



Application

19842 - 2024 Multiuse Trails and Bicycle Facilities
20485 - Bryant Avenue Pedestrian Bridge
Regional Solicitation - Bicycle and Pedestrian Facilities

Status: Submitted
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Primary Contact

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What Grant Programs are you most interested in? Regional Solicitation - Bicycle and Pedestrian Facilities

Organization Information

Name: SOUTH ST PAUL, CITY OF
Jurisdictional Agency (if different):
Organization Type: City
Organization Website:
Address: 125 3RD AVE N

* SO ST PAUL Minnesota 55075
City State/Province Postal Code/Zip
County: Dakota
Phone: * 612-450-8704
Ext.
Fax:
PeopleSoft Vendor Number 0000020997A1

Project Information

Project Name Bryant Avenue Pedestrian Bridge
Primary County where the Project is Located Dakota
Cities or Townships where the Project is Located: South St. Paul
Jurisdictional Agency (If Different than the Applicant):

Brief Project Description (Include location, road name/functional class, type of improvement, etc.) The Bryant Avenue Pedestrian Bridge will provide dedicated pedestrian and bicycle access from Concord Street (CSAH 156) to the Mississippi River Greenway/Robert Purum Trail and Kaposia Landing Park area. The proposed project is a 12? wide paved and ADA accessible multi-use trail constructed along the existing bridge. Although it currently lacks non-motorized facilities, this bridge crossing is already widely used by pedestrians and bicyclists attempting to cross the railroad yard east of CSAH 156 ? a Regional Bicycle Barrier Segment.

The installation of a mixed-use trail will connect non-motorized users to downtown Saint Paul ? a regional employment center ? via the Mississippi River Greenway and the Robert Purum Trail. This trail is a Tier 1 Bicycle and Trail Network alignment, extending 27 miles along the Mississippi River from St. Paul to Hastings. It will also connect residents to over 7,000 industrial employment opportunities along CSAH 156. This will encourage non-motorized travel to daily needs and services as well as provide a safe way to access employment opportunities via walking or biking.

There is documented need for dedicated pedestrian and bicycle facilities along the Bryant Avenue Bridge. Non-motorized users accessing Kaposia Landing or the Mississippi River Greenway are often seen travelling along the narrow roadway in the vehicle lanes. Because Bryant Avenue serves as the main entrance to Kaposia Landing and one of few entrances to the Mississippi River Greenway in South St. Paul, non-motorized users are forced to enter the vehicle lanes along the bridge. Metro Transit Route 71 stops at the intersection of CSAH 156 and Bryant Avenue, contributing further to the large number of pedestrians attempting to cross the bridge. Due to the narrow lanes and geometry that results in a 90 degree turn on the bridge?s east approach, entering the driving lanes can be very dangerous for pedestrians and bicyclists.

During the planning processes for the Dakota County Pedestrian and Bicycle Study, the South St. Paul Bicycle and Pedestrian Plan, and the South St. Paul 2040 Comprehensive Plan update, the Bryant Avenue bridge was identified as a pedestrian gap and a top priority for the addition of non-motorized facilities. The City of South St. Paul was successful in securing a Statewide Health Improvement Partnership (SHIP) grant to evaluate the design of the proposed project.

(Limit 2,800 characters; approximately 400 words)

TRANSPORTATION IMPROVEMENT PROGRAM (TIP) DESCRIPTION - will be used in TIP if the project is selected for funding. See MnDOT's TIP description guidance. Construct new, separated multi-use bridge adjacent to the existing traffic deck and within an existing bridge easement of the Bryant Avenue Bridge (Bridge No. 19544)

Include both the CSAH/MSAS/TH references and their corresponding street names in the TIP Description (see Resources link on Regional Solicitation webpage for examples).

Project Length (Miles) 0.2
to the nearest one-tenth of a mile

| Project Funding | |
|--|------------------|
| Are you applying for competitive funds from another source(s) to implement this project? | No |
| If yes, please identify the source(s) | |
| Federal Amount | \$4,960,000.00 |
| Match Amount | \$1,240,000.00 |
| Minimum of 20% of project total | |
| Project Total | \$6,200,000.00 |
| For transit projects, the total cost for the application is total cost minus fare revenues. | |
| Match Percentage | 20.0% |
| Minimum of 20% Compute the match percentage by dividing the match amount by the project total | |
| Source of Match Funds | Local City Funds |
| A minimum of 20% of the total project cost must come from non-federal sources; additional match funds over the 20% minimum can come from other federal sources | |

Preferred Program Year

Select one: 2028

Select 2026 or 2027 for TDM and Unique projects only. For all other applications, select 2028 or 2029.

Additional Program Years: 2026, 2027

Select all years that are feasible if funding in an earlier year becomes available.

Project Information

If your project has already been assigned a State Aid Project # (SAP or SP)

Please indicate here SAP/SP#.

Location

County, City, or Lead Agency

South St. Paul

Name of Trail/Ped Facility:

Bryant Avenue Pedestrian Bridge

(example; CEDAR LAKE TRAIL)

IF TRAIL/PED FACILITY IS ADJACENT TO ROADWAY:

Road System

City Street

(TH, CSAH, MSAS, CO. RD., TWP. RD., CITY STREET)

Road/Route No.

(Example: 53 for CSAH 53)

Name of Road

Bryant Avenue

(Example: 1st ST., Main Ave.)

TERMINI: Termini listed must be within 0.3 miles of any work

From:

Road System

CSAH

(TH, CSAH, MSAS, CO. RD., TWP. RD., CITY STREET)

Road/Route No.

156

(Example: 53 for CSAH 53)

Name of Road

Concord Street N

(Example: 1st ST., Main Ave.)

To:

Road System

DO NOT INCLUDE LEGAL DESCRIPTION; INCLUDE NAME OF ROADWAY
IF MAJORITY OF FACILITY RUNS ADJACENT TO A SINGLE CORRIDOR

Road/Route No.

(Example: 53 for CSAH 53)

Name of Road

Mississippi River Greeway at Kaposia Landing

(Example: 1st ST., Main Ave.)

In the City/Cities of:

South St. Paul

(List all cities within project limits)

IF TRAIL/PED FACILITY IS NOT ADJACENT TO ROADWAY:

Termini: Termini listed must be within 0.3 miles of any work

From:

To:

Or

At:

In the City/Cities of:

(List all cities within project limits)

Primary Types of Work (Check all that apply)

Multi-Use Trail

Yes

Reconstruct Trail

Resurface Trail

Bituminous Pavement

Yes

Concrete Walk

Yes

Pedestrian Bridge

Yes

Signal Revision

Landscaping

Other (do not include incidental items)

BRIDGE/CULVERT PROJECTS (IF APPLICABLE)

Old Bridge/Culvert No.:

19544

New Bridge/Culvert No.:

Structure is Over/Under
(Bridge or culvert name):

Zip Code where Majority of Work is Being Performed

55075

Approximate Begin Construction Date (MO/YR)

04/01/2028

Approximate End Construction Date (MO/YR)

10/31/2028

Miles of Pedestrian Facility/Trail (nearest 0.1 miles):

0.2

Miles of trail on the Regional Bicycle Transportation Network (nearest 0.1 miles):

0.2

Is this a new trail?

Yes

Requirements - All Projects

All Projects

1. The project must be consistent with the goals and policies in these adopted regional plans: Thrive MSP 2040 (2014), the 2040 Transportation Policy Plan (2018), the 2040 Regional Parks Policy Plan (2018), and the 2040 Water Resources Policy Plan (2015).

Check the box to indicate that the project meets this requirement.

Yes

2. The project must be consistent with the 2040 Transportation Policy Plan. Reference the 2040 Transportation Plan goals, objectives, and strategies that relate to the project.

Briefly list the goals, objectives, strategies, and associated pages:

GOAL: TRANSPORTATION SYSTEM STEWARDSHIP

Objective B: Operate the regional transportation system to efficiently and cost-effectively

connect people and freight to destinations. (P. 42)

GOAL: ACCESS TO DESTINATIONS

Objective A: Increase the availability of multimodal travel options, especially in congested highway corridors. (P. 46)

Objective D: Increase the number and share of trips taken using transit, carpool, bicycling, and walking. (P. 46)

Objective E: Improve the availability and quality of multimodal travel options for people of all ages and abilities to connect to jobs and other opportunities, particularly for historically underrepresented populations. (P. 46)

GOAL: COMPETITIVE ECONOMY

Objective A: Improve multimodal access to regional job concentrations identified in

Thrive MSP 2040. (P. 48)

Objective B: Invest in a multimodal transportation system to attract and retain businesses and residents. (P. 48)

GOAL: HEALTHY AND EQUITABLE COMMUNITIES

Objective A: Reduce transportation-related air emissions. (P. 50)

Objective C: Increase the availability and attractiveness of transit, bicycling, and walking to encourage healthy communities through the use of active transportation options. (P. 50)

GOAL: LEVERAGING TRANSPORTATION INVESTMENT TO GUIDE LAND USE

Objective A: Focus regional growth in areas that support the full range of multimodal travel. (P.53)

(Limit 2,800 characters; approximately 400 words)

3. The project or the transportation problem/need that the project addresses must be in a local planning or programming document. Reference the name of the appropriate comprehensive plan, regional/statewide plan, capital improvement program, corridor study document [studies on trunk highway must be approved by the Minnesota Department of Transportation and the Metropolitan Council], or other official plan or program of the applicant agency [includes Safe Routes to School Plans] that the project is included in and/or a transportation problem/need that the project addresses.

List the applicable documents and pages: Unique projects are exempt from this qualifying requirement because of their innovative nature.

Bryant Avenue Pedestrian Bridge study:

<https://www.southstpaul.org/DocumentCenter/View/8002/Bryant-Avenue-Pedestrian-Bridge-Study>

South St. Paul 2040 Comprehensive Plan (Pages 3-32, 3-38, 4-73, 4-78, 4-110, 5-128, 7-182, 9-247):

https://www.southstpaul.org/DocumentCenter/View/7446/SSP_2040CompPlan_January2021

South St. Paul Bicycle and Pedestrian Plan (Pages 23, 24, 33, 35, 37, 49, 51, 54, 88, 91): <http://www.southstpaul.org/DocumentCenter/View/989/DRAFT-SSP-bike-ped-plan-11-7-14---for-public-review>

Dakota County Pedestrian and Bicycle Study (Pages ES-4, ES-5, 2-7, C-2):

<https://www.co.dakota.mn.us/Transportation/TransportationStudies/Past/Pages/pedestrian-bike-plan.aspx>

(Limit 2,800 characters; approximately 400 words)

4. The project must exclude costs for studies, preliminary engineering, design, or construction engineering. Right-of-way costs are only eligible as part of transit stations/stops, transit terminals, park-and-ride facilities, or pool-and-ride lots. Noise barriers, drainage projects, fences, landscaping, etc., are not eligible for funding as a standalone project, but can be included as part of the larger submitted project, which is otherwise eligible. Unique project costs are limited to those that are federally eligible.

Check the box to indicate that the project meets this requirement.

Yes

5. Applicant is a public agency (e.g., county, city, tribal government, transit provider, etc.) or non-profit organization (TDM and Unique Projects applicants only). Applicants that are not State Aid cities or counties in the seven-county metro area with populations over 5,000 must contact the MnDOT Metro State Aid Office prior to submitting their application to determine if a public agency sponsor is required.

Check the box to indicate that the project meets this requirement.

Yes

6. Applicants must not submit an application for the same project in more than one funding sub-category.

Check the box to indicate that the project meets this requirement.

Yes

7. The requested funding amount must be more than or equal to the minimum award and less than or equal to the maximum award. The cost of preparing a project for funding authorization can be substantial. For that reason, minimum federal amounts apply. Other federal funds may be combined with the requested funds for projects exceeding the maximum award, but the source(s) must be identified in the application. Funding amounts by application category are listed below in Table 1. For unique projects, the minimum award is \$500,000 and the maximum award is the total amount available each funding cycle (approximately \$4,000,000 for the 2024 funding cycle).

Multiuse Trails and Bicycle Facilities: \$250,000 to \$5,500,000

Pedestrian Facilities (Sidewalks, Streetscaping, and ADA): \$250,000 to \$2,000,000

Safe Routes to School: \$250,000 to \$1,000,000

Check the box to indicate that the project meets this requirement.

Yes

8. The project must comply with the Americans with Disabilities Act (ADA).

Check the box to indicate that the project meets this requirement.

Yes

9. In order for a selected project to be included in the Transportation Improvement Program (TIP) and approved by USDOT, the public agency sponsor must either have a current Americans with Disabilities Act (ADA) self-evaluation or transition plan that covers the public right of way/transportation, as required under Title II of the ADA. The plan must be completed by the local agency before the Regional Solicitation application deadline. For future Regional Solicitation funding cycles, this requirement may include that the plan has undergone a recent update, e.g., within five years prior to application.

The applicant is a public agency that employs 50 or more people and has a completed ADA transition plan that covers the public right of way/transportation.

Yes

Date plan completed:

01/01/2018

Link to plan:

<https://www.southstpaul.org/512/ADA-Transition-Plan>

The applicant is a public agency that employs fewer than 50 people and has a completed ADA self-evaluation that covers the public right of way/transportation.

Date self-evaluation completed:

Link to plan:

Upload plan or self-evaluation if there is no link

Upload as PDF

10. The project must be accessible and open to the general public.

Check the box to indicate that the project meets this requirement.

Yes

11. The owner/operator of the facility must operate and maintain the project year-round for the useful life of the improvement. This includes assurance of year-round use of bicycle, pedestrian, and transit facilities, per FHWA direction established 8/27/2008 and updated 4/15/2019. Unique projects are exempt from this qualifying requirement.

Check the box to indicate that the project meets this requirement.

Yes

12. The project must represent a permanent improvement with independent utility. The term "independent utility" means the project provides benefits described in the application by itself and does not depend on any construction elements of the project being funded from other sources outside the regional solicitation, excluding the required non-federal match.

Projects that include traffic management or transit operating funds as part of a construction project are exempt from this policy.

Check the box to indicate that the project meets this requirement. Yes

13. The project must not be a temporary construction project. A temporary construction project is defined as work that must be replaced within five years and is ineligible for funding. The project must also not be staged construction where the project will be replaced as part of future stages. Staged construction is eligible for funding as long as future stages build on, rather than replace, previous work.

Check the box to indicate that the project meets this requirement. Yes

14. The project applicant must send written notification regarding the proposed project to all affected state and local units of government prior to submitting the application.

Check the box to indicate that the project meets this requirement. Yes

Requirements - Bicycle and Pedestrian Facilities Projects

1. All projects must relate to surface transportation. As an example, for multiuse trail and bicycle facilities, surface transportation is defined as primarily serving a commuting purpose and/or that connect two destination points. A facility may serve both a transportation purpose and a recreational purpose; a facility that connects people to recreational destinations may be considered to have a transportation purpose.

Check the box to indicate that the project meets this requirement. Yes

Multiuse Trails on Active Railroad Right-of-Way:

2. All multiuse trail projects that are located within right-of-way occupied by an active railroad must attach an agreement with the railroad that this right-of-way will be used for trail purposes.

Check the box to indicate that the project meets this requirement.

Upload Agreement PDF

Check the box to indicate that the project is not in active railroad right-of-way. Yes

Multiuse Trails and Bicycle Facilities projects only:

3. All applications must include a letter from the operator of the facility confirming that they will remove snow and ice for year-round bicycle and pedestrian use. The Minnesota Pollution Control Agency has a resource for best practices when using salt. Upload PDF of Agreement in Other Attachments.

Check the box to indicate that the project meets this requirement. Yes

Upload PDF of Agreement in Other Attachments.

Safe Routes to School projects only:

4. All projects must be located within a two-mile radius of the associated primary, middle, or high school site.

Check the box to indicate that the project meets this requirement.

5. All schools benefitting from the SRTS program must conduct after-implementation surveys. These include the student travel tally form and the parent survey available on the National Center for SRTS website. The school(s) must submit the after-evaluation data to the National Center for SRTS within a year of the project completion date. Additional guidance regarding evaluation can be found at the MnDOT SRTS website.

Check the box to indicate that the applicant understands this requirement and will submit data to the National Center for SRTS within one year of project completion.

