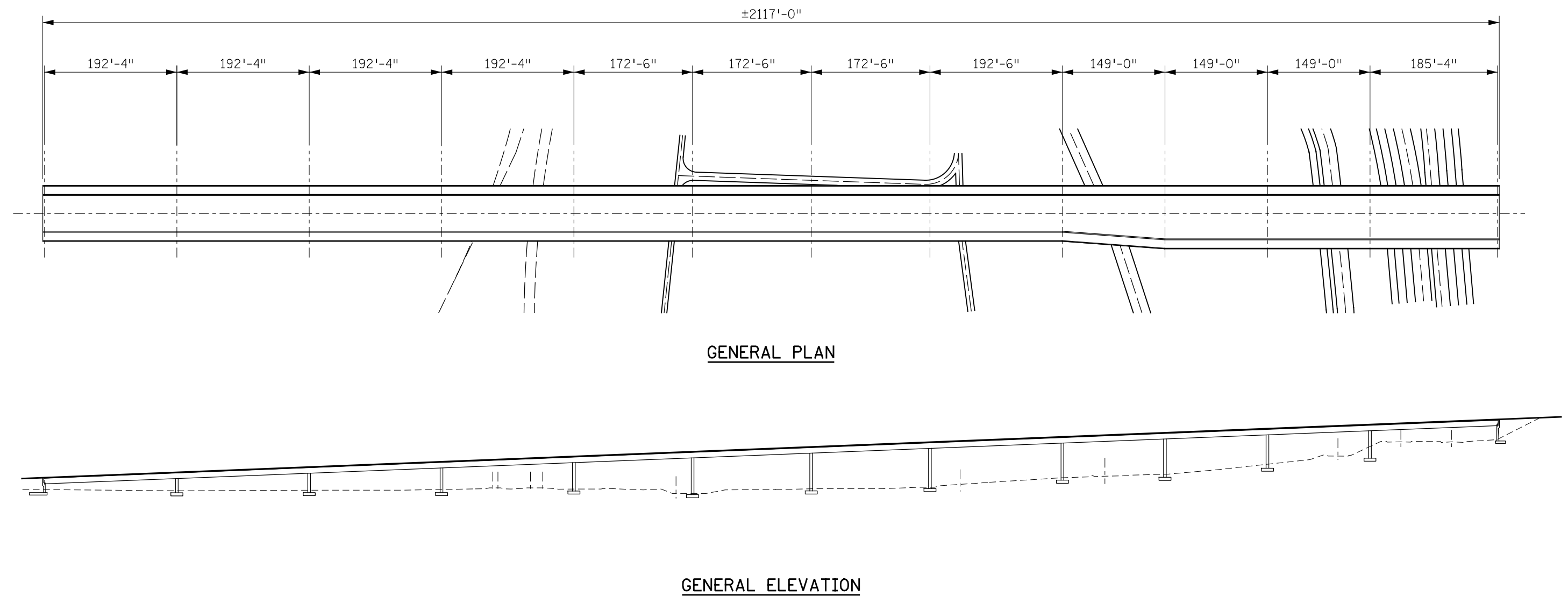
PROJECT LOCATION
COUNTY: RAMSEY
DISTRICT: METRO



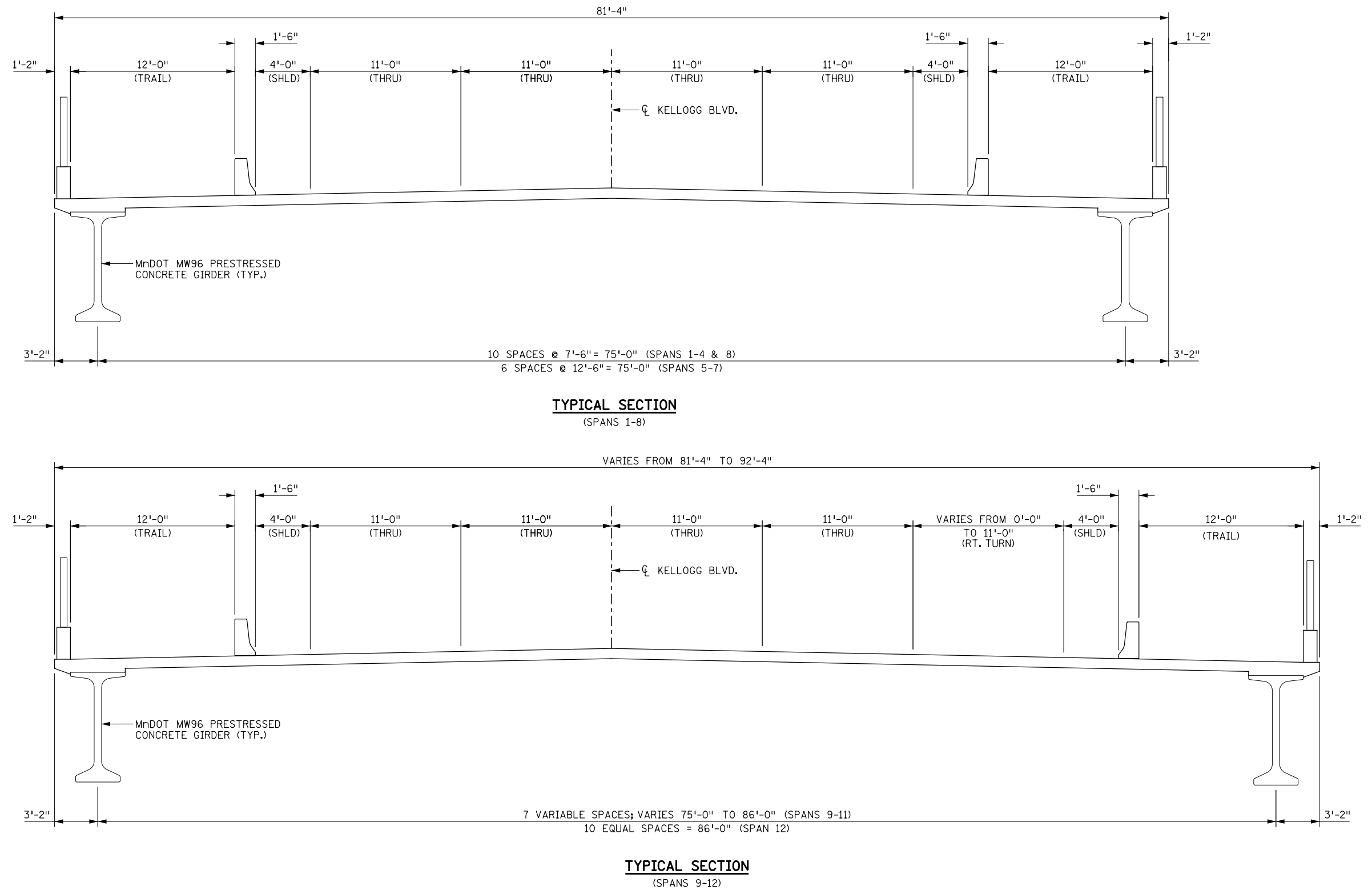
CONCEPTUAL LAYOUT

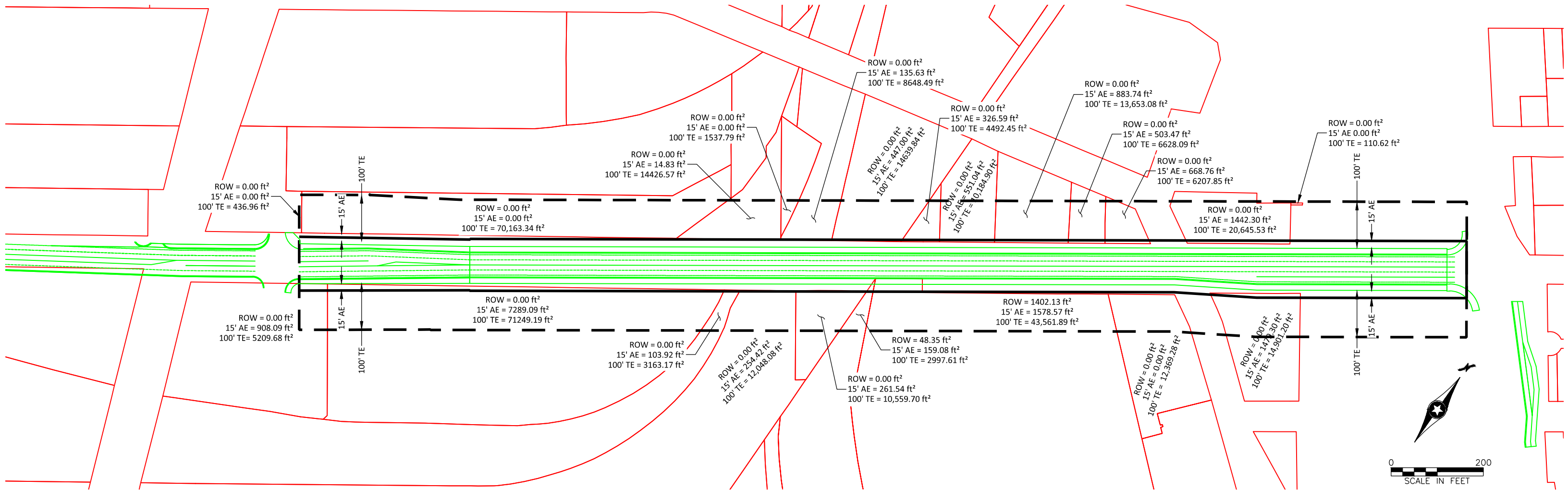


H:\Projects\8699\BR\Misc\8699_KelloggPrelimEst_151004Option.dgn



H:\Projects\8699\BR\Misc\8699_KelloggPrelimEst_151004Option.dgn

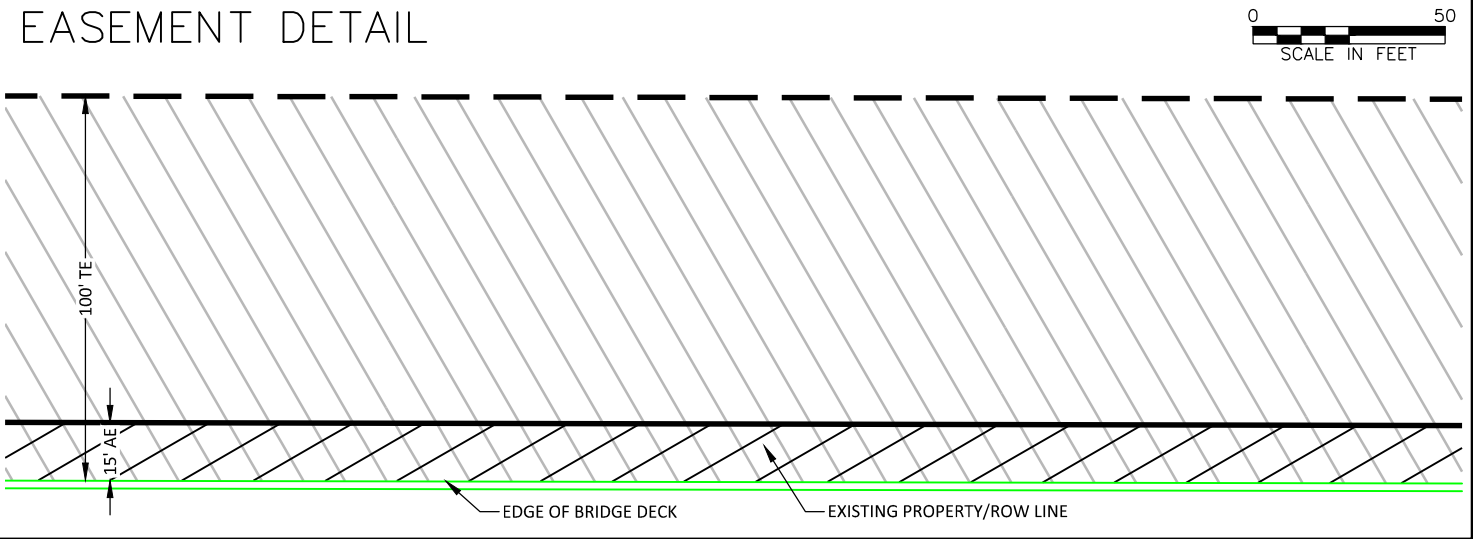




LEGEND

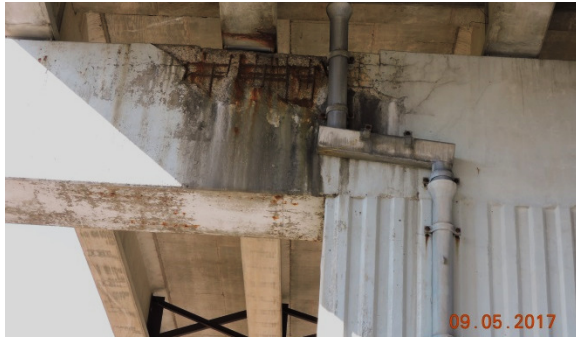
ROW	AREA FROM EDGE OF DECK TO EXISTING PROPERTY LINE
15' AE	(ACCESS EASEMENT) AREA FROM EDGE OF DECK TO 15' OFFSET (ASSUMES FOR BRIDGE MAINTENANCE/INSPECTION ACCESS)
100' TE	(TEMPORARY EASEMENT) 100' TEMPORARY CONSTRUCTION EASEMENT

EASEMENT DETAIL



PROJECT SUMMARY

Project Name: Replacement of Kellogg/3rd Street Bridge No. 62080 and 62080A
Applicant: Brent Christensen, applying on behalf of Saint Paul Public Works
Route: MSAS 158 (Kellogg Boulevard – Third Street)
Township/City/County: City of Saint Paul, MN
Requested Award Amount: \$7,000,000
Total Project Cost: \$63,903,000



*Photo 1. Cantilever pier cap cracking & deterioration.
(no traffic loads can be supported by cantilevers)*



*Photo 2. Outbound traffic backup across bridge
(photo taken west of bridge, with US Hwy 52. above)*

Project Description: This project is to reconstruct Kellogg Boulevard / Third Street retaining walls, approach roadway and Bridge Nos. 62080 and 62080A over Ramsey County Regional Rail Authority (RCRA), BNSF Railway, Bruce Vento Nature Sanctuary, Commercial Street, and Minnesota Department of Transportation (MnDOT) Trunk Highway I-94.