Requirements - Bicycle and Pedestrian Facilities Projects

Specific Roadway Elements

CONSTRUCTION PROJECT ELEMENTS/COST ESTIMATES

| | Cost |
|--|----------------|
| Mobilization (approx. 5% of total cost) | \$316,000.00 |
| Removals (approx. 5% of total cost) | \$246,000.00 |
| Roadway (grading, borrow, etc.) | \$36,000.00 |
| Roadway (aggregates and paving) | \$0.00 |
| Subgrade Correction (muck) | \$0.00 |
| Storm Sewer | \$60,000.00 |
| Ponds | \$0.00 |
| Concrete Items (curb & gutter, sidewalks, median barriers) | \$0.00 |
| Traffic Control | \$158,000.00 |
| Striping | \$60,000.00 |
| Signing | \$36,000.00 |
| Lighting | \$60,000.00 |
| Turf - Erosion & Landscaping | \$158,000.00 |
| Bridge | \$2,382,000.00 |
| Retaining Walls | \$600,000.00 |
| Noise Wall (not calculated in cost effectiveness measure) | \$0.00 |
| Traffic Signals | \$0.00 |
| Wetland Mitigation | \$0.00 |
| Other Natural and Cultural Resource Protection | \$0.00 |
| RR Crossing | \$1,440,000.00 |
| Roadway Contingencies | \$600,000.00 |

| | |
|------------------------|-----------------------|
| Other Roadway Elements | \$0.00 |
| Totals | \$6,152,000.00 |

Specific Bicycle and Pedestrian Elements

| CONSTRUCTION PROJECT ELEMENTS/COST ESTIMATES | Cost |
|--|--------------------|
| Path/Trail Construction | \$36,000.00 |
| Sidewalk Construction | \$0.00 |
| On-Street Bicycle Facility Construction | \$0.00 |
| Right-of-Way | \$0.00 |
| Pedestrian Curb Ramps (ADA) | \$6,000.00 |
| Crossing Aids (e.g., Audible Pedestrian Signals, HAWK) | \$0.00 |
| Pedestrian-scale Lighting | \$0.00 |
| Streetscaping | \$0.00 |
| Wayfinding | \$0.00 |
| Bicycle and Pedestrian Contingencies | \$6,000.00 |
| Other Bicycle and Pedestrian Elements | \$0.00 |
| Totals | \$48,000.00 |

Specific Transit and TDM Elements

| CONSTRUCTION PROJECT ELEMENTS/COST ESTIMATES | Cost |
|---|---------------|
| Fixed Guideway Elements | \$0.00 |
| Stations, Stops, and Terminals | \$0.00 |
| Support Facilities | \$0.00 |
| Transit Systems (e.g. communications, signals, controls, fare collection, etc.) | \$0.00 |
| Vehicles | \$0.00 |
| Contingencies | \$0.00 |
| Right-of-Way | \$0.00 |
| Other Transit and TDM Elements | \$0.00 |
| Totals | \$0.00 |

Transit Operating Costs

| | |
|--|--------|
| Number of Platform hours | 0 |
| Cost Per Platform hour (full loaded Cost) | \$0.00 |
| Subtotal | \$0.00 |
| Other Costs - Administration, Overhead, etc. | \$0.00 |

PROTECT Funds Eligibility

One of the new federal funding sources is Promoting Resilient Operations for Transformative, Efficient, and Cost-Saving Transportation (PROTECT). Please describe which specific elements of your project and associated costs out of the Total TAB-Eligible Costs are eligible to receive PROTECT funds. Examples of potential eligible items may include: storm sewer, ponding, erosion control/landscaping, retaining walls, new bridges over floodplains, and road realignments out of floodplains.

INFORMATION: Promoting Resilient Operations for Transformative, Efficient, and Cost-Saving Transportation (PROTECT) Formula Program Implementation Guidance (dot.gov).

| | |
|------------------|--|
| Response: | The Bryant Avenue Pedestrian Bridge Project will construct a new pedestrian and bicycle bridge over a floodplain near the Mississippi River. According to FEMA Flood Map 27037C0043E, effective 12/2/2011, the land under the proposed bridge is classified as a Special Flood Hazard Area and an Other Area of Flood Hazard. Part of the bridge will be constructed over Zone X with a 0.2% annual chance of flood hazard/area of 1% annual change of flood with an average depth less than one foot or with drainage areas of less than one square mile. Another part of the bridge will be constructed over Zone AE which is a special flood hazard area. This area has a 1% annual chance of flooding. |
|------------------|--|

Totals

| | |
|------------------------------|----------------|
| Total Cost | \$6,200,000.00 |
| Construction Cost Total | \$6,200,000.00 |
| Transit Operating Cost Total | \$0.00 |

Measure A: Project Location Relative to the RBTN

Select one:

Tier 1, Priority RBTN Corridor

Tier 1, RBTN Alignment

Tier 2, RBTN Corridor

Tier 2, RBTN Alignment

Yes

Direct connection to an RBTN Tier 1 corridor or alignment

Direct connection to an RBTN Tier 2 corridor or alignment

OR

Project is not located on or directly connected to the RBTN but is part of a local system and identified within an adopted county, city or regional parks implementing agency plan.

Upload Map

1702509324724_006_RBTN Orientation Map.pdf

Please upload attachment in PDF form

Measure A: Population Summary

Existing Population Within One Mile (Integer Only)

13495

Existing Employment Within One Mile (Integer Only)

7106

Upload the "Population Summary" map

1702509358630_004_Population-Employment Summary Map.pdf

Please upload attachment in PDF form

Measure A: Engagement

i. Describe any Black, Indigenous, and People of Color populations, low-income populations, disabled populations, youth, or older adults within a ½ mile of the proposed project. Describe how these populations relate to regional context. Location of affordable housing will be addressed in Measure C.

ii. Describe how Black, Indigenous, and People of Color populations, low-income populations, persons with disabilities, youth, older adults, and residents in affordable housing were engaged, whether through community planning efforts, project needs identification, or during the project development process.

iii. Describe the progression of engagement activities in this project. A full response should answer these questions:

1. What engagement methods and tools were used?
2. How did you engage specific communities and populations likely to be directly impacted by the project?
3. What techniques did you use to reach populations traditionally not involved in community engagement related to transportation projects?
4. How were the project's purpose and need identified?
5. How was the community engaged as the project was developed and designed?
6. How did you provide multiple opportunities for of Black, Indigenous, and People of Color populations, low-income populations, persons with disabilities, youth, older adults, and residents in affordable housing to engage at different points of project development?
7. How did engagement influence the project plans or recommendations? How did you share back findings with community and re-engage to assess responsiveness of these changes?
8. If applicable, how will NEPA or Title VI regulations will guide engagement activities?

Response:

The City of South St. Paul prioritizes public engagement and employs robust engagement strategies for all major projects. The public coordination process for the Bryant Avenue Pedestrian Bridge has emphasized reaching underrepresented populations, including black, indigenous, people of color (BIPoC), low-income individuals, persons with disabilities, youth, older adults, and residents in affordable housing.

The City was successful in securing a Statewide Health Improvement Partnership (SHIP) grant to evaluate design and funding for dedicated pedestrian access from Concord Street (CSAH 156) to the Kaposia Landing area. All stakeholder and community engagement aligned with SHIP funding requirements.

All past engagement for the proposed project was held online due to COVID-19 Health Emergency. In June 2021, the city-hosted webpage was made active with links to the draft study report for the project, a video presentation on the study, and a survey and public comment form. The engagement materials were advertised through the city website, social media accounts, and email lists. Flyers for the study were also posted at Kaposia Landing and on the Mississippi River Greenway trail. On July 1, 2021, the active webpage announcement was released with the city-wide newsletter.

The public comment period was open until July 2021. Following the end of this period, all public comments were logged and delivered to the project team. The project team received 37 responses on the survey, 95% of which were in favor of the bridge. Input from the survey was used to directly influence study goals and the design of the bridge.

The City of South St. Paul plans to conduct further engagement during design and construction to reach equity populations in the study area. This will include an open house hosted at an accessible community destination and specific outreach to residents of two low-income, high-rise facilities operated by the City--Nan McKay and John Carroll Apartments.

Both buildings are approximately one mile from the proposed bridge and are connected via Highway 156 and the Mississippi River Greenway Trail. The project team recognizes that the Bryant Avenue bridge is located in a census tract that is above the regional average for populations in poverty and populations of color. The project is also located in a regional environmental justice area. The City believes it is important to engage low-income housing residents in this study, as residents of both buildings will benefit directly from the proposed project. Future engagement materials will be advertised through the city website, city newsletter, social media accounts, email lists, and by posting flyers at public parks.

(Limit 2,800 characters; approximately 400 words):

Measure B: Disadvantaged Communities Benefits and Impacts

Describe the project's benefits to Black, Indigenous, and People of Color populations, low-income populations, children, people with disabilities, youth, and older adults. Benefits could relate to:

- ? pedestrian and bicycle safety improvements;
- ? public health benefits;
- ? direct access improvements for residents or improved access to destinations such as jobs, school, health care, or other;
- ? travel time improvements;
- ? gap closures;
- ? new transportation services or modal options;
- ? leveraging of other beneficial projects and investments;
- ? and/or community connection and cohesion improvements.

This is not an exhaustive list. A full response will support the benefits claimed, identify benefits specific to Disadvantaged communities residing or engaged in activities near the project area, identify benefits addressing a transportation issue affecting Disadvantaged communities specifically identified through engagement, and substantiate benefits with data.

Acknowledge and describe any negative project impacts to Black, Indigenous, and People of Color populations, low-income populations, children, people with disabilities, youth, and older adults. Describe measures to mitigate these impacts. Unidentified or unmitigated negative impacts may result in a reduction in points.

Below is a list of potential negative impacts. This is not an exhaustive list.

- ? Decreased pedestrian access through sidewalk removal / narrowing, placement of barriers along the walking path, increase in auto-oriented curb cuts, etc.
- ? Increased speed and/or cut-through traffic.
- ? Removed or diminished safe bicycle access.
- ? Inclusion of some other barrier to access to jobs and other destinations.

Response:

The Bryant Avenue bridge is a single-lane undivided roadway that lacks pedestrian and bicycle facilities. The proposed project will provide a new multi-use trail bridge adjacent to the existing bridge. All updates proposed as part of this effort will be ADA-compliant to serve limited mobility populations who heavily rely on these facilities. Improvements will improve the safety, security, and visibility of the roadway's most vulnerable travelers.

There is documented need for dedicated pedestrian and bicycle facilities along the Bryant Avenue Bridge. Pedestrians and bicyclists accessing Kaposia Landing or the Mississippi River Greenway are often seen travelling along the narrow roadway in the vehicle lanes because Bryant Avenue serves as either the main or one of few entrances to these amenities. Metro Transit Route 71 stops at the intersection of CSAH 156 and Bryant Avenue, contributing further to the large number of pedestrians attempting to cross the bridge. Due to the narrow lanes and geometry that results in a 90 degree turn on the bridge's east approach, entering the driving lanes can be very dangerous for pedestrians and bicyclists. There have been 20 vehicle crashes and one bicycle crash near the intersection of Bryant Avenue and Concord Street in the last ten years. The lack of dedicated facilities puts non-motorized users from nearby affordable housing units in greater danger of being involved in a collision.

The census tracts surrounding the Bryant Avenue Pedestrian Bridge are comprised of 37% people of color, 12% residents living with a disability, and 33% cost-burdened households. 10% of households do not have access to a personal vehicle and 36% of households only have one vehicle. The area has a median household income of \$52,750, far below the regional average.

Cost-burdened households and households with limited access to personal vehicles often rely on public transportation or walking and biking to access employment opportunities, daily needs and services, or recreational facilities. The installation of a mixed-use trail on the Bryant Avenue bridge will connect non-motorized users to downtown Saint Paul--a regional employment center--via the Mississippi River Greenway and the Robert Purum Trail. It will also connect residents of South St. Paul and Dakota County to over 7,000 industrial employment opportunities along CSAH 156. This will provide a safe way to access employment opportunities via walking or biking.

The project will not impose adverse health or environmental effects on equity populations. Project construction will incorporate proper noise, dust, and traffic mitigation as well as planned detour routes consistent with adopted City policies. The project requires no relocations of residences or businesses.

Measure C: Affordable Housing Access

Describe any affordable housing developments?existing, under construction, or planned?within ½ mile of the proposed project. The applicant should note the number of existing subsidized units, which will be provided on the Socio-Economic Conditions map. Applicants can also describe other types of affordable housing (e.g., naturally-occurring affordable housing, manufactured housing) and under construction or planned affordable housing that is within a half mile of the project. If applicable, the applicant can provide self-generated PDF maps to support these additions. Applicants are encouraged to provide a self-generated PDF map describing how a project connects affordable housing residents to destinations (e.g., childcare, grocery stores, schools, places of worship).

Describe the project?s benefits to current and future affordable housing residents within ½ mile of the project. Benefits must relate to affordable housing residents. Examples may include:

- ? specific direct access improvements for residents*
- ? improved access to destinations such as jobs, school, health care or other;*
- ? new transportation services or modal options;*
- ? and/or community connection and cohesion improvements.*

This is not an exhaustive list. Since residents of affordable housing are more likely not to own a private vehicle, higher points will be provided to roadway projects that include other multimodal access improvements. A full response will support the benefits claimed, identify benefits specific to residents of affordable housing, identify benefits addressing a transportation issue affecting residents of affordable housing specifically identified through engagement, and substantiate benefits with data.

Response:

There are 160 subsidized rental housing units within 0.5 miles of the project area. Within one mile of the project area, there are 447 subsidized rental units split between the following properties:

- Thompson Heights: Dakota County subsidized housing, 30 one-bedroom and 24 two-bedroom units available at 50% AMI
- River Heights Terrace: Dakota County and MHFA subsidized housing, 40 units available at 80% AMI
- Nan McKay Apartments: MHFA public housing, 132 one-bedroom units available at 30% AMI
- John Carroll Apartments: MHRA public housing, 165 one-bedroom and 1 two-bedroom units available at 30% AMI
- Dakota Heights: Dakota County subsidized housing, 31 one-bedroom and 25 two-bedroom units available at 80% AMI

Over 60% of South St. Paul's housing stock was built before 1960 and around 26% before 1939. This area of the city contains the highest concentration of 80+ years old housing, and Minnesota Compass estimates 74.1% of homes in this area were built in 1969 or earlier. In terms of owner-occupied housing, most homes (68.1%) in South St. Paul had an estimated market value of around \$243,500 or less in 2016. Although property values across the Twin Cities have continued to increase, many of the units found near the project area continue to be considered Natural Occurring Affordable Housing and therefore affordable to 80% AMI households.

Residents living in affordable housing units often rely on public transportation or walking and biking to access employment opportunities, daily needs and services, or recreational facilities. The installation of a mixed-use trail on the Bryant Avenue bridge will connect non-motorized users from the above-mentioned properties to downtown Saint Paul—a regional employment center—via the Mississippi River Greenway and the Robert Purum Trail. It will also connect residents to over 7,000 industrial employment opportunities along Concord Street N (CSAH 156). This will encourage non-motorized travel to daily needs and services as well as provide a safe way to access employment opportunities via walking or biking.

South St. Paul is committed to providing affordable housing options that benefit from regional trail connections, reliable access to transit, non-motorized transportation, employment centers, and educational facilities. Many of the residents living in the above units are likely single-vehicle or car-free households, increasing the importance of multimodal transportation options and efficient roadways. By providing pedestrian and bicycle facilities where they do not exist today, this project will increase connectivity and safety for cyclists and pedestrians from car-free households and encourage non-motorized travel to daily destinations.

Measure D: BONUS POINTS

Project is located in an Area of Concentrated Poverty:

Project's census tracts are above the regional average for population in poverty or population of color (Regional Environmental Justice Area): Yes

Project located in a census tract that is below the regional average for population in poverty or populations of color (Regional Environmental Justice Area):

Upload the "Socio-Economic Conditions" map used for this measure. 1702509665150_005_Socio-Economic Conditions Map.pdf

Measure A: Bikeway Network Gaps, Physical Barriers, and Continuity of Bicycle Facilities

PART 1: Qualitative assessment of project narrative discussing how the project will close a bicycle network gap, create a new or improved physical bike barrier crossing, and/or improve continuity and connections between jurisdictions.

Specifically, describe how the project would accomplish the following: Close a transportation network gap, provide a facility that crosses or circumvents a physical barrier, and/or improve continuity or connections between jurisdictions.

Bike system gap improvements include the following:

- Providing a missing link between existing or improved segments of a local transportation network or regional bicycle facility (i.e., regional trail or RBTN alignment);
- Improving bikeability to better serve all ability and experience levels by:
 - Providing a safer, more protected on-street facility or off-road trail;
 - Improving safety of bicycle crossings at busy intersections (e.g., through signal operations, revised signage, pavement markings, etc.); OR
 - Providing a trail adjacent or parallel to a highway or arterial roadway or improving a bike route along a nearby and parallel lower-volume neighborhood collector or local street.

Physical bicycle barrier crossing improvements include grade-separated crossings (over or under) of rivers and streams, railroad corridors, freeways and expressways, and multi-lane arterials, or enhanced routes to circumvent the barrier by channeling bicyclists to existing safe crossings or grade separations. Surface crossing improvements (at-grade) of major highway and rail barriers that upgrade the bicycle facility treatment or replace an existing facility at the end of its useful life may also be considered as bicycle barrier improvements. (For new barrier crossing projects, distances to the nearest parallel crossing must be included in the application to be considered for the full allotment of points under Part 1).

Examples of continuity/connectivity improvements may include constructing a bikeway across jurisdictional lines where none exists or upgrading an existing bicycle facility treatment so that it connects to and is consistent with an adjacent jurisdiction's bicycle facility.

Response:

The proposed project includes a grade-separated crossing over a Regional Bicycle Barrier, the Union Pacific Railroad Yard. The project will channel pedestrians and bicyclists onto a 12' wide paved and ADA-accessible multi-use trail, connecting South St. Paul and CSAH 156 to the Mississippi River Greenway/Robert Purum Trail. The proposed multi-use trail will provide a missing link between the existing local and regional trails within Kaposia Land Park and the sidewalk system west of CSAH 156.

The Mississippi River Greenway is a Tier 1 Bicycle and Trail Network alignment, extending 27 miles along the Mississippi River from St. Paul to Hastings. It connects South St. Paul and Dakota County to many regional destinations--including the employment and cultural centers of downtown Saint Paul. The project also connects residents west of the railroad yard to a RBTN Tier 1 corridor.

Six bridges in South St. Paul cross the railroad yard to access the Mississippi River Greenway. Three of these bridges do not include any pedestrian or bicycle facilities. South St. Paul residents must currently travel 0.6 miles north or 1.1 miles south of the proposed bridge to access the Mississippi River Greenway/Robert Purum Trail--a vital non-motorized connection between South St. Paul and downtown St. Paul, Kaposia Landing Park Area, and the industrial employment opportunities along CSAH 156.

The proposed project will also connect Metro Transit's regional bus system to the Mississippi River Greenway/Robert Purum Trail. The fixed-route bus line, Route 71, stops directly west of the project area at the intersection of Bryant Avenue and CSAH 156. This connection furthers the Metropolitan Council's goal of providing quality of multimodal travel options between economic and cultural destinations.

There is documented need for non-motorized facilities at this location. Users are often seen walking and biking within vehicle lanes to access the regional park and trail system. Because Bryant Avenue serves as the main entrance to Kaposia Landing and one of few entrances to the Mississippi River Greenway in South St. Paul, non-motorized users are forced to enter the roadway along the bridge. Due to the narrow lanes and geometry that result in a 90 degree turn on the bridge's east approach, entering the driving lanes can be very dangerous for pedestrians and bicyclists. There have been 20 vehicle crashes and one bicycle crash near the intersection of Bryant Avenue and Concord Street in the last ten years.

The Dakota County Pedestrian and Bicycle Study, the South St. Paul Bicycle and Pedestrian Plan, and the South St. Paul 2040 Comprehensive Plan update identify Bryant Avenue as a pedestrian gap and a top priority for the addition of non-motorized facilities.

(Limit 2,800 characters; approximately 400 words)

PART 2: Regional Bicycle Barrier Crossing Improvements and Major River Bicycle Barrier Crossings

DEFINITIONS:

Regional Bicycle Barrier Crossing Improvements include crossings of barrier segments within the ?Regional Bicycle Barrier Crossing Improvement Areas? as updated in the 2019 Technical Addendum to the Regional Bicycle Barriers Study and shown in the RBBS online map (insert link to forthcoming RBBS Online Map). Projects must create a new regional barrier crossing, replace an existing regional barrier crossing at the end of its useful life, or upgrade an existing barrier crossing to a higher level of bike facility treatment, to receive points for Part 2.

Major River Bicycle Barrier Crossings include all existing and planned highway and bicycle/pedestrian bridge crossings of the Mississippi, Minnesota and St. Croix Rivers as identified in the 2018 update of the 2040 Transportation Policy Plan. Projects must create a new major river bicycle barrier crossing, replace an existing major river crossing at the end of its useful life, or upgrade the crossing to a higher level of bike facility treatment, to receive points for Part 2.

Projects that construct new or improve existing Regional Bicycle Barrier Crossings or Major River Bicycle Barrier Crossings will be assigned points as follows: (select one)

Tier 1

Tier 1 Regional Bicycle Barrier Crossing Improvement Area segments & any Major River Bicycle Barrier Crossings

Tier 2

Tier 2 Regional Bicycle Barrier Crossing Improvement Area segments

Tier 3

Non-tiered

Yes

Crossings of non-tiered Regional Bicycle Barrier segments

No improvements

No Improvements to barrier crossings

If the project improves multiple regional bicycle barriers, check box.

Multiple

Projects that improve crossing of multiple regional bicycle barriers receive bonus points (except Tier 1 & MRBBCs)

Measure B: Deficiencies corrected or safety problems addressed

Response:

The intersection of CSAH 156 (Concord Street N) and Bryant Avenue just to the southeast of the proposed trail and bridge addition is one with high vehicles volumes and a significant crash history. This intersection and the Bryant Avenue bridge pose safety threats to pedestrians and bicycles who currently attempt to travel along the bridge.

CSAH 156 is an undivided two-lane roadway with a speed limit of 35 and an AADT of 9,100 (per a 2019 count). Bryant Avenue is also an undivided two-lane roadway with a past AADT record of 1,650 (draft records show a decreased count in 2021 was 1,109). Even with lower volumes and lower speeds, the needs for this stretch of road to have pedestrian facilities is critical. This roadway connects residential areas in South St. Paul to nearby parks and trails, including Kaposia Landing Park and the Mississippi River Greenway/Robert Purum Trail, an RBTN Tier 1 Alignment. With no paths or means to safely cross the bridge, bicyclists and pedestrians are left to use the roadway with vehicles otherwise unaware of their presence.

In the last ten years, one bicycle crash was recorded at the intersection of CSAH 156 and Bryant Avenue. The bicyclist involved in the crash was using the shoulder on CSAH 156 to cross at the traffic signal during a green phase. A vehicle turned right onto CSAH 156 and struck the bicyclist. In lieu of a proper trail or sidewalk, the bicyclist chose to use the wide shoulder to travel. Although the bicyclist was out of the vehicle lane and in a separated shoulder, the driver was not specifically looking out for pedestrians or bicyclists, as there are currently no dedicated facilities for non-motorized roadway users along Bryant Avenue. This accident demonstrates that even when pedestrians and bicyclists do their best to use separate facilities, accidents still occur. In the last three years, three crashes were reported at this intersection just southwest of the bridge (one being the previously mentioned bicycle crash). The other two crashes were an angle crash and a rear-end crash, resulting in both a minor injury and property damage only crash.

The railway under the existing vehicle bridge is a major barrier to both pedestrians and bicyclists. The alternative is to use the narrow bridge in conjunction with the vehicular traffic. The lanes on the bridge are narrow with no shoulder. The ends of the bridge are a signal and a sharp 90-degree turn. Both are barriers to safe travel. Pedestrians and bicyclists are unexpected on that facility and may be difficult to see. This geometry demonstrates the necessary of the proposed multi-use trail on the Bryant Avenue Bridge.

(Limit 2,800 characters; approximately 400 words)

Measure A: Multimodal Elements

Response:

The Bryant Avenue Pedestrian Bridge will connect residents of South St. Paul to a Tier 1 Bicycle and Trail Network alignment--the Mississippi River Greenway/Robert Purum Trail. This paved trail extends 27 miles along the Mississippi River from St. Paul to Hastings, connecting South St. Paul and Dakota County to many regional destinations. Bicycle connections are a priority of this project, given the bridge's proximity to the Mississippi River Greenway and the current lack of bicycle or pedestrian facilities. The project also connects residents west of the railroad yard to a RBTN Tier 1 corridor.

The proposed project will provide a 12' wide paved and ADA accessible connection via the Bryant Avenue bridge. Although it currently lacks non-motorized facilities, this bridge crossing is already widely used by pedestrians and bicyclists attempting to cross the railroad yard east of Concord Street North (CSAH 156)--a Regional Bicycle Barrier Segment.

The proposed connection will provide South St. Paul residents with non-motorized access to downtown St. Paul--a regional employment destination. The proposed bridge will improve South St. Paul's local network of trails and crossings, enabling access to local and regional destinations, providing better continuity, connecting existing dead-end trail segments, improving crossings of a Regional Bicycle Barrier Segment, and creating more connectivity to the regional trail network.

Currently, the only vehicular access to Kaposia Landing is at the south end of the park via Concord Street North (CSAH 156), by way of the Bryant Avenue bridge. There are no sidewalks or separated pedestrian accommodations on the bridge. The proposed bridge will provide direct access from the pedestrian crossing and bus stop at Concord Street over the railroad yard to Kaposia Landing and the Mississippi River Greenway.

There are six bridges in South St. Paul that cross the railroad yard to access the Mississippi River Greenway. Three of these bridges do not include any pedestrian or bicycle facilities. South St. Paul residents must currently travel 0.6 miles north or 1.1 miles south of the proposed bridge to access the Mississippi River Greenway/Robert Purum Trail--a vital non-motorized connection between South St. Paul and downtown St. Paul, Kaposia Landing Park Area, and the industrial employment opportunities along Concord Street N (CSAH 156).

Improved pedestrian accommodations will provide better access to the Landing's internal and regional trails. In turn, this will help to promote healthy lifestyles and improve non-vehicular access to the park. With the Mississippi River Greenway being a regional connection, additional users will continue to originate from beyond the borders of South St. Paul and Dakota County.

(Limit 2,800 characters; approximately 400 words)

Upload Transit map

1702564991842_007_Transit Connections Map.pdf

Transit Projects Not Requiring Construction

If the applicant is completing a transit application that is operations only, check the box and do not complete the remainder of the form. These projects will receive full points for the Risk Assessment.

Park-and-Ride and other transit construction projects require completion of the Risk Assessment below.

Measure A: Risk Assessment - Construction Projects

1. Public Involvement (20 Percent of Points)

Projects that have been through a public process with residents and other interested public entities are more likely than others to be successful. The project applicant must indicate that events and/or targeted outreach (e.g., surveys and other web-based input) were held to help identify the transportation problem, how the potential solution was selected instead of other options, and the public involvement completed to date on the project. The focus of this section is on the opportunity for public input as opposed to the quality of input. NOTE: A written response is required and failure to respond will result in zero points.

Multiple types of targeted outreach efforts (such as meetings or online/mail outreach) specific to this project with the general public and partner agencies have been used to help identify the project need. Yes

100%

At least one meeting specific to this project with the general public has been used to help identify the project need.

50%

At least online/mail outreach effort specific to this project with the general public has been used to help identify the project need.

50%

No meeting or outreach specific to this project was conducted, but the project was identified through meetings and/or outreach related to a larger planning effort.

25%

No outreach has led to the selection of this project.

0%

Describe the type(s) of outreach selected for this project (i.e., online or in-person meetings, surveys, demonstration projects), the method(s) used to announce outreach opportunities, and how many people participated. Include any public website links to outreach opportunities.

Response:

The City of South St. Paul was successful in securing a Statewide Health Improvement Partnership grant to evaluate the design and funding for a dedicated pedestrian access from Concord Street (CSAH 156) to the Kaposia Landing Park. All stakeholder and community engagement aligned with SHIP funding requirements.

In June 2021, the City hosted an online engagement opportunity that included draft study report, a video presentation on the study, and a public comment form. The event was advertised through the city website, newsletter, social media accounts, and email lists. Flyers for the study were posted at Kaposia Landing and on the Mississippi River Greenway trail. The project team received 37 responses on the survey, 95% of which were in favor of the bridge. Input from the survey was used to directly influence study goals and will be used to influence the design of the bridge.

Moving forward, the South St. Paul plans to conduct further engagement during design and construction phases, including an open house hosted at an accessible community destination and specific outreach to residents of two low-income, high-rise facilities operated by the City--Nan McKay Apartments and the John Carroll Building.

The need for pedestrian and bicycle facilities along the Bryant Avenue Bridge is identified multiple times in the South St. Paul 2040 Comprehensive Plan. The 2040 planning process included six engagement events and a community survey during 2017. Throughout the planning process, the City received feedback from more than 330 members of the community. The input gathered from these efforts led to the formulation of the goals and policies found throughout this Comprehensive Plan update.

The Bryant Avenue Bridge was identified as a pedestrian gap in the Dakota County Pedestrian and Bicycle Study. Two community engagement events were held during this study (7/2017 & 10/2017). In total, there were more than twenty engagement activities with more than 875 people engaged. Events were conducted using various formats including an open house, pop-up style events, listening sessions, emails and phone calls, intercept flyers, and an online survey. The project team was especially mindful of reaching stakeholder groups who are most likely to experience health disparities, such as people over 60, children, people with low incomes, minorities, and other underrepresented populations.

The South St. Paul Bicycle and Pedestrian Plan also identifies a trail gap on the Bryant Avenue Bridge and proposed the addition of non-motorized facilities. Engagement for this study includes a traveling pop-up booth, website and social media updates, a survey, and six community outreach events. The project team collected in-person feedback from 87 residents and 50 survey responses.

(Limit 2,800 characters; approximately 400 words)

2. Layout (25 Percent of Points)

Layout includes proposed geometrics and existing and proposed right-of-way boundaries. A basic layout should include a base map (north arrow; scale; legend; city and/or county limits; existing ROW, labeled; existing signals;* and bridge numbers*) and design data (proposed alignments; bike and/or roadway lane widths; shoulder width;* proposed signals;* and proposed ROW). An aerial photograph with a line showing the project's termini does not suffice and will be awarded zero points. *If applicable*

Layout approved by the applicant and all impacted jurisdictions (i.e., cities/counties/MnDOT. If a MnDOT trunk highway is impacted, approval by MnDOT must have occurred to receive full points. A PDF of the layout must be attached along with letters from each jurisdiction to receive points. Yes

100%

A layout does not apply (signal replacement/signal timing, stand-alone streetscaping, minor intersection improvements). Applicants that are not certain whether a layout is required should contact Colleen Brown at MnDOT Metro State Aid ? colleen.brown@state.mn.us.

100%

For projects where MnDOT trunk highways are impacted and a MnDOT Staff Approved layout is required. Layout approved by the applicant and all impacted local jurisdictions (i.e., cities/counties), and layout review and approval by MnDOT is pending. A PDF of the layout must be attached along with letters from each jurisdiction to receive points.

75%

Layout completed but not approved by all jurisdictions. A PDF of the layout must be attached to receive points.

50%

Layout has been started but is not complete. A PDF of the layout must be attached to receive points.

25%

Layout has not been started

0%

Attach Layout

1702510958450_003_Concept Drawing.pdf

Please upload attachment in PDF form

Additional Attachments

Please upload attachment in PDF form

3. Review of Section 106 Historic Resources (15 Percent of Points)

No known historic properties eligible for or listed in the National Register of Historic Places are located in the project area, and project is not located on an identified historic bridge

Yes

100%

There are historical/archeological properties present but determination of ?no historic properties affected? is anticipated.

100%

Historic/archeological property impacted; determination of ?no adverse effect? anticipated

80%

Historic/archeological property impacted; determination of ?adverse effect? anticipated

40%

Unsure if there are any historic/archaeological properties in the project area.

0%

Project is located on an identified historic bridge

4. Right-of-Way (25 Percent of Points)

Right-of-way, permanent or temporary easements, and MnDOT agreement/limited-use permit either not required or all have been acquired

Yes

100%

Right-of-way, permanent or temporary easements, and/or MnDOT agreement/limited-use permit required - plat, legal descriptions, or official map complete

50%

Right-of-way, permanent or temporary easements, and/or MnDOT agreement/limited-use permit required - parcels identified

25%

Right-of-way, permanent or temporary easements, and/or MnDOT agreement/limited-use permit required - parcels not all identified

0%

5. Railroad Involvement (15 Percent of Points)

No railroad involvement on project or railroad Right-of-Way agreement is executed (include signature page, if applicable)

Yes

100%

Signature Page

Please upload attachment in PDF form

Railroad Right-of-Way Agreement required; negotiations have begun

50%

Railroad Right-of-Way Agreement required; negotiations have not begun.

0%

Measure A: Cost Effectiveness

| | |
|--|----------------|
| Total Project Cost (entered in Project Cost Form): | \$6,200,000.00 |
| Enter Amount of the Noise Walls: | \$0.00 |
| Total Project Cost subtract the amount of the noise walls: | \$6,200,000.00 |
| Points Awarded in Previous Criteria | |

Other Attachments

| File Name | Description | File Size |
|--|--|-----------|
| 001_One Page Description.pdf | One Page Project Summary | 443 KB |
| 002_Existing Condition Photos.pdf | Existing Conditions Photos | 714 KB |
| 003_Concept Drawing.pdf | Concept Drawing of Proposed Improvements | 460 KB |
| 004_Population-Employment Summary Map.pdf | Population and Employment Summary Map | 3.4 MB |
| 005_Socio-Economic Conditions Map.pdf | Socio-Economic Conditions Map | 2.3 MB |
| 006_RBTN Orientation Map.pdf | RBTN Orientation Map | 1.9 MB |
| 007_Transit Connections Map.pdf | Transit Connections Map | 1.6 MB |
| 008_City Council Resolution.pdf | City of South St. Paul Council Resolution | 863 KB |
| 009_Letters of Support.pdf | Additional Letters of Support | 1.6 MB |
| 010_Bryant Ave Ped Bridge Study_Chapter 1-6.pdf | Bryant Ave Ped Bridge Study: Chapter 1-6 | 646 KB |
| 011_Bryant Ave Ped Bridge Study_Chapter 7-12.pdf | Bryant Ave Ped Bridge Study: Chapter 7-12 | 458 KB |
| 012_Bryant Ave Ped Bridge Study_Appendix A.pdf | Bryant Ave Ped Bridge Study: Appendix A | 1.1 MB |
| 013_Bryant Ave Ped Bridge Study_Appendix B.pdf | Bryant Ave Ped Bridge Study: Appendix B | 1.8 MB |
| 014_Bryant Ave Ped Bridge Study_Appendix C.pdf | Bryant Ave Ped Bridge Study: Appendix C | 445 KB |
| 015_Bryant Ave Ped Bridge Study_Appendix D.pdf | Bryant Ave Ped Bridge Study: Appendix D | 450 KB |
| 016_Bryant Ave Ped Bridge Study_Appendix E.pdf | Bryant Ave Ped Bridge Study: Appendix E | 511 KB |
| 017_UPRR Easement_Bryant Ave Ped Bridge Study_Appendix F.pdf | Bryant Ave Ped Bridge Study: Appendix F | 14.5 MB |
| 018_Bryant Ave Ped Bridge Study_Appendix G.pdf | Bryant Ave Ped Bridge Study: Appendix G | 1.2 MB |
| 019_Bryant Ave Ped Bridge Study_Appendix H.pdf | Bryant Ave Ped Bridge Study: Appendix H | 124 KB |
| 020_South St. Paul Bicycle and Pedestrian Plan.pdf | South St. Paul Bicycle and Pedestrian Plan | 2.5 MB |
| 021_Dakota County Pedestrian and Bicycle Study.pdf | Dakota County Pedestrian and Bicycle Study | 3.2 MB |
| 022_Mississippi River Trail Map.pdf | Mississippi River Trail Map | 1.0 MB |
| 023_South St. Paul Comprehensive Plan.pdf | South St. Paul Comprehensive Plan | 2.2 MB |
| 024_ADA Transition Plan.pdf | ADA Transition Plan | 1.8 MB |

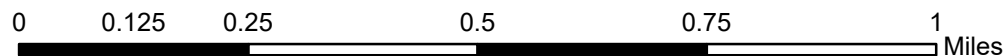
Project to RBTN Orientation

Multiuse Trails and Bicycle Facilities Project: Bryant Avenue Pedestrian Bridge | Map ID: 1701710172834

Pigs Eye



- Project
- RBTN Tier 2 Alignment
- Railroads
- RBTN Corridor Centerlines
- Principal Arterials
- RBTN Tier 1 Alignment
- Minor Arterials
- RBTN Tier 1
- RBTN Tier 2