List of Project Benefits: Improved bike/ped/ADA facilities, access between job centers and under-represented populations, road and intersection safety improvements, mass transit accommodations, improved traffic level of service, and restoration of bridge capacity, serviceability, and functionality as major downtown route and freeway access connection.

Other Pertinent Information:

In 2014 a structural evaluation determined that the deteriorated pier cap cantilevers could not support any live load under the current MnDOT-approved analysis method. The bridge was temporarily closed to allow for installation of concrete barriers that restrict all modes of traffic to the center portion of the pier caps. The bridge reopened as a reconfigured three vehicular lane bridge (two inbound and one outbound) with substandard 1.75' shoulders and a substandard 6 foot bicycle/pedestrian walk.

The City has started the design of the new bridge using local funds and is actively pursuing State legislative funding assistance.

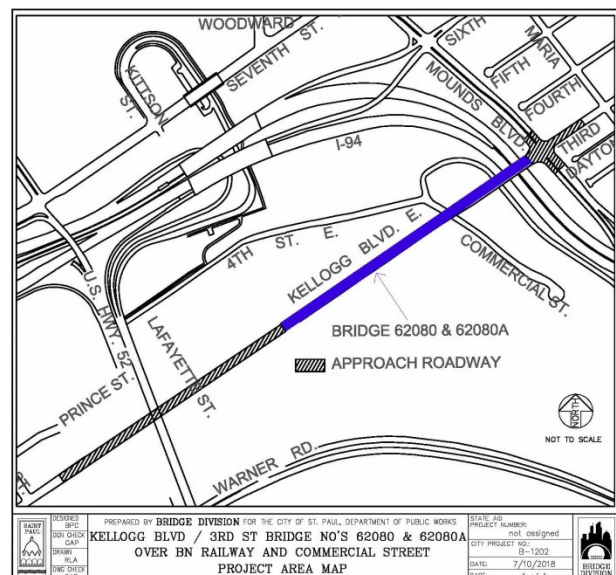




Figure 1. General Elevation View, West Portion of Bridge No. 62080



Figure 2. General Elevation View, East Portion of Bridge No. 62080A (over freeway)



07/11/2017

Figure 3. Deterioration of cantilever bridge pier caps. Even without deterioration, cantilever design capacity is insufficient to support traffic.