Created: 12/4/2023
LandscapeRSA6



For complete disclaimer of accuracy, please visit
<https://giswebsite.metc.state.mn.us/gissite/notice.aspx>

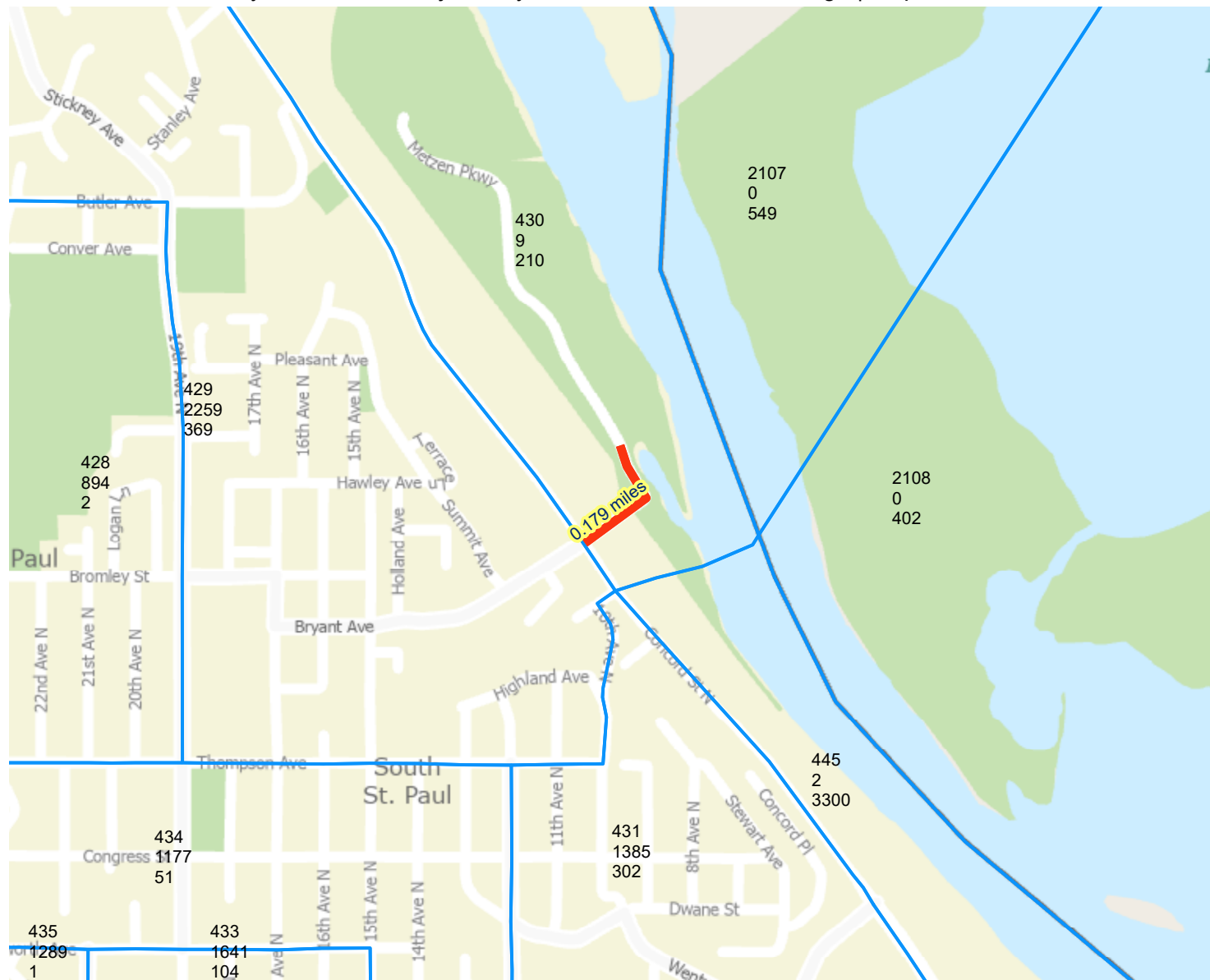


Population/Employment Summary

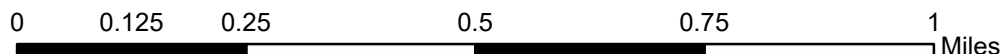
Results

Within ONE Mile of project:
Total Population: 13495
Total Employment: 7106

Multiuse Trails and Bicycle Facilities Project: Bryant Avenue Pedestrian Bridge | Map ID: 1701710172834



- Project Points
- Project Area
- Project
- 2016 TAZ



Created: 12/4/2023
LandscapeRSA4



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Socio-Economic Conditions

Multiuse Trails and Bicycle Facilities Project: Bryant Avenue Pedestrian Bridge | Map ID: 1701710172834

Results

Total of publicly subsidized rental housing units in census tracts within 1/2 mile: 263

Project located in census tract(s) that are ABOVE the regional average for population in poverty or population of color.



— Lines

Area of Concentrated Poverty

Regional Environmental Justice Area

0 0.15 0.3 0.6 0.9 1.2 Miles

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LandscapeRSA2



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Transit Connections

Multiuse Trails and Bicycle Facilities Project: Bryant Avenue Pedestrian Bridge | Map ID: 1701710172834

Results

Transit with a Direct Connection to project:
71

**indicates Planned Alignments*

Transit Market areas: 3

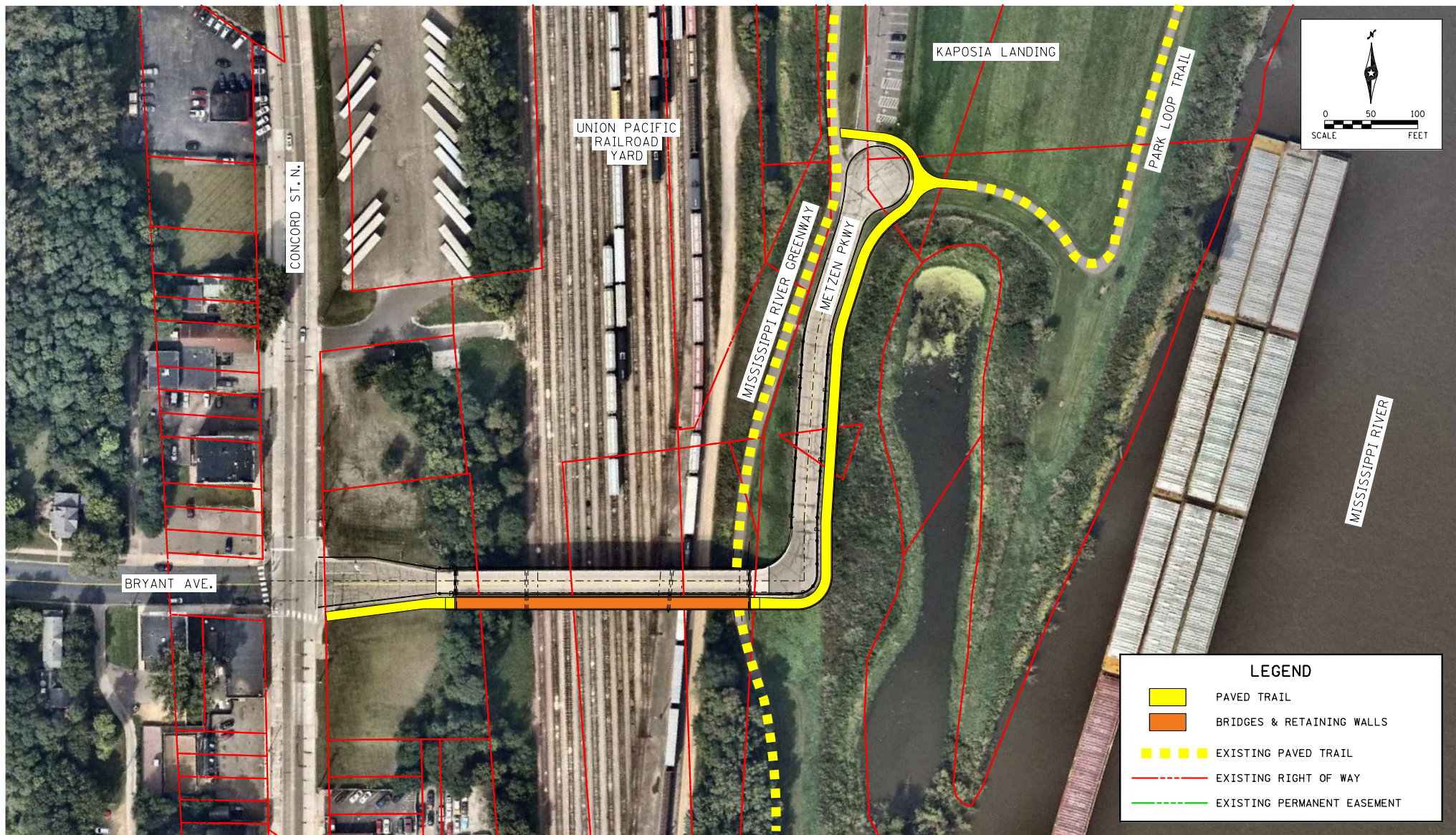




Real People. Real Solutions.

Proposed Geomentry: Bridge No. 19544 Substructure Modificattion for Separate Pedestrian Bridge Deck

Bryant Avenue Pedestrian Bridge
City of South St. Paul



BRYANT AVENUE PEDESTRIAN BRIDGE

South St. Paul, Minnesota



Project Name: Bryant Avenue Pedestrian Bridge

Applicant: City of South St. Paul

Primary Contact:

Nicholas Guilliams, P.E.
City Engineer
City of South St. Paul
125 3rd Avenue N
South St. Paul, MN 55075

(651) 554-3214

nguilliams@southstpaul.org



Location & Route:

Bryant Avenue Bridge (Bridge No. 19544)



Application Category:

Multi-use Trails and Bicycle Facilities



Funding Information:

Requested Award Amount:

\$4,960,000

Local Match: \$1,240,000

Construction Cost Total:

\$6,200,000



Corridor Fast Facts:

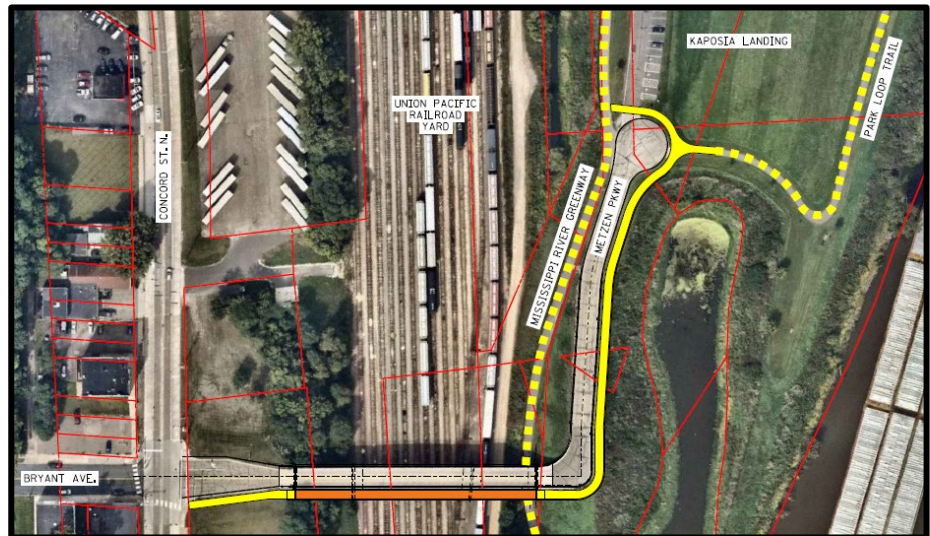
- Connects to a Tier 1 Regional Bicycle Trail Network Alignment – the Mississippi River Greenway Trail
- Prioritized in the Dakota County Pedestrian and Bicycle Study and the South St. Paul Bicycle and Pedestrian Plan.
- Previously awarded a Statewide Health Improvement Partnership (SHIP) grant.

Project Description

The Bryant Avenue Pedestrian Bridge will provide dedicated pedestrian and bicycle access along a Tier 2 Bicycle and Trail Network alignment from Concord Street (CSAH 156) to the Mississippi River Greenway/Robert Purum Trail and Kaposia Landing Park area. The proposed project is a 12' wide paved and ADA accessible multi-use trail constructed along the existing bridge. Although it currently lacks non-motorized facilities, this bridge crossing is already widely used by pedestrians and bicyclists attempting to cross the railroad yard east of CSAH 156 – a Regional Bicycle Barrier Segment.

Regional Significance

The installation of a mixed-use trail will connect non-motorized users to downtown Saint Paul – a regional employment center – via the Mississippi River Greenway and the Robert Purum Trail. This trail is a Tier 1 Bicycle and Trail Network alignment, extending 27 miles along the Mississippi River from St. Paul to Hastings. It will also connect residents to over 7,000 industrial employment opportunities along CSAH 156. This will encourage non-motorized travel to daily needs and services as well as provide a safe way to access employment opportunities via walking or biking.



Documented Need

There is documented need for dedicated pedestrian and bicycle facilities along the Bryant Avenue Bridge. Non-motorized users accessing Kaposia Landing or the Mississippi River Greenway are often seen travelling along the narrow roadway in the vehicle lanes. Because Bryant Avenue serves as the main entrance to Kaposia Landing and one of few entrances to the Mississippi River Greenway in South St. Paul, non-motorized users are forced to enter the vehicle lanes along the bridge. Metro Transit Route 71 stops at the intersection of CSAH 156 and Bryant Avenue, contributing further to the large number of pedestrians attempting to cross the bridge. Due to the narrow lanes and geometry that results in a 90 degree turn on the bridge's east approach, entering the driving lanes can be very dangerous for pedestrians and bicyclists.



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VIEW 1: WEST BRIDGE APPROACH LOOKING EAST TOWARD BRIDGE



VIEW 3: SOUTHWEST BRIDGE APPROACH LOOKING SOUTHWEST FROM END OF APPROACH PANEL



VIEW 2: WEST BRIDGE APPROACH LOOKING WEST FROM BRIDGE



VIEW 4: NORTHWEST BRIDGE APPROACH LOOKING NORTHWEST FROM END OF APPROACH BRIDGE



Job #13908
2/27/2021

Site Photos

Bryant Avenue Pedestrian Bridge Feasibility Study
for the City of South St. Paul

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VIEW 5: RAILROAD TRACKS SOUTH OF BRIDGE LOOKING SOUTH FROM BRIDGE



VIEW 7: TYPICAL BRIDGE DECK LOOKING WEST FROM EAST APPROACH PANEL



VIEW 6: RAILROAD TRACKS NORTH OF BRIDGE LOOKING NORTH FROM BRIDGE



VIEW 8: EAST BRIDGE APPROACH LOOKING NORTH FROM INTERSECTION OF XXX



Job #13908
2/27/2021

Site Photos

Bryant Avenue Pedestrian Bridge Feasibility Study
for the City of South St. Paul

pw:\\srf-pw.bentley.com:srf-pw\Documents\Projects\13000\13918\02_Structures\Plan_Struc\13918_AppA_site.dgn



VIEW 9: VIEW OF BRIDGE FROM NORTHEAST OF BRIDGE LOOKING SOUTHWEST



VIEW11: VIEW OF EAST APPROACH EMBANKMENT LOOKING SOUTH



VIEW 10: VIEW OF EAST APPROACH LOOKING SOUTH FROM ROUNDABOUT NEAR PARKING LOT



VIEW 12: VIEW OF EAST APPROACH EMBANKMENT LOOKING NORTH



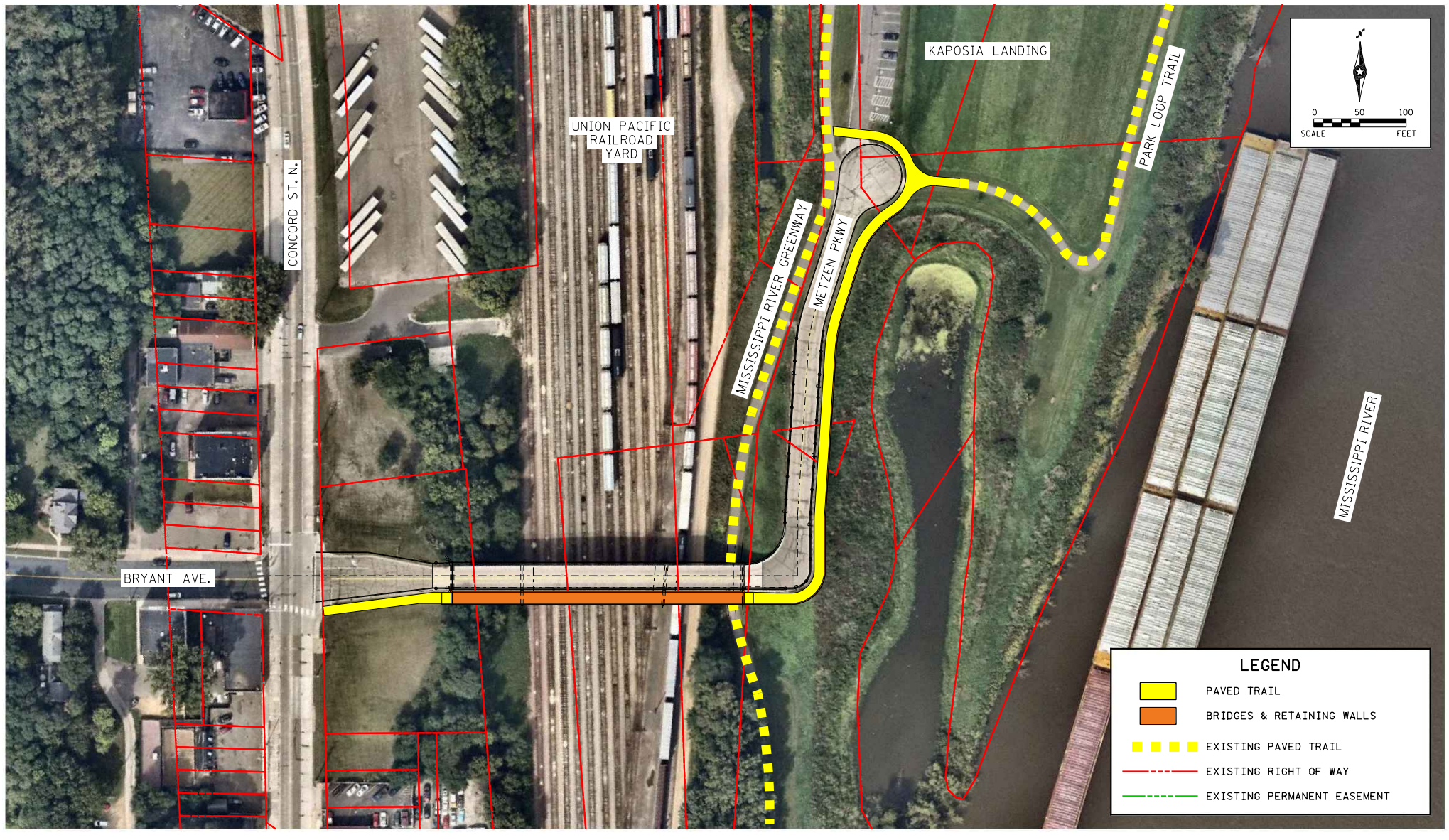
Job #13908
2/27/2021

Site Photos

Bryant Avenue Pedestrian Bridge Feasibility Study
for the City of South St. Paul

Proposed Geomentry: Bridge No. 19544 Substructure Modificattion for Separate Pedestrian Bridge Deck