Figure 4. Reconfigured bridge deck, prohibiting loading upon cantilevered portion.



Figure 5. Reduced and narrow lanes contribute to increased incidents and maintenance.



Figure 6. Reduced lanes result in decreased reliability and increased traffic delay.



Figure 1. General Elevation View, West Portion of Bridge No. 62080



Figure 2. General Elevation View, East Portion of Bridge No. 62080A (over freeway)



07/11/2017

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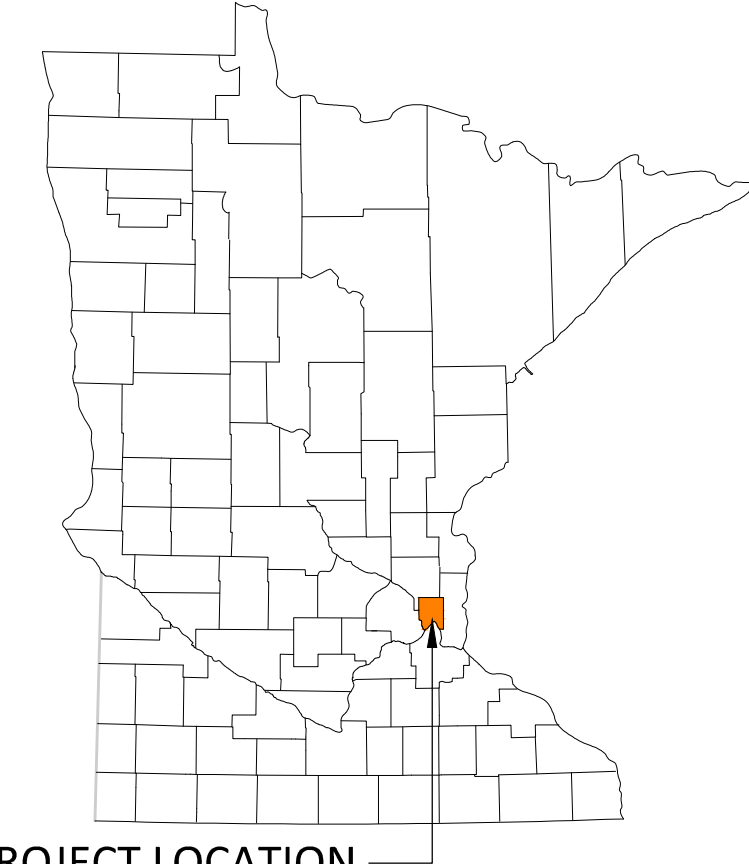


Figure 6. Reduced lanes result in decreased reliability and increased traffic delay.

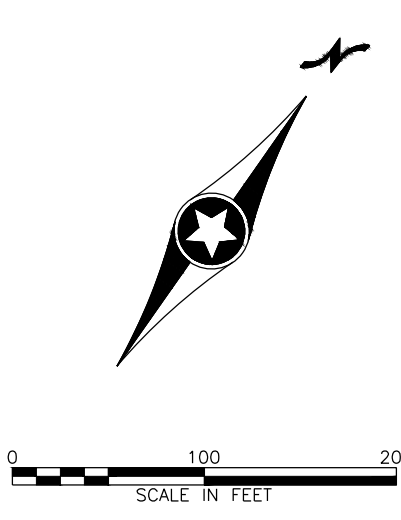
LAYOUT HISTORY		
NUMBER	DATE	DESCRIPTION
①	DATE	
②	DATE	
③	DATE	
④	DATE	

PRELIMINARY
SUBJECT TO CHANGE
10/12/2015
CSN CONSULTING GROUP, INC.

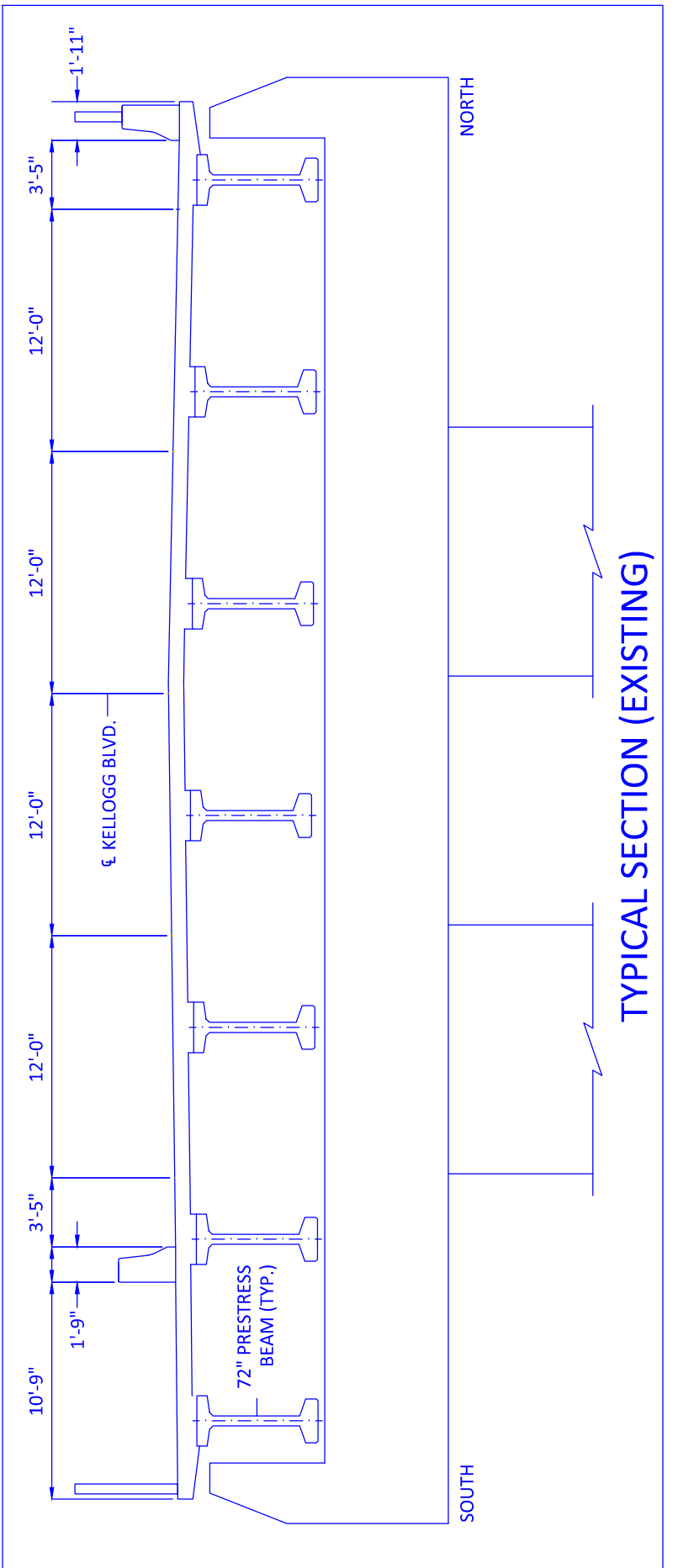
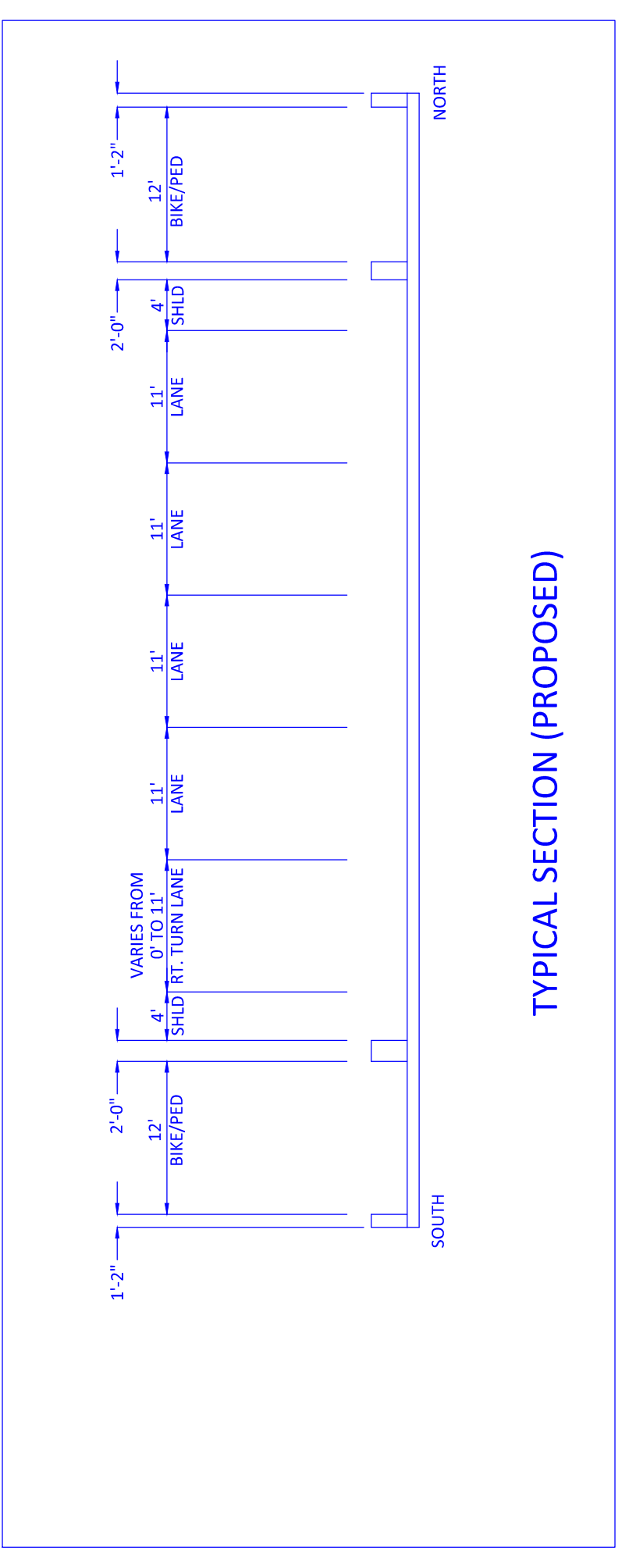
- LEGEND
- PAVED ROADWAY
 - BRIDGE & RETAINING WALLS
 - RAISED MEDIANS & CURBS
 - WALKS / TRAILS
 - EXISTING PONDING LOCATION
 - FUTURE CONSTRUCTION "BY OTHERS"
 - WETLAND LIMITS
 - EXISTING TRAFFIC SIGNAL
 - PROPOSED TRAFFIC SIGNAL TO BE REPLACED OR REVISED
 - EXISTING RAMP METER
 - EXISTING RIGHT OF WAY