Bryant Avenue Pedestrian Bridge
City of South St. Paul

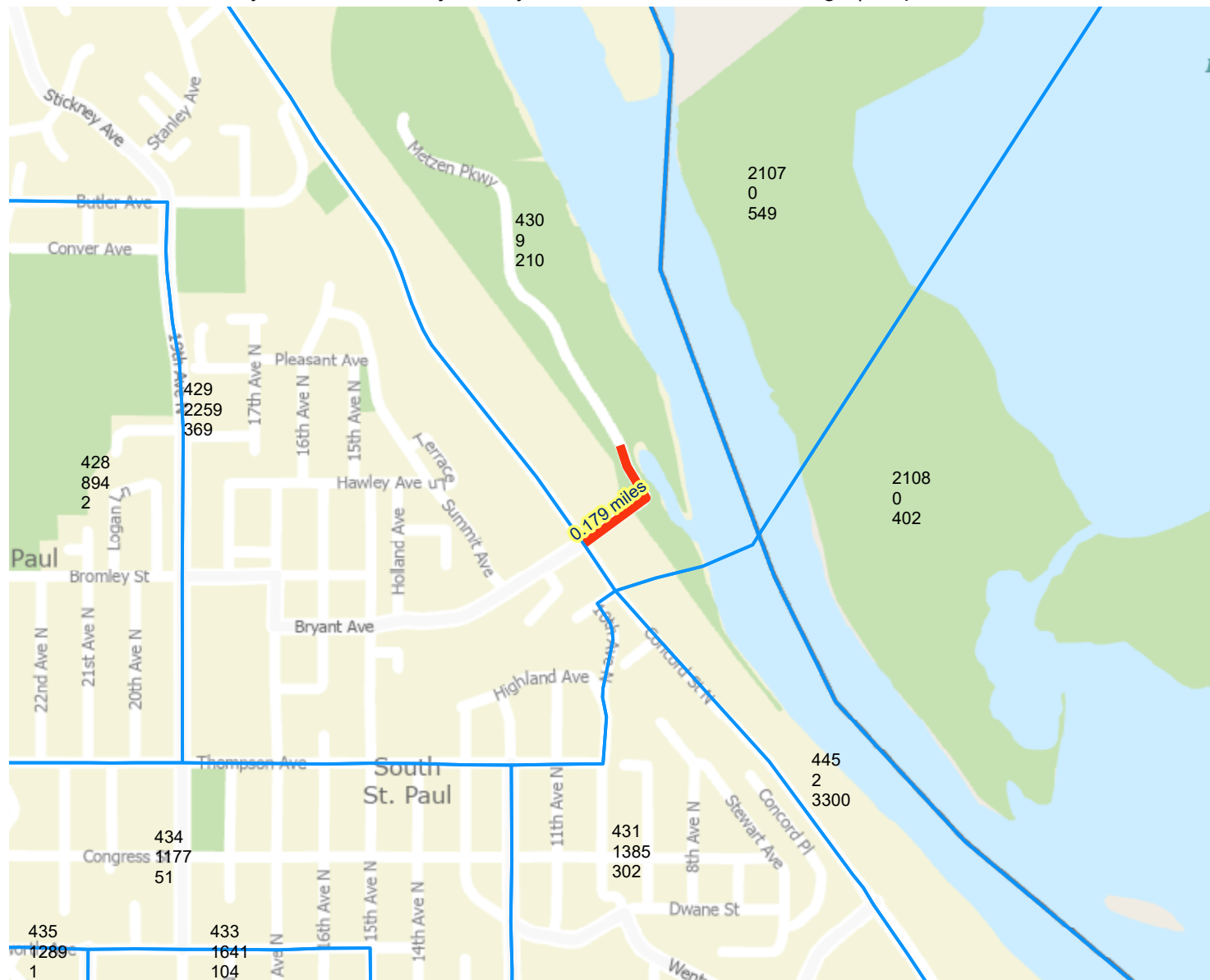


Population/Employment Summary

Results

Within ONE Mile of project:
Total Population: 13495
Total Employment: 7106

Multiuse Trails and Bicycle Facilities Project: Bryant Avenue Pedestrian Bridge | Map ID: 1701710172834



- Project Points
- Project Area
- Project
- 2016 TAZ

0 0.125 0.25 0.5 0.75 1 Miles

Created: 12/4/2023
LandscapeRSA4



For complete disclaimer of accuracy, please visit
<https://giswebsite.metc.state.mn.us/gissite/notice.aspx>



Socio-Economic Conditions

Multiuse Trails and Bicycle Facilities Project: Bryant Avenue Pedestrian Bridge | Map ID: 1701710172834

Results

Total of publicly subsidized rental housing units in census tracts within 1/2 mile: 263

Project located in census tract(s) that are ABOVE the regional average for population in poverty or population of color.



Project to RBTN Orientation

Multiuse Trails and Bicycle Facilities Project: Bryant Avenue Pedestrian Bridge | Map ID: 1701710172834

Pigs Eye



- Project
- RBTN Tier 2 Alignment
- Railroads
- RBTN Corridor Centerlines
- Principal Arterials
- RBTN Tier 1 Alignment
- Minor Arterials
- RBTN Tier 1
- RBTN Tier 2

0 0.125 0.25 0.5 0.75 1 Miles

Created: 12/4/2023
LandscapeRSA6



For complete disclaimer of accuracy, please visit
<https://giswebsite.metc.state.mn.us/gissite/notice.aspx>



Transit Connections

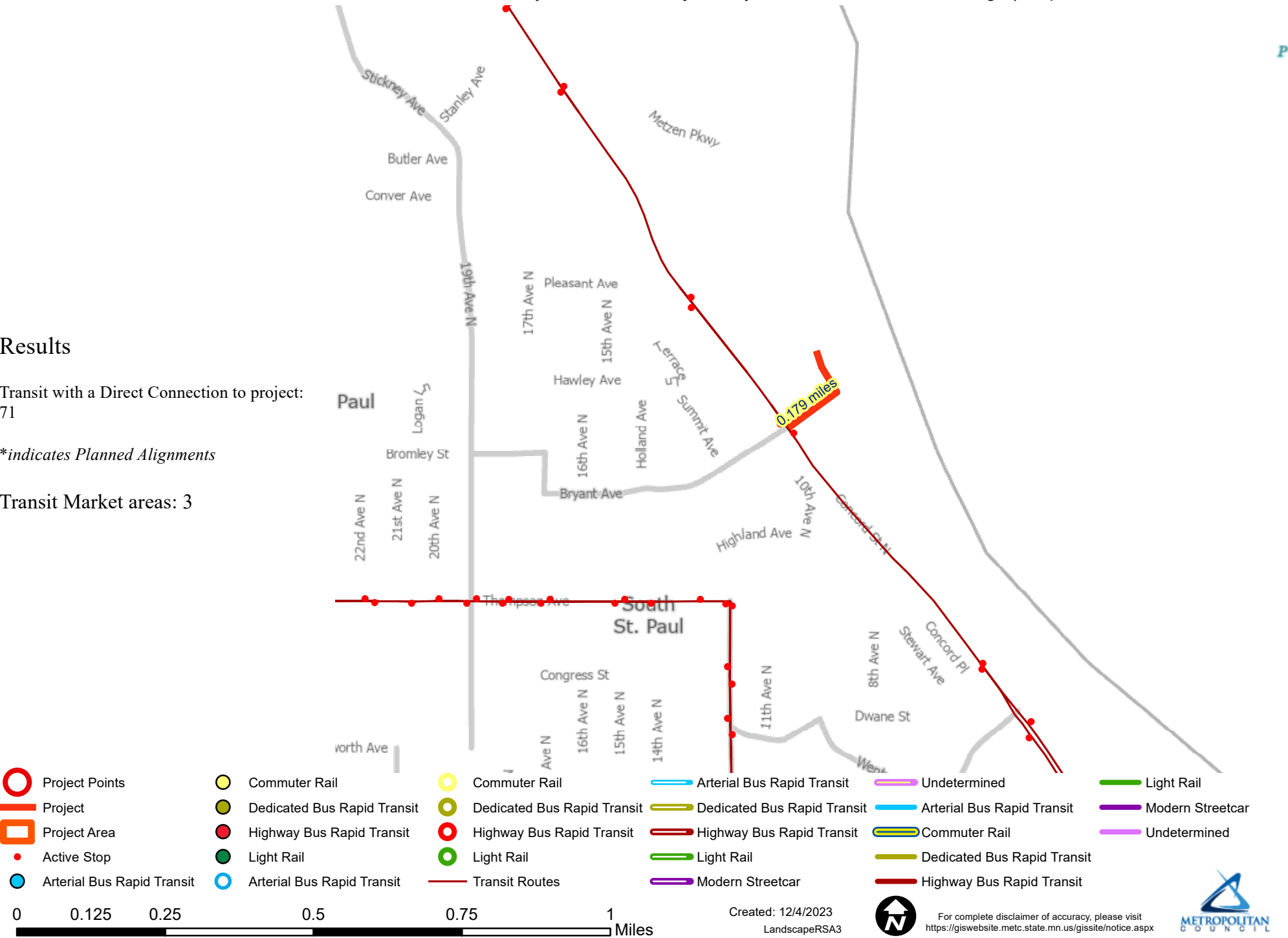
Multiuse Trails and Bicycle Facilities Project: Bryant Avenue Pedestrian Bridge | Map ID: 1701710172834

Results

Transit with a Direct Connection to project:
71

**indicates Planned Alignments*

Transit Market areas: 3



**City of South St. Paul
Dakota County, Minnesota**

CERTIFICATION

The undersigned, City Clerk of the City of South St. Paul, Minnesota (the City), hereby certifies as follows:

Attached hereto is a true and correct copy of Resolution No. 2023-177 duly adopted by the City Council of the City at a lawful meeting duly called on November 20, 2023, at which meeting a quorum was present and acting throughout, which resolution remains in full force and effect in the form which adopted.

WITNESS my hand and the corporate seal of the city this 20th day of November, 2023.

Deanna Werel
City Clerk

Seal

City of South St. Paul
Dakota County, Minnesota

RESOLUTION NO. 2023-177
RESOLUTION FOR APPROVAL OF 2023 REGIONAL SOLICITATION
APPLICATION SUBMITTAL

WHEREAS, the City of South St. Paul values safety and accessible transportation options and encourages the use of non-motorized modes of transportation like walking and biking; and

WHEREAS, a pedestrian and bicycle bridge over Bryant Avenue is needed to promote safety and accessibility following the City's adopted Bicycle and Pedestrian Plan, and

WHEREAS, the estimated cost of the project is \$6.2 million; and

WHEREAS, the addition of pedestrian and bicycle facilities along the Bryant Avenue Bridge will provide access on a Regional Bike Transportation (RBTN) Tier 2 Alignment through the heart of South St. Paul from Concord Street to the regional trail system and Kaposia Landing.

NOW, THEREFORE, BE IT RESOLVED AS FOLLOWS:

1. The City Council of South St. Paul hereby authorizes and supports the application made to the 2023 Regional Solicitation for Multiuse Trails and Bicycle Facilities funding to construct a much-needed pedestrian and bicycle connection over Bryant Avenue, and
2. The City of South St. Paul, if awarded funding for the new bridge, hereby agrees to accept the grant award, enter into an agreement for Multiuse Trails and Bicycle Facilities funding for the above-referenced project, and provide the required local match.
3. The City of South St. Paul will comply with all applicable laws, environmental requirements and regulations as stated in the grant agreement.
4. The City of South St. Paul hereby assures the facility will be maintained on a year-round basis.

Adopted this 20th day of November, 2023.



City Clerk

Rick Hansen
State Representative

District 53B



Minnesota House of Representatives

November 15, 2023

Nick Guiliams, P.E.
City Engineer
City of South St. Paul
125 3rd Avenue N
South St. Paul, MN 55075

RE: Letter of Support for City of South St. Paul's 2023 Regional Solicitation Application to fund the Bryant Avenue Pedestrian Bridge

Dear Mr. Guiliams,

I have been notified that the City of South St. Paul is applying for funding as part of the 2023 Regional Solicitation through the Metropolitan Council. The proposed project, a multi-use trail, will connect a RBTN Tier 2 Alignment – the Mississippi River Greenway/Robert Pira Regional Trail, a regional park – Kaposia Landing Park Area, the Metro Transit Regional Bus System via Route 71, and over 7,000 employment opportunities via the Bryant Avenue bridge.

This project will provide a pedestrian and bicycle trail to Kaposia Landing Park Area where none existing today. Currently, there are no dedicated non-motorized facilities leading into the south side of the park area. This specific project will greatly improve the safety of crossing a busy and important transportation corridor, CSAH 156, to connect Dakota County residents to Kaposia Landing Park Area and a Tier 1 RBTN alignment, the Mississippi River Greenway/Robert Pira Regional Trail. With the Mississippi River Greenway being a regional connection, additional users will continue to originate from beyond the borders of South St. Paul and Dakota County.

As you know, the City of South St. Paul was recently awarded Statewide Health Improvement Partnership (SHIP) funding to evaluate the proposed pedestrian and bicycle access and is now attempting to secure funding to the construction of this project. Throughout the evaluation process, the city has demonstrated commitment to bringing an important trail connection to the residents of South St. Paul and Dakota County. I appreciate the continued efforts by the City of South St. Paul to make these project benefits real to the local community and region.

I fully support the funding application for this necessary and timely project. I look forward to working with the City of South St. Paul to deliver a pedestrian and bicycle trail crossing where one does not currently exist.

Sincerely,

A handwritten signature in black ink, appearing to read "Rick Hansen", with a long horizontal flourish extending to the right.

Rick Hansen
State Representative, 53B



City of South St. Paul

Office of the Mayor
James P. Francis

November 27, 2023

Attn: Nick Guilliams, City Engineer
City South St. Paul
125 Third Avenue N
South St. Paul, MN 55075

RE: Letter of Support for City of St. Paul's 2023 Regional Solicitation Application to fund the Bryant Avenue Pedestrian Bridge

Dear Mr. Guilliams,

Thank you for leading the effort to fund the necessary and long overdue addition of pedestrian and bicycle facilities on the Bryant Avenue Bridge connecting South St. Paul to Kaposia Landing Park Area and a Tier 2 RBTN alignment, the Mississippi River Greenway. I am aware of and support the city's application to the Metropolitan Council's Regional Solicitation.

As you know, the City of South St. Paul has studied and been planning to add pedestrian and bicycle facilities to this corridor for many years. The city has already been awarded Statewide Health Improvement Partnership (SHIP) funding to evaluate the proposed pedestrian and bicycle access and is now attempting to secure funding to the construction of this project. Throughout the evaluation process, the city has demonstrated commitment to bringing an important trail connection to the residents of South St. Paul and Dakota County. We recognize that the proposed addition of pedestrian and bicycle facilities will increase access to a RBTN Tier 1 Alignment – the Mississippi River Greenway/Robert Pira Regional Trail, a regional park – Kaposia Landing Park Area, the Metro Transit Regional Bus System via Route 71, and over 7,000 employment opportunities.

I am looking forward to delivering the proposed project. I know the City of South St. Paul is capable and committed to constructing pedestrian and bicycle facilities along the Bryant Avenue Bridge. Thank you again for your efforts in making this project a reality.

Sincerely,

James P. Francis, Mayor
City of South St. Paul



**MnDOT Metro District
1500 West County Road B-2
Roseville, MN 55113**

April 12, 2022

Ross Tillman
Bolton & Menk, Inc, on behalf of,
City of Victoria

**Re: MnDOT Letter for City of Victoria's Metropolitan Council/Transportation Advisory Board
2022 Regional Solicitation Funding Request for the 78th Street Pedestrian Grade Separation project**

Ross Tillman,

This letter documents MnDOT Metro District's recognition for City of Victoria to pursue funding for the Metropolitan Council/Transportation Advisory Board's (TAB) 2022 Regional Solicitation for the 78th Street Pedestrian Grade Separation project.

As proposed, this project impacts MnDOT right-of-way on TH 5 Victoria. As the agency with jurisdiction over TH 5, MnDOT will allow the City to seek improvements proposed in the application. Details of any future maintenance agreement will need to be determined during project development to define how the improvements will be maintained for the project's useful life if the project receives funding.