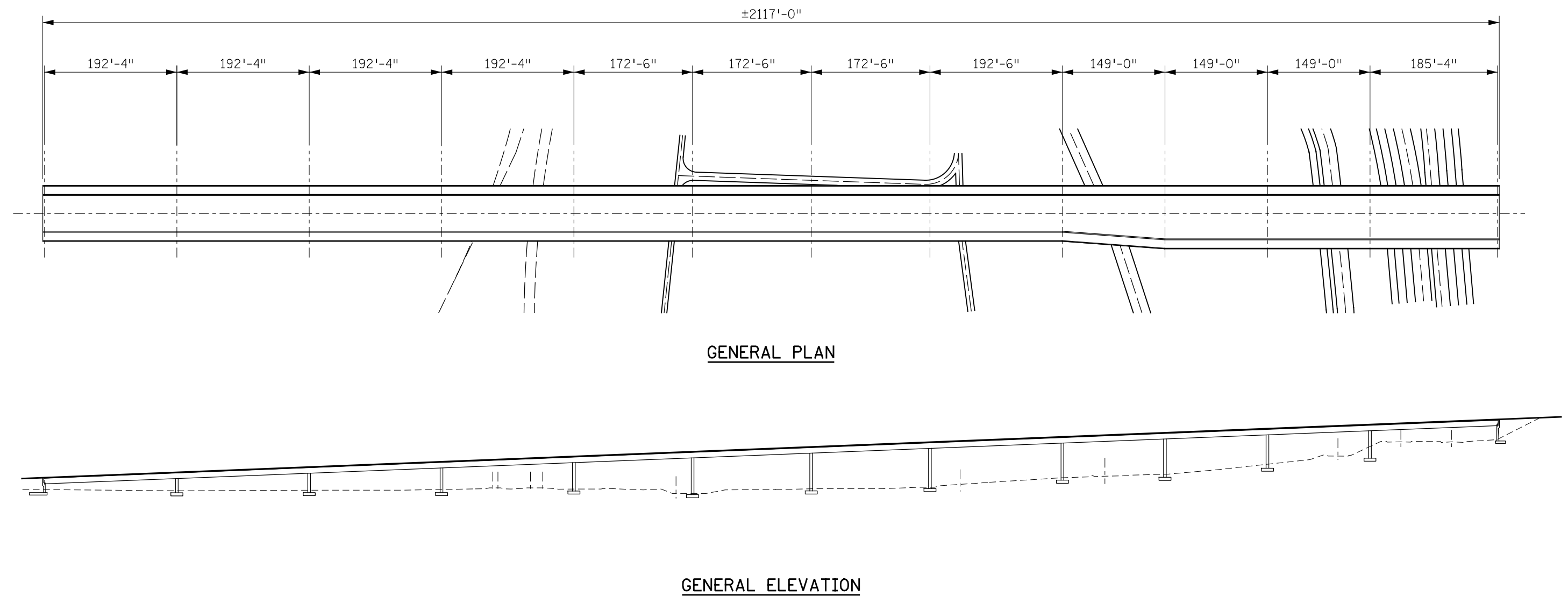
PROJECT LOCATION
COUNTY: RAMSEY
DISTRICT: METRO



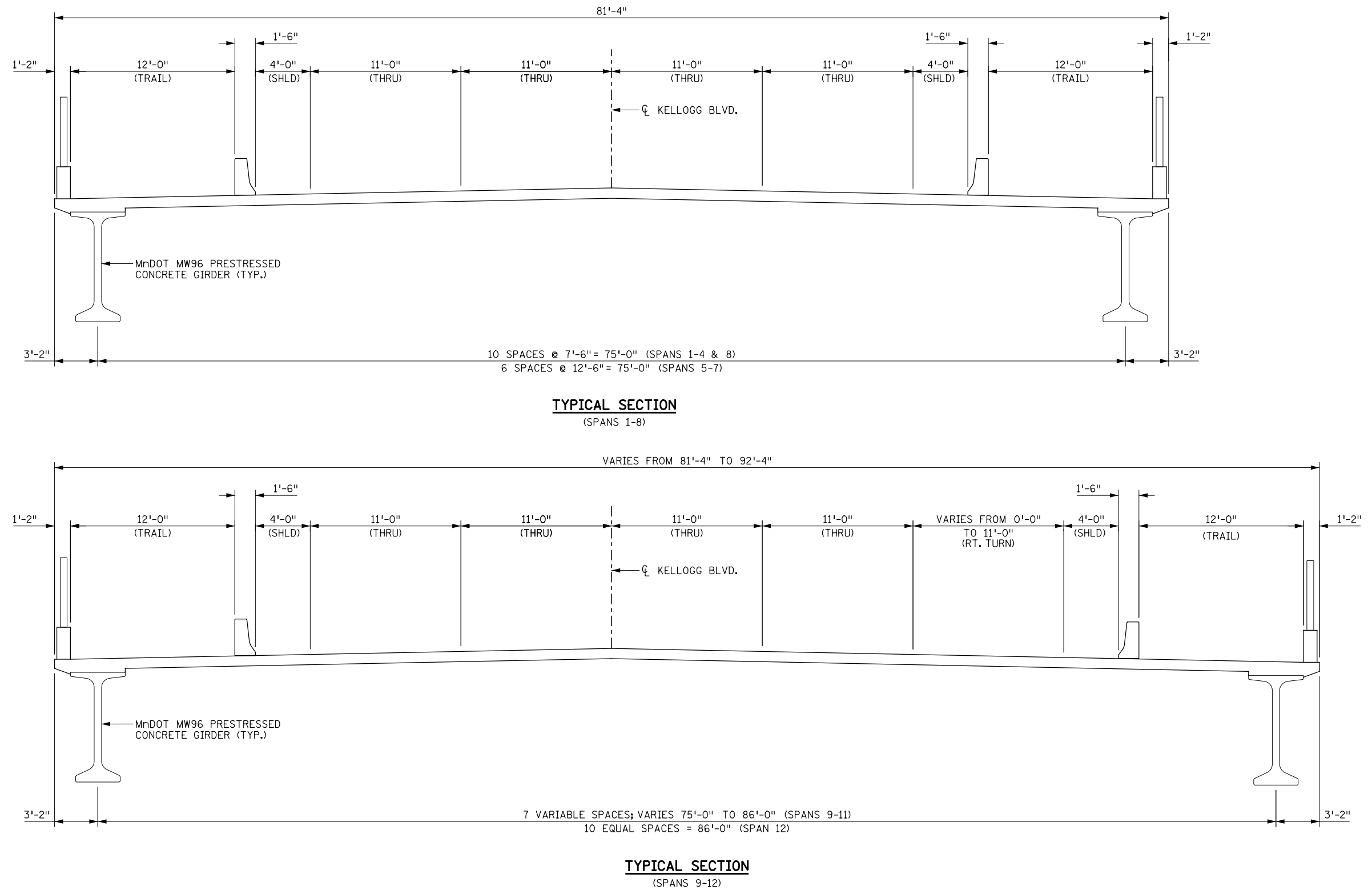
CONCEPTUAL LAYOUT

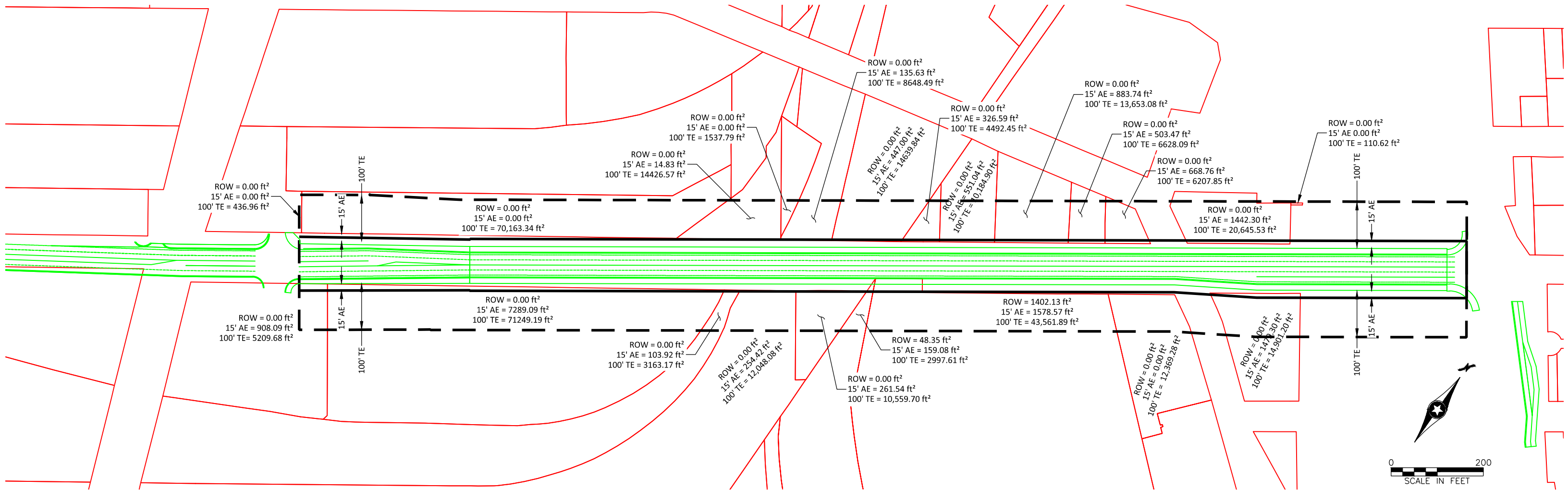


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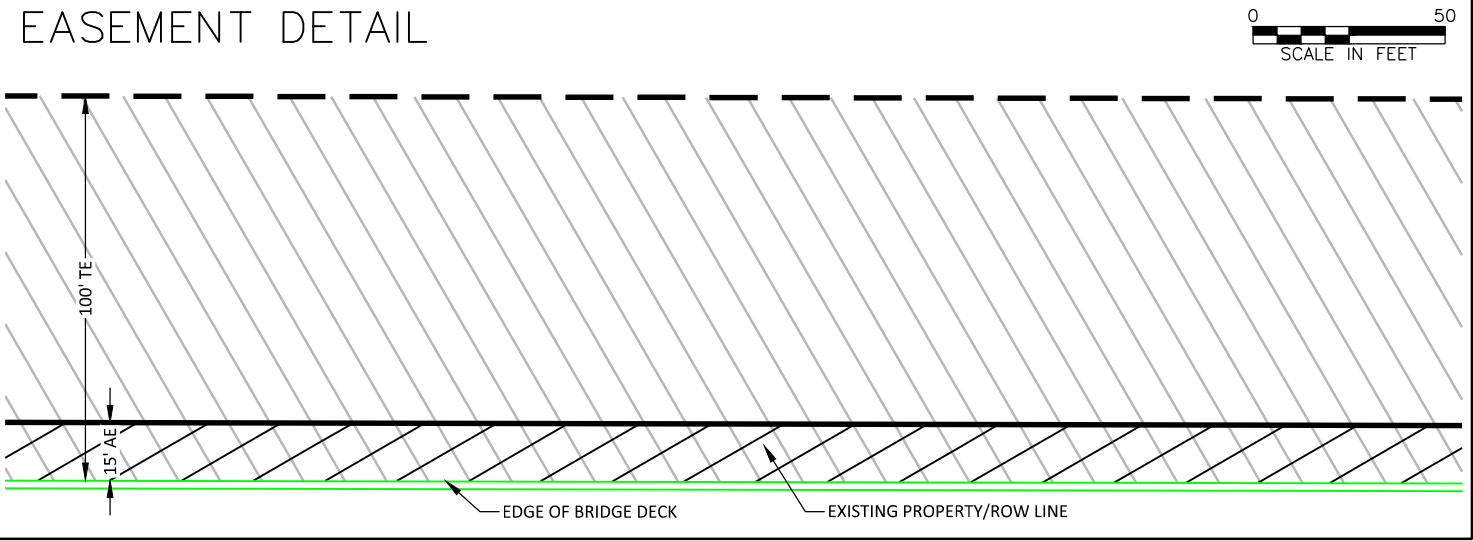