There is no funding from MnDOT currently planned or programmed for this improvement. If your project receives funding, continue to work with MnDOT Area staff to coordinate needs and opportunities for cooperation.

MnDOT Metro District looks forward to continued cooperation with Victoria as this project moves forward and as we work together to improve safety and travel options within the Metro Area.

If you have questions or require additional information at this time, please reach out to South Area Manager Ryan Wilson at Ryan.wilson@state.mn.us.

Sincerely,

Michael Barnes, PE
Metro District Engineer

CC: Ryan Wilson, Metro District Area Manager; Dan Erickson, Metro State Aid Engineer; Molly McCartney, Metro Program Director



April 7, 2022

Sue Polka, P.E.
City Engineer
City of South St. Paul
125 3rd Avenue N
South St. Paul, MN 55075

RE: City of South St. Paul's 2022 Regional Solicitation Application for the Bryant Avenue Pedestrian Bridge

Dear Ms. Polka:

Dakota County has been notified that the City of South Saint Paul is applying for funding through the Metropolitan Council's 2022 Regional Solicitation. The proposed project, a multi-use trail, will connect a Regional Bicycle Transportation Network (RBTN) Tier 1 Alignment – the Mississippi River Greenway/Robert Pira Regional Trail, a regional park – Kaposia Landing Park Area, the Metro Transit Regional Bus System via Route 71, and over 7,000 employment opportunities via the Bryant Avenue bridge.

This project will provide a pedestrian and bicycle trail between Concord Boulevard and Kaposia Landing Park Area. Currently, a dedicated non-motorized facility leading into the south side of the park does not exist and people must walk or bike in the travel lanes of the narrow Bryant Avenue bridge. By providing a dedicated space for pedestrians and bicyclists, the project will greatly improve the safety of crossing a railroad barrier to connect residents to destinations such as the park, as well as the regional trail and bikeway network.

Dakota County recognizes that the City of South St. Paul was recently awarded Statewide Health Improvement Partnership (SHIP) funding to evaluate the proposed pedestrian and bicycle access and is now attempting to secure funding for the construction of this project. Throughout the evaluation process, the city has demonstrated commitment to bringing an important trail connection to the residents of South St. Paul and Dakota County. We appreciate the continued efforts by the City of South St. Paul to bring these project benefits to the local community and region.

If funded, this project would provide an improved connection to the Mississippi River Greenway, an important regional trail for Dakota County that serves residents of the County and broader region.

Best Regards,

A handwritten signature in black ink, reading "Kathleen A. Gaylord".

Kathleen A. Gaylord, Chair
Dakota County Board of Commissioners

Kathleen Gaylord

Commissioner – District 2

O 651-438-4428 **F** 651-438-4405 **E** Kathleen.gaylord@co.dakota.mn.us **W** www.dakotacounty.us
A Dakota County Administration Center • 1590 Highway 55 • Hastings • MN 55033



April 4, 2022

Sue Polka, P.E.
City Engineer
City of South St. Paul
125 3rd Avenue N
South St. Paul, MN 55075

RE: Letter of Support for City of St. Paul's 2022 Regional Solicitation Application to fund the Bryant Avenue Pedestrian Bridge

Dear Ms. Polka,

Dakota County has been notified that the City of South Saint Paul is submitting an application for funding as part of the 2022 Regional Solicitation through the Metropolitan Council. The proposed project, a multi-use trail, will connect a RBTN Tier 1 Alignment – the Mississippi River Greenway/Robert Pira Regional Trail, a regional park – Kaposia Landing Park Area, the Metro Transit Regional Bus System via Route 71, and over 7,000 employment opportunities via the Bryant Avenue bridge.

This project will provide a pedestrian and bicycle trail to Kaposia Landing Park Area where none existing today. Currently, there are no dedicated non-motorized facilities leading into the south side of the park area. This specific project will greatly improve the safety of crossing a busy and important transportation corridor, CSAH 156, to connect Dakota County residents to Kaposia Landing Park Area and a Tier 1 RBTN alignment, the Mississippi River Greenway/Robert Pira Regional Trail. With the Mississippi River Greenway being a regional connection, additional users will continue to originate from beyond the borders of South St. Paul and Dakota County.

Dakota County recognizes that the City of South St. Paul was recently awarded Statewide Health Improvement Partnership (SHIP) funding to evaluate the proposed pedestrian and bicycle access and is now attempting to secure funding to the construction of this project. Throughout the evaluation process, the city has demonstrated commitment to bringing an important trail connection to the residents of South St. Paul and Dakota County. We appreciate the continued efforts by the City of South St. Paul to make these project benefits real to the local community and region.

Dakota County supports the funding application for this necessary and timely project. The County looks forward to working with the City of Saint Paul to deliver a pedestrian and bicycle trail crossing where one does not currently exist.

Sincerely,



Rep. Keith Franke

District 54A



April 8, 2022

Sue Polka, P.E.
City Engineer
City of South St. Paul
125 3rd Avenue N
South St. Paul, MN 55075

RE: Letter of Support for City of St. Paul's 2022 Regional Solicitation Application to fund the Bryant Avenue Pedestrian Bridge

Dear Ms. Polka,

The River Heights Chamber of Commerce would like to demonstrate our support of the City of South St. Paul's application for funding as part of the 2022 Regional Solicitation through the Metropolitan Council. We have been made aware of the city's plans to add pedestrian and bicycle facilities to the Bryant Avenue bridge, linking the Mississippi River Greenway/Robert Pira Regional Trail and Kaposia Landing Park Area to businesses in South St. Paul.

The addition of pedestrian and bicycle facilities in this area is essential to the continued growth of businesses in South St. Paul and the south metro region. The Robert Pira Regional Trail links the city to downtown St. Paul, a regional employment and population center. The trail connection also links to the many livable-wage industrial jobs along Concord Street North (CSAH 156). The area surrounding the Bryant Avenue Bridge is a major employment area in South St. Paul. The proposed additions would provide employees with another safe and accessible way to access jobs and services.

The River Heights Chamber of Commerce supports this funding application and acknowledges that the project aligns with our association's goal of working to improve access to South St. Paul's businesses while improving the safety of the broader community. We look forward to completion of the project.

Sincerely,

A handwritten signature in black ink, appearing to read "Colleen Roth", with a long, sweeping horizontal line extending to the right.

Colleen Roth
President

Bryant Ave. Pedestrian Bridge Study

Bridge No. 19544 over Union Pacific Railroad Pedestrian Accommodations

Final Report



Prepared for:



Prepared by:



Date: July 2021

SRF No. 13918.00

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Chapter 1 Executive Summary

The City of South St. Paul was successful in securing a Statewide Health Improvement Partnership (SHIP) grant to evaluate design and funding for a dedicated pedestrian access from Concord Street (CSAH 156) to the Kaposia Landing park area. This study investigated alternatives for improved pedestrian accommodations for better access to the Landing's internal and regional trails to promote healthy lifestyles and improve non-vehicular access to the park. Alternatives were evaluated based on conceptual costs to construct, maintenance of traffic on the bridge and railroad impacts to develop a recommended project.

Currently, the only vehicular access to the Landing is at the south end of the park via Concord Street North (CSAH 156), by way of Bryant Avenue and Bridge No. 19544 to cross over an active rail yard. There are no sidewalks or separated pedestrian accommodations on the bridge. All geometric concepts considered in this study proposed to locate the pedestrian accommodations along the south side of the bridge. This provides direct access from the pedestrian crossing and bus stop at Concord Street over the railroad yard and along the east side of Metzen Parkway to enter Kaposia Landing without conflict with the Mississippi River Greenway.

This study developed and evaluated three alternatives. Alternative 1 has been recommended as the preferred alternative resulting in the expansion of existing substructures to support a new separated multi-use trail bridge adjacent to the existing traffic deck and within an existing bridge easement with the Union Pacific Railroad. This can be accomplished without the need to modify the existing traffic deck. In addition, two alternatives for the east approach along Metzen Parkway were developed and Approach Alternative 2 has been recommended, which eliminates the guardrail in favor of a trail section located at the back of curb. This reduces the width of trail and clear zone required and provides an easily maintainable facility.

This study details the existing conditions and needs at the project site, proposes alternatives, and documents the evaluation criteria and outcomes and provides recommendations. Consideration has been given to property and utility impacts, conceptual construction costs and railroad impacts. It is anticipated this report will be used as a basis for grant applications for funding to be allocated for future construction design and construction phases.

Chapter 2 Study Introduction & Purpose

2.1 Study Introduction

The City of South St. Paul was successful in securing a Statewide Health Improvement Partnership (SHIP) grant to evaluate design and funding for a dedicated pedestrian access from Concord Street (CSAH 156) to the Kaposia Landing park area. The proposed access would either utilize the Bryant Avenue Bridge over Union Pacific Railroad (Bridge No. 19544) or provide a new pedestrian bridge that parallels the existing vehicular bridge.

Improved pedestrian accommodations will provide better access to the Landing's internal and regional trails. In turn, this will help to promote healthy lifestyles and improve non-vehicular access to the park. With the Mississippi River Greenway being a regional connection, additional users will continue to originate from beyond the borders of South St. Paul and Dakota County.

2.2 Study Purpose

The purpose of this study is to:

1. Perform a feasibility analysis of pedestrian crossing options which considers:
 - a. Crossing Location
 - b. Schematic Design Alternatives
 - c. Connectivity to Modes of Transportation
 - d. Estimated Construction Costs
2. Conduct stakeholder processes and community engagement according to 2020 modified Statewide Health Improvement Partnership (SHIP) funding requirements.

Study considerations were evaluated against the following criteria to generate project recommendations:

- Estimated Construction Costs
- Maintenance of Traffic in Construction
- Railroad Impacts

Chapter 3 Site & Transportation Connections

3.1 Site Information

Located on the bank of the Mississippi River, Kaposia Landing is the site of a former landfill known as Port Crosby. The site was improved for public use with State funding in the mid-2000's. Site rehabilitation included capping the landfill and stabilizing nearly a mile of Mississippi River shoreline.

Opened to the public in 2008, the 87-acre park has become a regional destination and is connected to Dakota County's Mississippi River Greenway and River to River Greenway as well as the City of St. Paul's Robert Pira Regional Trail. The Landing contains about two miles of trails for biking and walking on the South Saint Paul Riverfront Trail. The Landing has a pavilion, picnic shelter, restrooms, playground, softball field, tennis courts, volleyball, ice skating, and dog park. In addition, Kaposia Landing has become a popular fishing location with over 5,000 feet of shoreline along the Mississippi River.

Access to the Kaposia Landing property is achieved via a vehicular bridge that was constructed in 2003, before landfill closure and park development. While the bridge provides access for motorized vehicles, it does not provide safe pedestrian accommodations.

Select site photos taken during the study are included in [Appendix A](#).

3.2 Transportation Connections

Access points into Kaposia Landing are limited to two northern trail connections and one southern vehicular connection. Concord Street North (CSAH 156) is the primary local road. Concord Street is anticipated to undergo substantial corridor improvements with construction anticipated to begin in 2021.

3.2.1 Vehicular Access

The only vehicular access to the Landing is at the south end of the park via Concord Street North (CSAH 156), by way of Bryant Avenue. The Bryant Avenue connection utilizes existing Bridge No. 19544 to cross over an active rail yard. The existing bridge has three-spans, is 317.5-feet long and has two 11-foot lanes, 2-foot shoulders and 42-inch-tall concrete parapets. Streetlights are installed on the bridge, but there are no sidewalks or separated pedestrian accommodations.

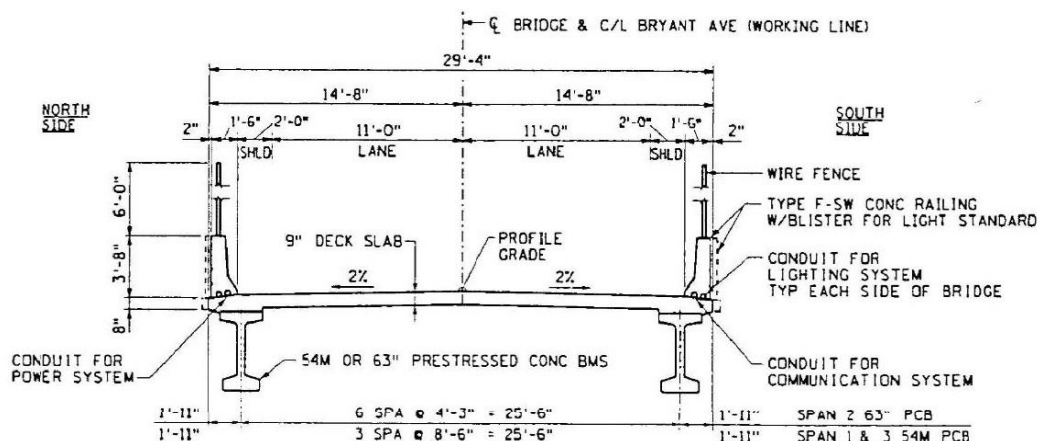


Figure 4.2.1: Existing Bridge No. 19544 Transverse Section

The bridge is owned and maintained by the City. Copies of the existing bridge plans are on file at the city and existing Bridge condition information is available through MnDOT's eDocs Public Webpage at:

- https://edocs-public.dot.state.mn.us/edocs_public/DMResultSet/Index

3.2.2 Transit

Metro Transit operates bus route 71 on Concord Street. Bus stops 9723 (northbound) and 10062 (southbound) are located at the southeast and northwest quadrants respectively of the intersection of Concord St. and Bryant Ave. which is located less than 200-feet from the bridge and access to Kaposia Landing. Route information is available at:

- <https://www.metrotransit.org/route/71>

3.2.3 Trail Connectivity

Kaposia Landing includes a well-developed trail system within the landing and serves as a destination along state and regional trail corridors.

Mississippi National River and Recreation Area (MNRRA)

The stretch of the Mississippi River from Ramsey and Dayton down through the Twin Cities area to Hastings is designated as a National Park titled the Mississippi National River and Recreation Area (MNRRA). A prominent part of the MNRRA is the 87-acre Kaposia Landing in South St. Paul. Partnerships with local agencies along the Mississippi river have advanced trail development and connections throughout the MNRRA, part of the Mississippi Regional Trail system (MRRT). Additional information is available at:

- <https://www.nps.gov/miss/index.htm>

Mississippi River Greenway (MRG)

Dakota County's (MRG) runs along the west side of Kaposia Landing parallel to the railroad yard and passes below the easternmost span of the existing bridge.

Trailheads for the north segment are located at Kaposia Landing Park and Simon's Ravine Trailhead in South St. Paul off of Concord Street and Swing Bridge Trailhead in Inver Grove Heights. The MRG is part of a collaborative resources and park enhancement partnership with the City of South St. Paul. Additional information is available at:

- <https://www.co.dakota.mn.us/parks/parksTrails/MississippiRiver>

River to River Greenway (RRG)

Dakota County's (RRG) connects to one of Kaposia Landings three access points by way of a trail bridge (Bridge No. 19537) over Concord Street and the railroad yard at Simon's Ravine Trailhead at the north end of the Landing. The RRG is in place between Robert Street and the Mississippi River in South St. Paul at the Landing. The RRG is part of a collaborative resources and park enhancement partnership with the City of South St. Paul. Additional information is available at:

- <https://www.co.dakota.mn.us/parks/parksTrails/river-to-river/Pages/default.aspx>

Robert Piram Regional Trail (RPRT)

The City of St. Paul's RPRT new trail segment will connect the Harriet Island Regional Park and its trail system to Kaposia Landing Park in South St. Paul. The RPRT accesses the north end of the Landing at a second trail access point by way of a new bridge (Bridge No. R0733) over an active railroad spur line. Additional information is available at:

- <https://www.stpaul.gov/departments/parks-recreation/design-construction/current-projects/robert-piram-regional-trail>

Chapter 4 Proposed Geometry

4.1 Preferred Alignment

The goal of this project is to maintain two traffic lanes while providing a 12-foot wide raised, protected or separated pedestrian trail facility. [Chapter 8](#) identifies alternatives which will allow for the intended improvement through modification of the existing bridge structure or by constructing a separate adjacent pedestrian facility. All locations considered for the pedestrian accommodations were in the vicinity of the existing bridge to allow the existing eastern embankment to be utilized, limiting the amount of imported fill required.

All geometric concepts proposed locate the pedestrian accommodations along the south side of the bridge. This provides direct access from the pedestrian crossing and bus stop at the southeast quadrant of Concord Street and Bryant Avenue across the bridge to Metzen Parkway. The pedestrian accommodations will be located along the northbound lane of Metzen Parkway and traverse the east bridge approach to enter Kaposia Landing. This allows the new pedestrian facility to access the Landing without conflicting with The Mississippi River Greenway which parallels the southbound lane of Metzen Parkway at the toe of the embankment slope and passes below the bridge. A connection to the Mississippi River Greenway and the Kaposia Landing trail network can be established at the cul-de-sac and parking lot at the bottom of the embankment. See [Appendix B](#) for proposed project layouts and profile information.

4.2 Alignments Not Advanced for Comparison

4.2.1 Pedestrians on North Side of Bridge

This geometric alternative was considered but not advanced due to the tight horizontal curve created by being on the inside of the turn from Bryant Avenue to Metzen Parkway at the east end of the bridge. Additionally, impacts to the Mississippi River Greenway would occur at the bottom of the east approach slope creating a difficult condition to provide a safe trail connection.