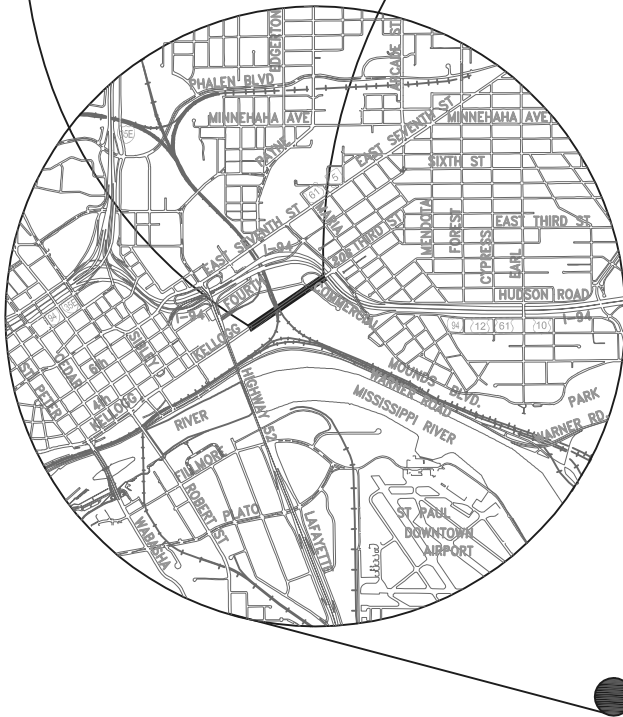
LEGEND

ROW	AREA FROM EDGE OF DECK TO EXISTING PROPERTY LINE
15' AE	(ACCESS EASEMENT) AREA FROM EDGE OF DECK TO 15' OFFSET (ASSUMES FOR BRIDGE MAINTENANCE/INSPECTION ACCESS)
100' TE	(TEMPORARY EASEMENT) 100' TEMPORARY CONSTRUCTION EASEMENT

EASEMENT DETAIL



**KELLOGG / 3RD ST.
BRIDGE NO'S 62080
AND 62080A**



NOT TO SCALE

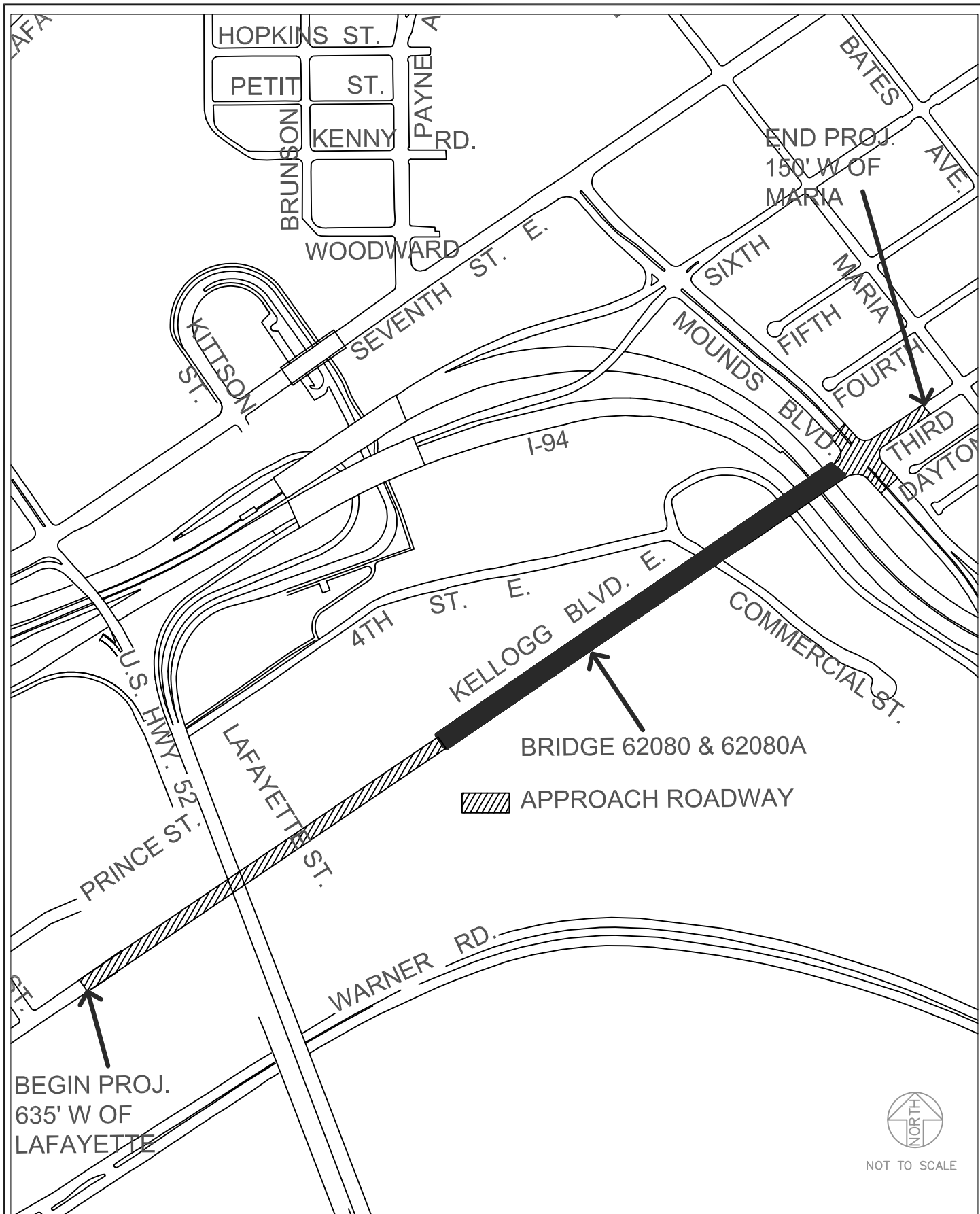


DESIGNED
BPC
DGN CHECK
GAP
DRAWN
RLA
DWG CHECK
GAP

PREPARED BY **BRIDGE DIVISION** FOR THE CITY OF ST. PAUL, DEPARTMENT OF PUBLIC WORKS
KELLOGG BLVD / 3RD ST BRIDGE NO'S 62080 & 62080A
OVER BN RAILWAY AND COMMERCIAL STREET
PROJECT AREA MAP

STATE AID
PROJECT NUMBER:
not assigned
CITY PROJECT NO.:
B-1202
DATE: 7/10/2018
PAGE: 1 of 2





DESIGNED
BPC
DGN CHECK
GAP
DRAWN
RLA
DWG CHECK
GAP

PREPARED BY **BRIDGE DIVISION** FOR THE CITY OF ST. PAUL, DEPARTMENT OF PUBLIC WORKS
**KELLOGG BLVD / 3RD ST BRIDGE NO'S 62080 & 62080A
OVER BN RAILWAY AND COMMERCIAL STREET
PROJECT AREA MAP**

STATE AID
PROJECT NUMBER:
not assigned
CITY PROJECT NO.:
B-1202
DATE: 7/10/2018
PAGE: 2 of 2



Level of Congestion

Bridges Project: Replacement of Kellogg/3rd Street Bridge | Map ID: 1530639399475



- Project Points
- Principal Arterials
- Principal Arterials Planned
- Project
- A Minor Arterials
- A Minor Arterials Planned

0 0.075 0.15 0.3 0.45 0.6 Miles

Created: 7/3/2018
LandscapeRSA1



For complete disclaimer of accuracy, please visit
<http://giswebsite.metc.state.mn.us/gissitenew/notice.aspx>



Regional Economy

Bridges Project: Replacement of Kellogg/3rd Street Bridge | Map ID: 1530639399475