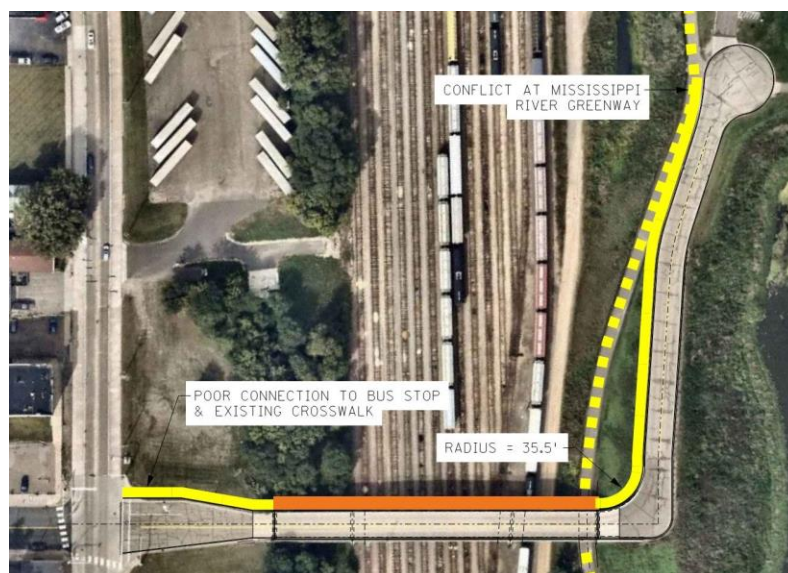


Fig. 4.2.1: North Side of Bridge Alternative

4.2.2 Pedestrian Approach at Southeast Corner of the Bridge

This geometric alternative was considered but not advanced due to the steeper embankment slope and limited space available for an ADA compliant approach to the bridge. Additionally, an approach which parallels Metzen Parkway from the bridge to the cul-de-sac is anticipated to be better utilized than a pedestrian route from the Landing, under span three of the bridge to a connection point on the bridge's south side.



Fig. 4.2.2: Approach at SE Bridge Corner Alternative

Chapter 5 Utilities

A preliminary investigation into existing utilities has been completed. Continuation of this utility investigation will be required in the preliminary design phase of the project as potential impacts become better defined. The options presented in this report will require varying amounts of adjustment/relocation of the utilities for the design and construction of a pedestrian facility.

Utilities included within the project area were contacted through Gopher State One Call (GSOC). Requests for as-built plans, maps, and field locates were made on call ticket number 202683945. Utility companies contacted by GSOC include:

Table 5.1 Property Identification

| Utility Name | Utility Abbreviation | Facility | Notes |
|---|----------------------|---|--|
| Comcast | COMCST01 | Television (TV) | No response |
| City of SSP | CSSTPL01 | Electric (E), Other (O), Sewer (S), Water (W) | Mapping received, power on bridge, san. sewer in Concord St., storm in Kaposia Landing |
| Centurylink | CTLQL - CTLMN01 | Telephone (TEL) | Mapping received, facility along Concord St. |
| Metropolitan Council Environmental Services | METWAS01 | Sanitary Sewer (S) | Mapping received, facility below bridge |
| MnDOT | MNSDOT01 | Electric (E) | Mapping received, facility at Concord St. & Bryant Ave. intersection |
| Sprint | USSPNT01 | Fiber Optic (FO) | Mapping received, facility below bridge |
| Xcel Energy | XCEL06 | Electric (E) | No response, power on bridge |

Source: Gopher State One Call Ticket No. 202683945

Refer to [Appendix C](#) for a map detailing existing utility facilities in the project vicinity.

Chapter 6 Property Identification

Properties included within and adjacent to the project area which may be directly or indirectly impacted by the construction of the crossing and temporary facilities were identified. The impacted parcels within the vicinity of the project are listed below:

Table 6.1 Property Identification

| Parcel No. | Parcel ID | Owner |
|------------|-----------------|---|
| 1 | 36-42800-00-110 | City of South St. Paul Economic Development Authority |
| 2 | n/a | Unassigned (See Note 1) |
| 3 | 36-42800-00-182 | Port Crosby Association |
| 4 | 36-42800-00-183 | City of South St. Paul |
| 5 | 36-01600-80-030 | City of South St. Paul |
| 6 | 36-45800-00-101 | City of South St. Paul |
| 7 | 36-01600-75-010 | City of South St. Paul |
| 8 | 36-45800-00-096 | City of South St. Paul |
| 9 | 36-45800-00-095 | City of South St. Paul |

Source: Dakota County Property Information Online Search

Notes:

1. Parcel 2 is unidentified and assumed to be under the ownership of the Union Pacific Railroad or a State agency. Initial coordination was conducted with Dakota County Taxation & Records Dept. and Chris Rice (MnDOT Rail) on February 24, 2021. No title search was performed as part of the study.

Refer to [Appendix D](#) for a map detailing property identification for parcels listed above.

Chapter 7 Property Easements & Acquisitions

7.1 Property Acquisitions & Easements

Land anticipated to be impacted by the construction of the new pedestrian accommodations is primarily owned by the City of South St. Paul. This includes land to the east and west of the Bryant Avenue bridge. For this reason, no property acquisitions or permanent easements are anticipated for construction staging, operation or maintenance of the pedestrian facility. This includes the length of the pedestrian facility from Concord Avenue to the west end of the bridge and from the east end of the bridge along Metzen Parkway as the facility descends into Kaposia Landing.

Dakota County operates the MRG on City land by agreement with the City. The MRG traverses the western edge of Kaposia Landing. It passes below span three of the existing bridge and travels north at the toe of the embankment slope just west of Metzen Parkway. It is anticipated that construction of the new pedestrian facility will impact the MRG. Temporary trail accommodations should be considered to allow the MRG to safely bypass construction. A concept has been developed and is represented in [Appendix E](#).

7.2 Railroad Overpass Agreement

Parcel 3 identified in [Appendix D](#), is owned by the Port Crosby Association, and is assumed to be under the ownership of the Union Pacific Railroad (UPR). Adjacent parcels are identified as being owned by the UPR and share a common address as listed for the Port Crosby Association.

An overpass agreement between the City of South St. Paul and the UPR was executed for the Bryant Avenue Bridge when originally constructed. This agreement addressed the ownership and maintenance of the bridge. As part of the agreement, a 66-foot wide permanent easement for Bridge No. 19544 was established. A copy of the easement document is included in [Appendix F](#).

This study reviewed options for the modification of the existing bridge and the construction of a separate new bridge. Alternatives have been developed which explore the potential to 1) widen the bridge or 2) widen only the substructures to support a separate pedestrian deck immediately adjacent to the existing deck. These modified bridge alternatives may be constructed within the limits of the existing easement. Modified bridge alternatives that fit within the existing easement and do not further restrict railroad clearance windows (vertical and horizontal) established by the original bridge may not require the need for a new overpass agreement. A Request for Exception should be submitted to the railroad in future design phases

per UPRR Guidelines Section 3.3.2 to address temporary horizontal clearance impacts at the piers. Existing piers are located within the allowable 15-foot temporary horizontal clearance window.

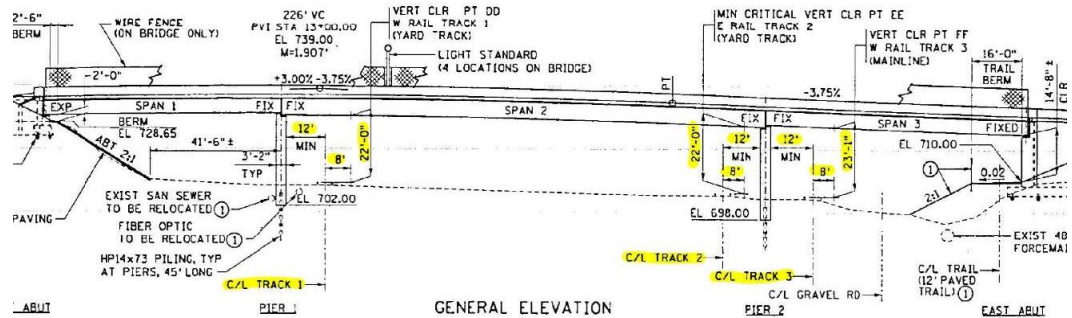


Fig. 7.2.1: Railroad Clearances Established by Bridge No. 19544

However, alternatives that explore the construction of a separate pedestrian bridge structure are anticipated to require a new overpass agreement and new permanent easement. The design of the new structure would be subject to the requirements of the current Union Pacific Railroad Guidelines for Railroad Grade Separation Projects.

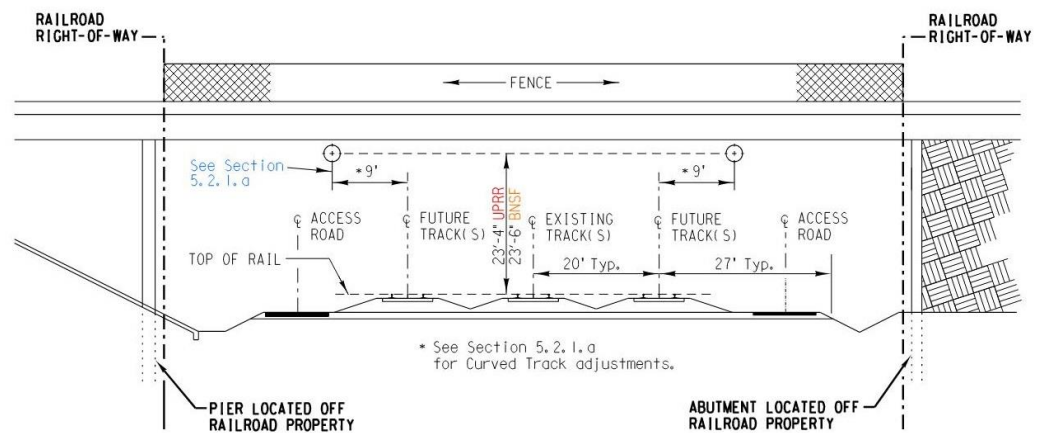


Fig. 7.2.2: Current UPRR Clearances for New Railroad Grade Separation Projects

Chapter 8 Proposed Bridge Alternatives

8.1 Bridge Type Alternatives

The bridge geometrics of the proposed pedestrian accommodations are assumed to closely match the current bridge length, span and substructure configurations of Bridge No. 19544. This is due to the limited available space for pier locations in the area of existing tracks within the active railroad facility below the bridge.

8.1.1 Bridge Alternative 1: Bridge No. 19544 Substructure Widening

This alternative proposes leave the existing bridge deck unchanged. Modifications would be made to the bridge substructures by widening the abutments and piers to the south to accommodate a new trail bridge superstructure. The new superstructure would be separated from the existing bridge deck by approximately 2-feet.

This alternative utilizes a series of prefabricated steel trusses as shown in [Appendix G1](#). The trusses would be fabricated off-site, delivered, erected, and placed with a crane. This structure type is similar to Bridge No. R0733 which carries the RPRT at the north entrance to the Landing. The structure depth would be shallower than the existing bridge providing increased vertical clearance over the railroad. The truss segments could be set, and concrete deck constructed with minimal railroad impacts.

The conceptual cost to construct Alternative 1 is \$5,100,000 (2021 dollars). Utility and easement costs are not included. Maintenance costs may be similar or slightly more than the current bridge no. 19544 maintenance program.

8.1.2 Bridge Alternative 2: Bridge No. 19544 Deck Widening

This alternative proposes to remove the concrete barrier and a portion of the deck along the south edge of the Bryant Avenue bridge. A widened deck would be constructed that maintains the 26-foot-wide roadway and provides a 12-foot multi-use trail along the south side of the bridge. The trail may be protected with a curb and raised sidewalk or separated from traffic with a concrete parapet.

Two new beam lines would be added to each span. The new beam types and depths would be similar to the existing beams as shown in [Appendix G2](#). The beams and deck would be of similar construction to the original bridge and provide for similar structure depth and vertical clearance over the railroad. Abutments and piers would be expanded to accommodate the widened bridge.

The conceptual cost to construct Alternative 2 is \$7,100,000 (2021 dollars). Utility and easement costs are not included. Maintenance costs should be similar to the current bridge no. 19544 maintenance program.

8.1.3 Bridge Alternative 3: Separate Trail Bridge

This alternative proposes to leave the existing bridge structure intact. A new three-span trail bridge would be constructed to the south of the existing bridge. This alternative utilizes 63-inch prestressed concrete beams with a concrete deck constructed between the beam webs as shown in [Appendix G3](#). It is similar in style to Bridge No. 19537 which carries the RRG located at Simon's Ravine Trailhead.

Constructing a deck located through the girders provides a shallower structure depth and increased vertical clearance over the railroad. New abutments and piers would be constructed to the south of the existing Bryant Avenue bridge and would provide for approximately 10-feet of separation between the structures.

The conceptual cost to construct Alternative 3 is \$4,500,000 (2021 dollars). Utility and easement costs are not included. Maintenance cost considerations for a new bridge facility will be in addition to the current bridge no. 19544 maintenance program.

8.1.4 Bridge Alternatives Not Advanced for Comparison

A three-span continuous steel girder bridge could be used in place of the truss option and prestressed beam option in Alternatives 1 and 3 respectfully. However, this structure type is anticipated result in higher construction and long-term maintenance costs than other bridge types. For these reasons, continuous steel girders were excluded from further consideration.

8.2 East Bridge Approach Alternatives

The east bridge approach which includes Metzen Parkway is a long embankment originally constructed for the bridge overpass of the Union Pacific rail yard. Pedestrian accommodations will need to traverse the embankment along side Metzen Parkway which includes a curb and gutter section and guardrail. Options to utilize the embankment for pedestrian accommodations include a short to medium height retaining wall to widen out the embankment for the trail section. The trail can be located behind the existing guardrail or the guardrail can be removed and the trail constructed at the back of curb. Approach alternatives are shown in [Appendix G4](#), along with the existing embankment conditions at Metzen Parkway.

8.2.1 Approach Alternative 1: Separated Trail with Guardrail Protection

The existing guardrail can be maintained in place to provide additional protection from traffic. However, guardrail posts are considered a potential hazard to trail users and a 3-foot clear zone from the edge of trail is recommended. The area between the back of curb and edge of trail can be turf or paved to reduce maintenance. A concrete curb and fall protection can be located on top of the retaining wall.

8.2.2 Approach Alternative 2: Trail at Back of Curb

The existing guardrail may be removed, and the new trail located at the back of curb. Elimination of the guardrail would reduce maintenance and improve snow removal operations on the trail, but may offer less protection from traffic to the pedestrian accommodations. A concrete parapet and fall protection can be located on top of the retaining wall.

8.3 Comparison of Alternatives

8.3.1 Bridge Alternatives Comparison

Bridge Alternative 1:

Advantages

- Lower construction cost compared to alternative 2
- No bridge deck demolition/modification over railroad
- Maintains two-way traffic/emergency access on bridge in construction
- Fits within existing railroad easement
- Considered a bridge modification. No new railroad grade separation agreement anticipated to be required.
- Least construction within railroad ROW compared to alternatives 2 and 3.
- Superstructure setting can occur with night-time closures and short durations
- Streetlights on existing bridge will provide pedestrian lighting on new bridge

Disadvantages

- Slightly higher construction cost compared to alternative 3
- Requires abutment modification/widening
- Requires pier modification/widening in railroad ROW
- Minor traffic impacts to Bryant Avenue bridge in construction

Bridge Alternative 2:

Advantages

- Fits within existing railroad easement
- Considered a bridge modification. No new railroad grade separation agreement anticipated to be required.

Disadvantages

- Highest construction cost compared to alternatives 1 and 3
- Requires bridge deck demolition/modification over railroad
- Requires abutment modification/widening
- Requires pier modification/widening in railroad ROW
- Restricts traffic/emergency access on bridge in construction

- Most construction within railroad ROW compared to alternatives 1 and 3.

Bridge Alternative 3:

Advantages

- Lower construction cost compared to alternative 1 and 2
- No bridge deck demolition/modification over railroad
- Maintains two-way traffic/emergency access on bridge in construction
- Less construction within railroad ROW compared to alternative 2.

Disadvantages

- Requires construction of new abutments & east embankment.
- Requires construction of new piers in railroad ROW.
- Requires a new railroad grade separation agreement and easement.
- More construction within railroad ROW compared to alternative 1.

8.3.2 Bridge Approach Alternatives Comparison

Approach Alternative 1:

Advantages

- Eliminates guardrail removal
- Provides additional pedestrian protection from traffic
- Provides area between trail and curb for snow storage
- Negligible cost difference to approach alternative 2

Disadvantages

- Higher cost to maintain guardrail
- More to clear and maintain trail
- Requires wider trail section with clear zone & taller retaining wall
- Requires a slightly taller retaining wall for wider trail

Approach Alternative 2:

Advantages

- Less room required for trail width resulting in short retaining wall
- Easier trail maintenance and clearing
- Negligible cost difference to approach alternative 2

Disadvantages

- Less pedestrian protection from traffic
- No snow storage area.

Chapter 9 Study Recommendations

9.1 Bridge Type Alternative

Although Alternative 2 results in a widened existing bridge it is more costly and the most impactful to railroad operations. The widened deck would combine traffic and pedestrian accommodations and be subject to the existing railroad overpass agreement and permanent easement. However, it requires the most demolition and construction over an active rail yard, impacts to the only traffic and emergency vehicle access to Kaposia Landing and is anticipated to be the highest cost project to construct.

Although Alternative 3 eliminates the need for all bridge demolition and modification to Bridge No. 19544, it requires construction of a new bridge that would be subject to a new railroad overpass agreement and permanent easement, and involves more construction impacts to an active rail yard than Alternative 1.

Implementation of Alternative 1 which is comprised of expanded abutments and piers to support a separate pedestrian bridge deck comprised of simple span prefabricated steel trusses will reduce the demolition and construction over the railroad, maintain traffic and emergency vehicle access to Kaposia Landing in construction and may not require a new railroad overpass agreement or permanent easement. In addition, pedestrian safety may be enhanced as the trail facility is separated from vehicular traffic and can benefit from the existing streetlights on the original bridge. Therefore, the recommended project and bridge type is Alternative 1, Bridge No. 19544 Substructure Widening with a superstructure consisting of prefabricated steel trusses.