Results

WITHIN ONE MI of project:
Postsecondary Students: 8424

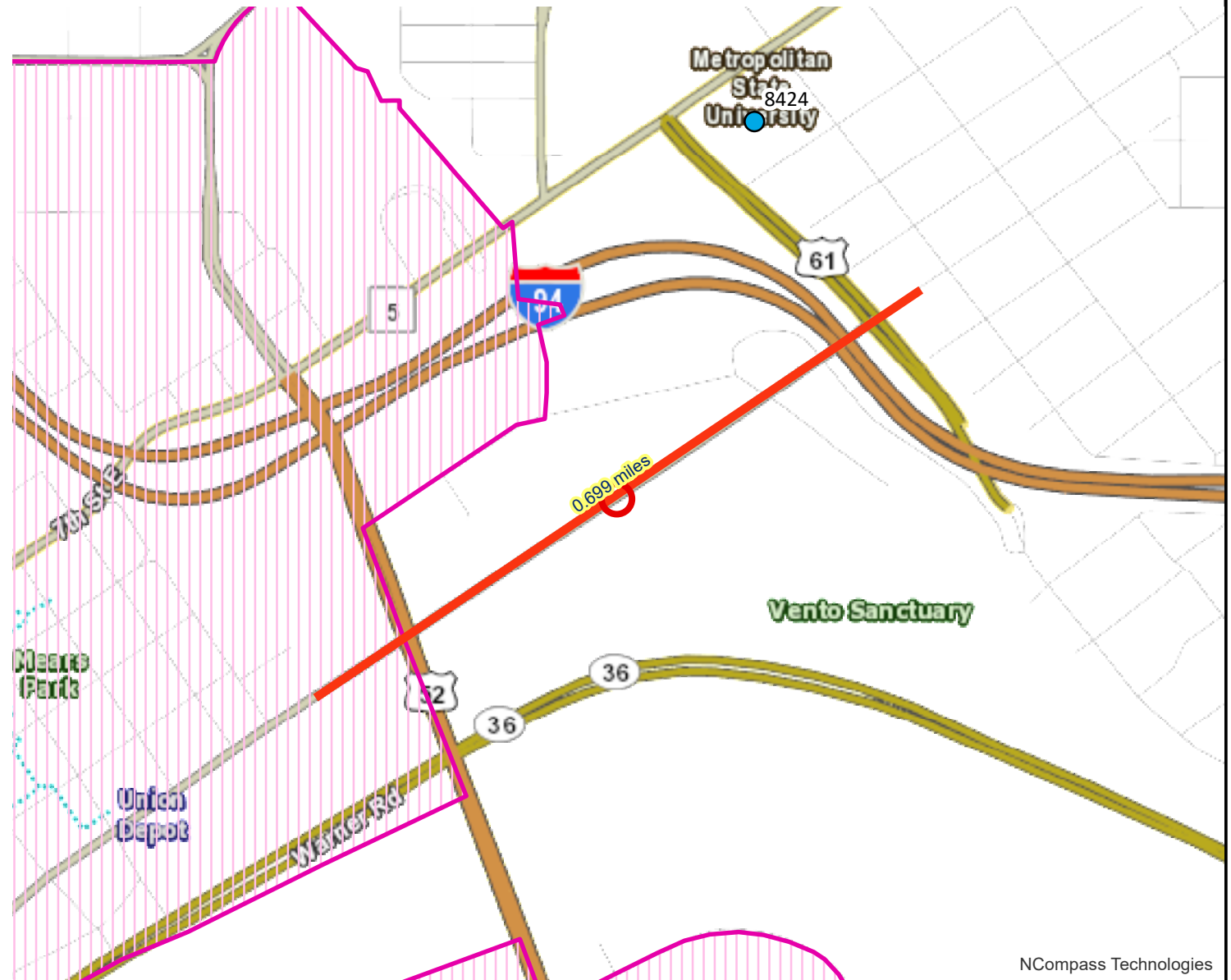
Totals by City:

St. Paul

Population: 25737

Employment: 70343

Mfg and Dist Employment: 3728



NCompass Technologies

- Project Points
- Postsecondary Education Centers
- Job Concentration Centers
- Project
- Manufacturing/Distribution Centers

0 0.075 0.15 0.3 0.45 0.6 Miles

Created: 7/3/2018
LandscapeRSA5



For complete disclaimer of accuracy, please visit
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Socio-Economic Conditions

Bridges Project: Replacement of Kellogg/3rd Street Bridge | Map ID: 1530639399475

Results

Project located **IN**
Area of Concentrated Poverty
with 50% or more of residents
are people of color (ACP50):
(0 to 30 Points)



NCompass Technologies



Project Points



Project



Area of Concentrated Poverty > 50% residents of color



Area of Concentrated Poverty



Above reg'l avg conc of race/poverty

0 0.075 0.15 0.3 0.45 0.6 Miles

Created: 7/3/2018
LandscapeRSA2

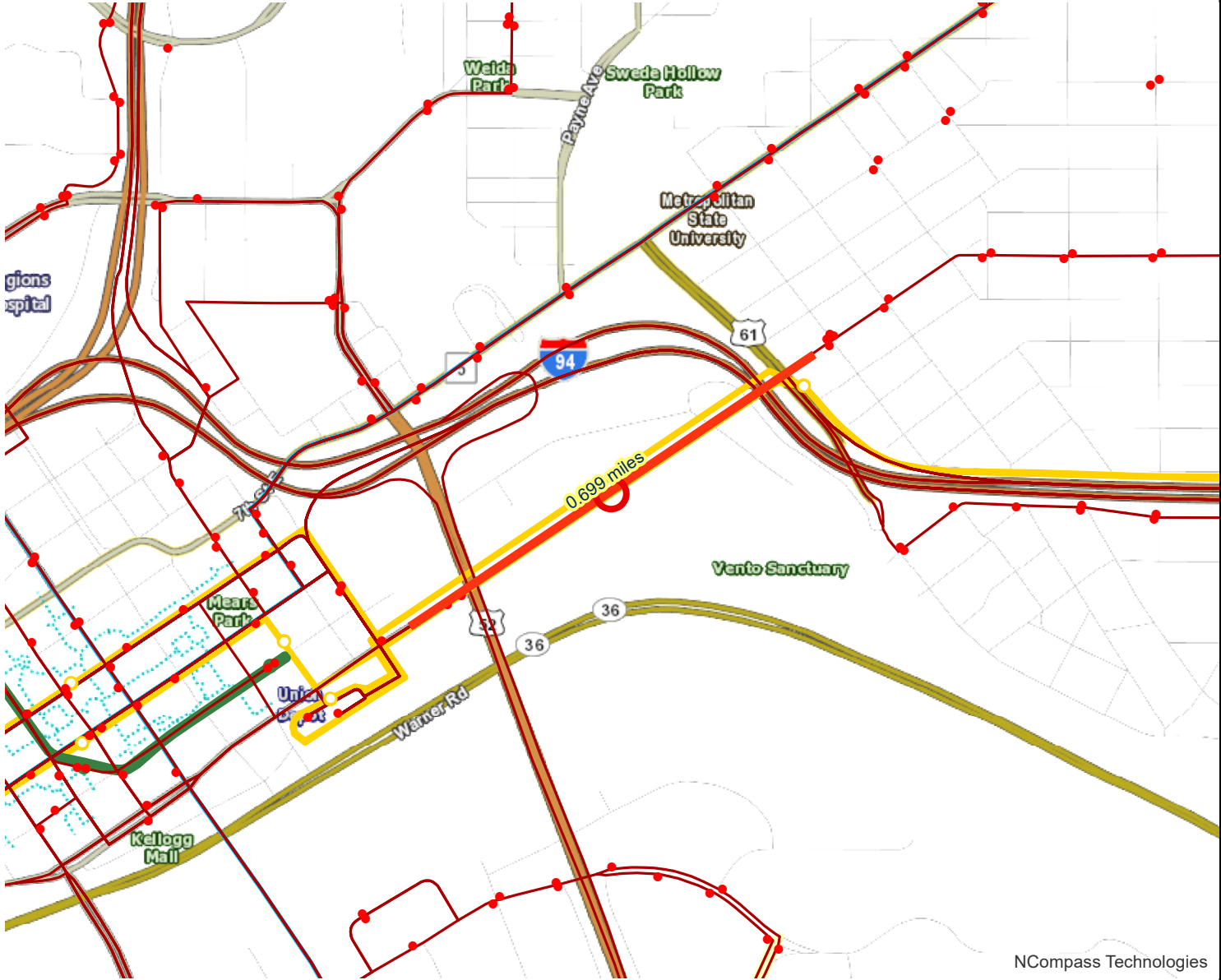


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Transit Connections

Bridges Project: Replacement of Kellogg/3rd Street Bridge | Map ID: 1530639399475



Results


Transit with a Direct Connection to project:
16 21 262 294 3 350 351 353 355 361 364
365 375 417 452 480 484 489 54 63 70 94

*Gold Line
*Gold Line


*indicates Planned Alignments

 Project Points


 Project

 Active Stop

Transitway Stations

 Green Line

Planned Transitway Stations


 Gold Line


 Transit Routes

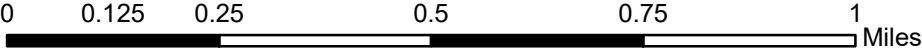
Transitway

 Green Line

Planned Transitway Alignments

 Gold Line

 Arterial BRT



Created: 7/3/2018
LandscapeRSA3



For complete disclaimer of accuracy, please visit
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NCompass Technologies



Legislation Text

File #: RES 18-803, Version: 1

Authorizing the Departments of Public Works and Parks and Recreation to submit eleven project applications for federal funding into the 2018 Metropolitan Council Regional Solicitation Program and to authorize the commitment of a twenty percent local funding match plus engineering for any project that is awarded federal funding.

WHEREAS, The Departments of Public Works and Parks and Recreation are proposing to submit eleven project applications for federal funding into the 2018 Metropolitan Council Regional Solicitation Program; and

WHEREAS, there is a required twenty percent local funding match to any project awarded to an agency under the Regional Solicitation Program; and

WHEREAS, the City commits to ensuring that all sidewalks and bikeways included in these project applications will be fully open for use and cleared of snow throughout the winter, either by City staff or by adjacent property owners per existing City ordinances; and

WHEREAS, the projects to be submitted by the City under the Metropolitan Council Regional Solicitation are:

- Kellogg/3rd Street Bridge Replacement
- Capital City Bikeway - Kellogg Boulevard from Jackson to St. Peter
- Troutbrook Road Connection - Kittson to Lafayette
- West Side Signalized Intersection Control Enhancements
- Sidewalk In-Fill Project - south side of Front Street from Dale to Mackubin
- Safe Routes to School Project - Bruce Vento Elementary School
- Sam Morgan Regional Trail Segment 1 Reconstruction
- Fish Hatchery Trail Stabilization and Reconstruction
- Point Douglas Regional Trail Phase 1 Construction
- Robert Pira Regional Trail Grade Separation at Barge Channel Rd
- HourCAR Expansion and Electrification

WHEREAS, these projects fall within appropriate funding categories and meet the conditions and requirements specified for eligibility of federal funding; now, therefore, be it

RESOLVED, that the Council of the City of Saint Paul authorizes submission of the project applications for possible award of federal transportation funds through the Metropolitan Council Regional Solicitation Program:

and be it

FURTHER RESOLVED, that the Council of the City of Saint Paul authorizes the commitment of local funds on a twenty percent match basis plus engineering for any project awarded federal funding under the Regional Solicitation Program.



City of Saint Paul

Signature Copy

Resolution: RES 14-2129

City Hall and Court
House
15 West Kellogg
Boulevard
Phone: 651-266-8560

File Number: RES 14-2129

Creating a prioritized bridge replacement list.

WHEREAS, the Department of Public Works has been tasked to create a Prioritized Bridge Replacement List by MnDOT, with estimated costs by funding type; and

WHEREAS, the prioritized list will be used by MnDOT for funding decisions; and

WHEREAS, the City of Saint Paul has reviewed the pertinent data on bridges requiring replacement, rehabilitation, or removal, supplied by local citizenry and local units of government; and

WHEREAS, the City of Saint Paul has identified those bridges that are high priority and that require replacement, rehabilitation, or removal within the next five years;

NOW, THEREFORE BE IT RESOLVED that the following deficient bridges are high priority and the City of Saint Paul intends to replace, rehabilitate, or remove these bridges as soon as possible when funds are available,

Old Br.No.	Road No. or Name	Total Proj. Cost*	State Br. Funds	Federal Funds	Local/MSA	
90396	Wheelock	\$3,095,645	\$2,549,016	\$0	\$546,629 2014	
92797	Kellogg (MSA 158)	\$5,632,000	\$686,400	\$2,745,600	\$2,200,000 2015	
92798	Kellogg (MSA 158)	\$3,301,000	\$350,000	\$1,400,000	\$1,551,000 2015	
62574	Kellogg (MSA 158)	\$500,000	\$0	\$0	\$500,000 2015	
62080	Kellogg (MSA 158)	tbd,	scoping rehabilitation and replacement options			2016-2018
5962	Forest (MSA 135)	\$6,000,000	\$1,000,000	\$4,000,000	\$1,000,000 2018	
62515	Lafayette (MSA 113)	\$4,750,000	\$750,000	\$3,000,000	\$1,000,000 2018	
90378	Kellogg	\$4,750,000	\$750,000	\$3,000,000	\$1,000,000 2019	

*all dollars approximate

FURTHERMORE, the City of Saint Paul does hereby request authorization to replace, rehabilitate, or remove such bridges.

At a meeting of the City Council on 12/17/2014, this Resolution was Passed.

Yea: 7 Councilmember Bostrom, Councilmember Brendmoen, City Council President Lantry, Councilmember Stark, Councilmember Thao, Councilmember Thune, and Councilmember Tolbert

Nay: 0

Vote Attested by
Council Secretary



Trudy Moloney

Date 12/17/2014

Approved by



Chris Coleman

Date 12/19/2014



CITY OF SAINT PAUL

Melvin Carter III, Mayor

Glenn Pagel, Division Manager
Bridge Engineering Division
900 City Hall Annex
25 West Fourth Street
Saint Paul, MN 55102-1660

Telephone: 651-266-6180
Fax: 651-292-6315

July 12, 2018

Mr. Ted Schoenecker
Public Works Director
Ramsey County
1425 Kirkwood Drive
Arden Hills, MN 55112

Re: Notification of Replacement of the Kellogg/3rd Street Bridge, Nos. 62080 and 62080A

Dear Mr. Schoenecker:

This letter is to notify you that the City of Saint Paul intends to reconstruct Kellogg Bridge nos. 62080 and 62080A over BNSF Railway property between John Street and Mounds Boulevard. The city is applying for 2022/2023 federal funding under the current Metropolitan Council Regional Solicitation.

The city has begun the preliminary design process and intends to construct the replacement bridge as soon as final plans are approved and funding has been secured. It is a possibility that demolition of the current bridge could begin during 2019, but more likely will begin in a subsequent construction season.

Thank you,

Glenn Pagel
Division Manager
Bridge Division



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CITY OF SAINT PAUL

Melvin Carter III, Mayor

Glenn Pagel, Division Manager
Bridge Engineering Division
900 City Hall Annex
25 West Fourth Street
Saint Paul, MN 55102-1660

Telephone: 651-266-6180

Fax: 651-292-6315

July 12, 2018

Ms. Johanna Berg
Interim Director
Ramsey County Regional Railroad Authority
Union Depot
214 4th Street East
Suite 200
Saint Paul, MN 55102

Re: Notification of Replacement of the Kellogg/3rd Street Bridge, Nos. 62080 and 62080A

Dear Ms. Berg:

This letter is to notify you that the City of Saint Paul intends to reconstruct Kellogg Bridge nos. 62080 and 62080A over BNSF Railway property between John Street and Mounds Boulevard. The city is applying for 2022/2023 federal funding under the current Metropolitan Council Regional Solicitation.

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Glenn Pagel
Division Manager
Bridge Division



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The Most Livable
City in America

CITY OF SAINT PAUL

Melvin Carter III, Mayor

Glenn Pagel, Division Manager
Bridge Engineering Division
900 City Hall Annex
25 West Fourth Street
Saint Paul, MN 55102-1660

Telephone: 651-266-6180

Fax: 651-292-6315

July 12, 2018

Mr. Richard Scott
Manager of Public Projects
BNSF Railway
80 44th Avenue Northeast
Fridley, MN 55421

Re: Notification of Replacement of the Kellogg/3rd Street Bridge, Nos. 62080 and 62080A

Dear Mr. Scott:

This letter is to notify you that the City of Saint Paul intends to reconstruct Kellogg Bridge nos. 62080 and 62080A over BNSF Railway property between John Street and Mounds Boulevard. The city is applying for 2022/2023 federal funding under the current Metropolitan Council Regional Solicitation.

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