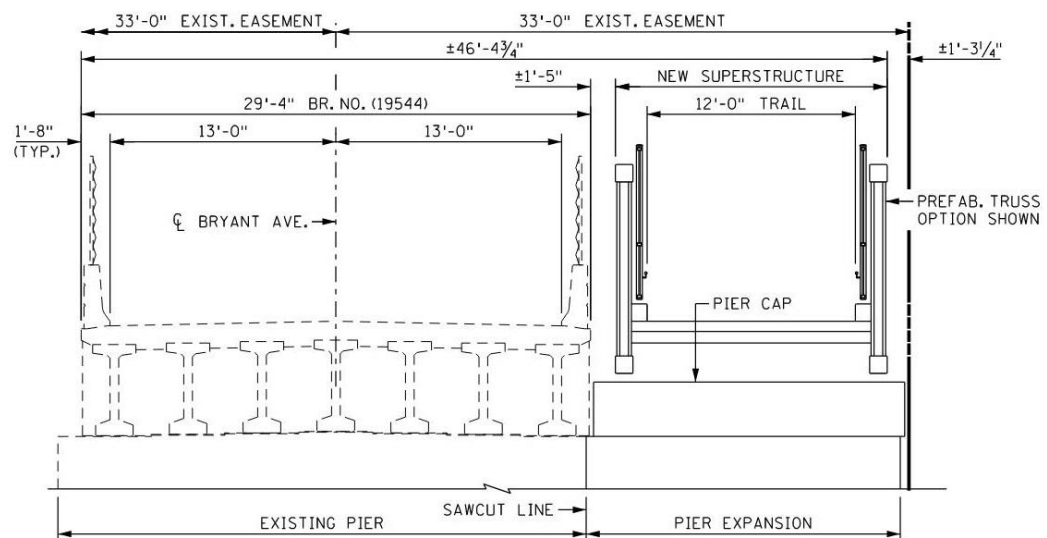


Figure 10.1: Recommended Bridge Alternative 1 with Prefabricated Steel Truss

9.2 East Bridge Approach Alternatives

Although Approach Alternative 1 provides more pedestrian protection, Bryant Avenue and Metzen Parkway are low speed local roads and the guardrail is not required if the curb and gutter section is maintained and a parapet is constructed on the retaining wall. Guardrail posts are a potential hazard to trail users. In addition, it is more difficult to clear and maintain the trail with guardrail installed. Guardrail increases long-term maintenance costs for needed repairs and replacement.

Implementation of Approach Alternative 2, which eliminates the guardrail, removes a potential hazard from pedestrians and reduces the overall width of trail and clear zone required. The narrower trail section reduces the height of retaining wall needed for the trail. In addition, the trail will be easier to clear and maintain without the guardrail as an obstruction. Therefore, the recommended east approach alternative is Alternative 2, Trail at Back of Curb & Gutter.

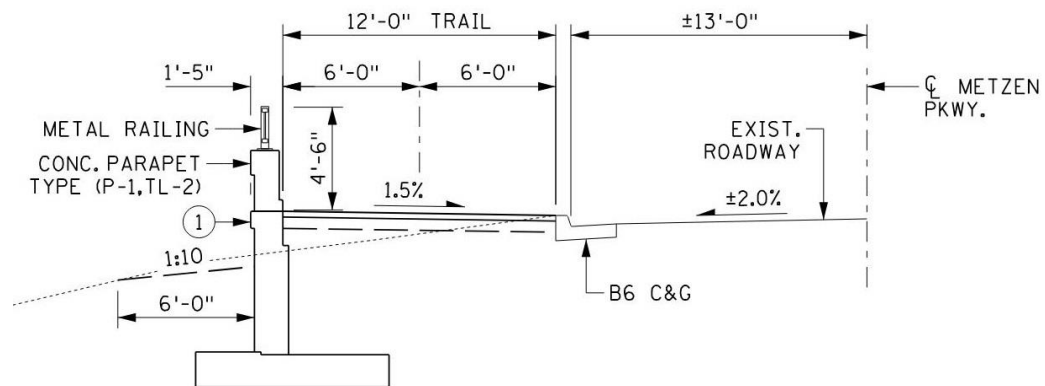


Figure 10.2: Recommended Approach Alternative 2 with Trail at Back of Curb & Gutter

Chapter 10 Agency Coordination & Public Process

10.1 Agency Coordination

10.1.1 Public Agencies

A summary of the agency coordination conducted as part of the study is recorded below. Members of the Project Management Team (PMT) include, the City of South St. Paul, Dakota County and SRF Consulting Group.

- January 14, 2021 PMT Meeting
- March 9, 2021 City Parks & Recreation Advisory Commission
- July 12, 2021 City Council Work Session

10.1.2 Railroad Coordination

The Union Pacific Railroad (UPR) Guidelines for Railroad Grade Separation Projects requires that prior to any review, the Railroad Local Representative shall receive written notice from the project owner agreeing to pay all costs associated with the Railroad's project review and participation. The Union Pacific Railroad was not engaged in the study through a contract or agreement and for this reason is not anticipated to provide comment on the study. A copy of the study may be provided to the UPR Public Projects Manager as information for future coordination.

10.2 Public Process

10.2.1 Study Engagement

A summary of the Public Process conducted as part of this study is recorded below.

- The week of June 28th, 2021: A City-hosted webpage was made active with links to the Draft Report, a Vimeo presentation on the study and a survey and public comment form.
- July 1st, 2021: The active webpage announcement was released with the city-wide newsletter.
- July 9th, 2021: Public comment period closed. Comments were logged, provided to City staff and are included in the appendix to this report.

10.2.2 Future Engagement Opportunities

A summary of potential opportunities for public engagement to be conducted in future design and construction phases is outlined below.

- Public open house(s).
- Advertise information, locations and to current partners to offer active programs at Kaposia Landing and on the Mississippi River Greenway.
- Conduct outreach with residents of two low-income high-rise facilities operated by the City to gain feedback through the City's Community Affairs Liaison. This effort reaches three SHIP populations including, persons 60 and older, low-income populations and persons experiencing health disparity.

Chapter 11 Project Schedule

11.1 Planning

The City of South St. Paul's 2040 Comprehensive Plan includes the Bryant Avenue pedestrian bridge accommodations project as an element of planning and community significance (Action 9.1). In addition, both South St. Paul and Dakota County have identified the Mississippi River Greenway and its components in their continuing Comprehensive Plans. Dakota County and the City of South St. Paul have discussed additional interpretative elements based on Dakota County's Mississippi River Trail Interpretative & Experience Design plan.

Through their active partnership, the City of South St. Paul and Dakota County received an Environmental Legacy Grant (ELF) for the Phase 2 development of Kaposia Landing. Phase 2 directly links to the Bryant Avenue bridge and south park access. The intent is to continue development of Kaposia Landing through future grant opportunities to fund development of Phase 3 of the park. Phase 3 may include the construction of the pedestrian accommodations at the Bryant Avenue bridge. This study and future design phases will assist with Capital Improvement Plan (CIP) programming and preparation of grant applications for construction. The City intends for the project to be "shovel ready" when funding sources become available.

Through the existing Mississippi River Greenway joint power agreement, South St. Paul will continue to partner with Dakota County on related operation and maintenance of adjacent trail facilities. The City of South St. Paul is anticipated to own and maintain the Bryant Avenue pedestrian accommodations, once completed.

11.2 Design

The City of South St Paul will coordinate future design phases which are anticipated to include preliminary and final engineering, project delivery and construction. The following is a summary of anticipated next steps for design and construction. Design phases may be advanced ahead of project funding. However, project authorization and final approvals will require funding to be secured. Actual project timeline will be subject to funding source requirements and active railroad participation.

Anticipated Future Design Phases:

- Identify funding opportunities and complete grant applications
- Secure a Union Pacific Railroad (UPR) engineering review agreement
- 6 – 9 months Preliminary Civil & Bridge Design:

- Geotechnical & Environmental Investigation
- 12 – 24 months Final Civil & Bridge Design
 - UPR Agreement / R.O.W. / Utility / Project Authorization
- 6 – 12 months Construction

Chapter 12 Appendix

Appendix A: Site Photos

Appendix B: Proposed Geometry

Appendix C: Existing Utilities

Appendix D: Property Identification

Appendix E: Easements & Acquisitions

Appendix F: Existing Union Pacific Railroad Permanent Easement

Appendix G: Bridge Type Alternatives

Appendix H: Survey Results & Public Comment Log

Appendix A

Site Photos

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VIEW 1: WEST BRIDGE APPROACH LOOKING EAST TOWARD BRIDGE



VIEW 3: SOUTHWEST BRIDGE APPROACH LOOKING SOUTHWEST FROM END OF APPROACH PANEL



VIEW 2: WEST BRIDGE APPROACH LOOKING WEST FROM BRIDGE



VIEW 4: NORTHWEST BRIDGE APPROACH LOOKING NORTHWEST FROM END OF APPROACH BRIDGE



Site Photos

Bryant Avenue Pedestrian Bridge Feasibility Study
for the City of South St. Paul

Job #13908
2/27/2021

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VIEW 5: RAILROAD TRACKS SOUTH OF BRIDGE LOOKING SOUTH FROM BRIDGE



VIEW 7: TYPICAL BRIDGE DECK LOOKING WEST FROM EAST APPROACH PANEL



VIEW 6: RAILROAD TRACKS NORTH OF BRIDGE LOOKING NORTH FROM BRIDGE



VIEW 8: EAST BRIDGE APPROACH LOOKING NORTH FROM INTERSECTION OF XXX



Job #13908
2/27/2021

Site Photos

Bryant Avenue Pedestrian Bridge Feasibility Study
for the City of South St. Paul

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VIEW 9: VIEW OF BRIDGE FROM NORTHEAST OF BRIDGE LOOKING SOUTHWEST



VIEW11: VIEW OF EAST APPROACH EMBANKMENT LOOKING SOUTH



VIEW 10: VIEW OF EAST APPROACH LOOKING SOUTH FROM ROUNDABOUT NEAR PARKING LOT



VIEW 12: VIEW OF EAST APPROACH EMBANKMENT LOOKING NORTH



Job #13908
2/27/2021

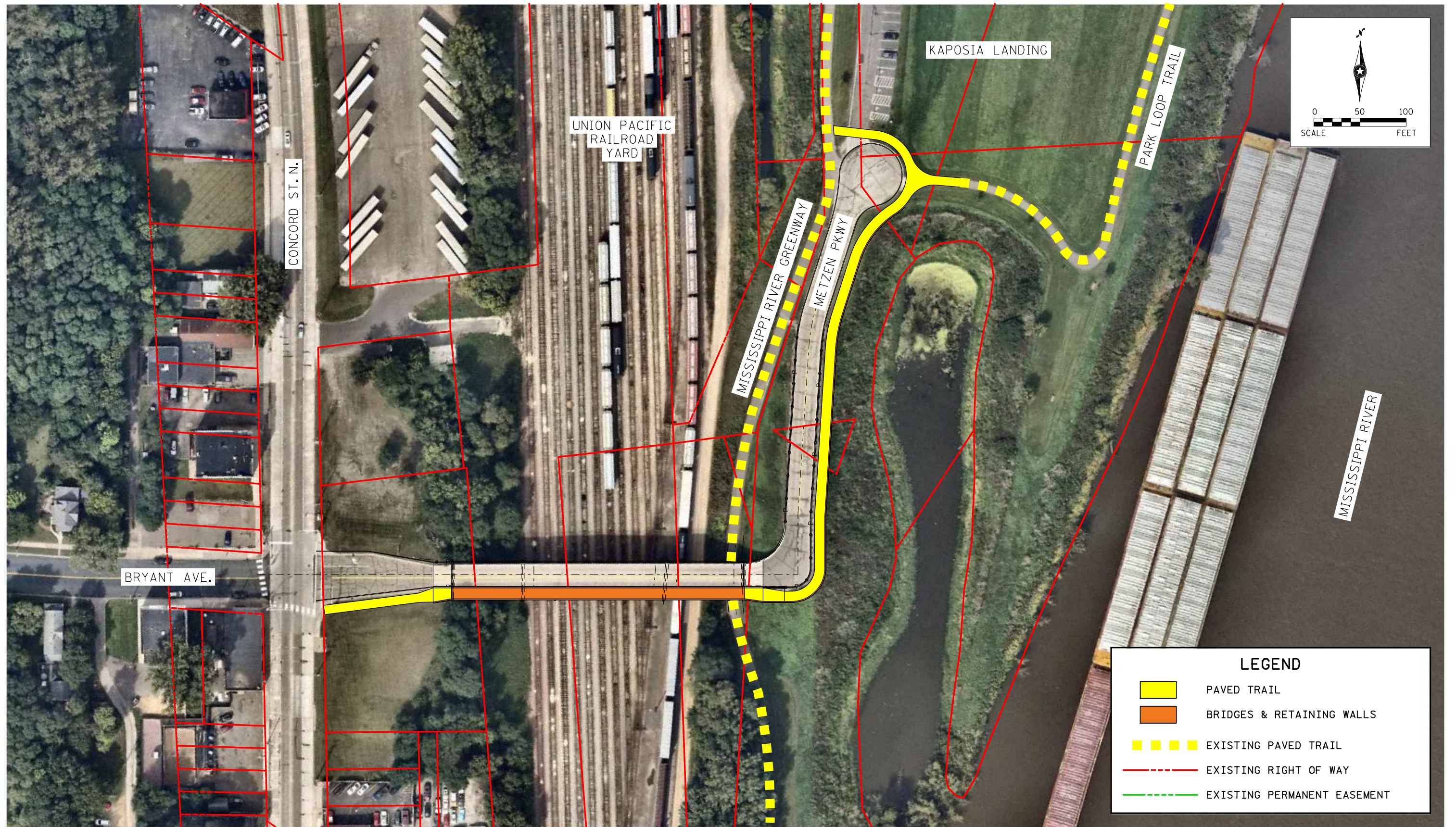
Site Photos

Bryant Avenue Pedestrian Bridge Feasibility Study
for the City of South St. Paul

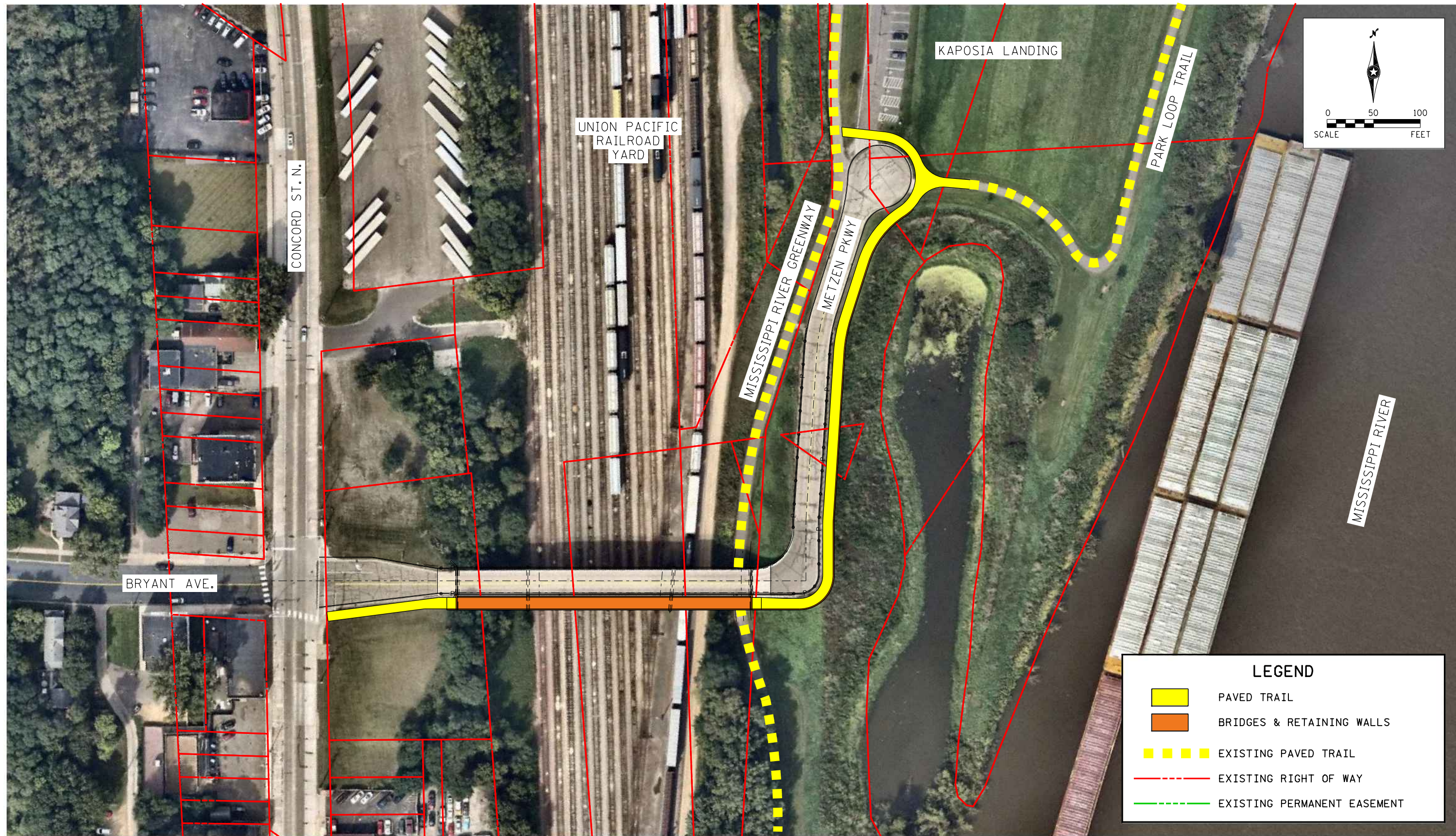
Appendix B

Proposed Geometry

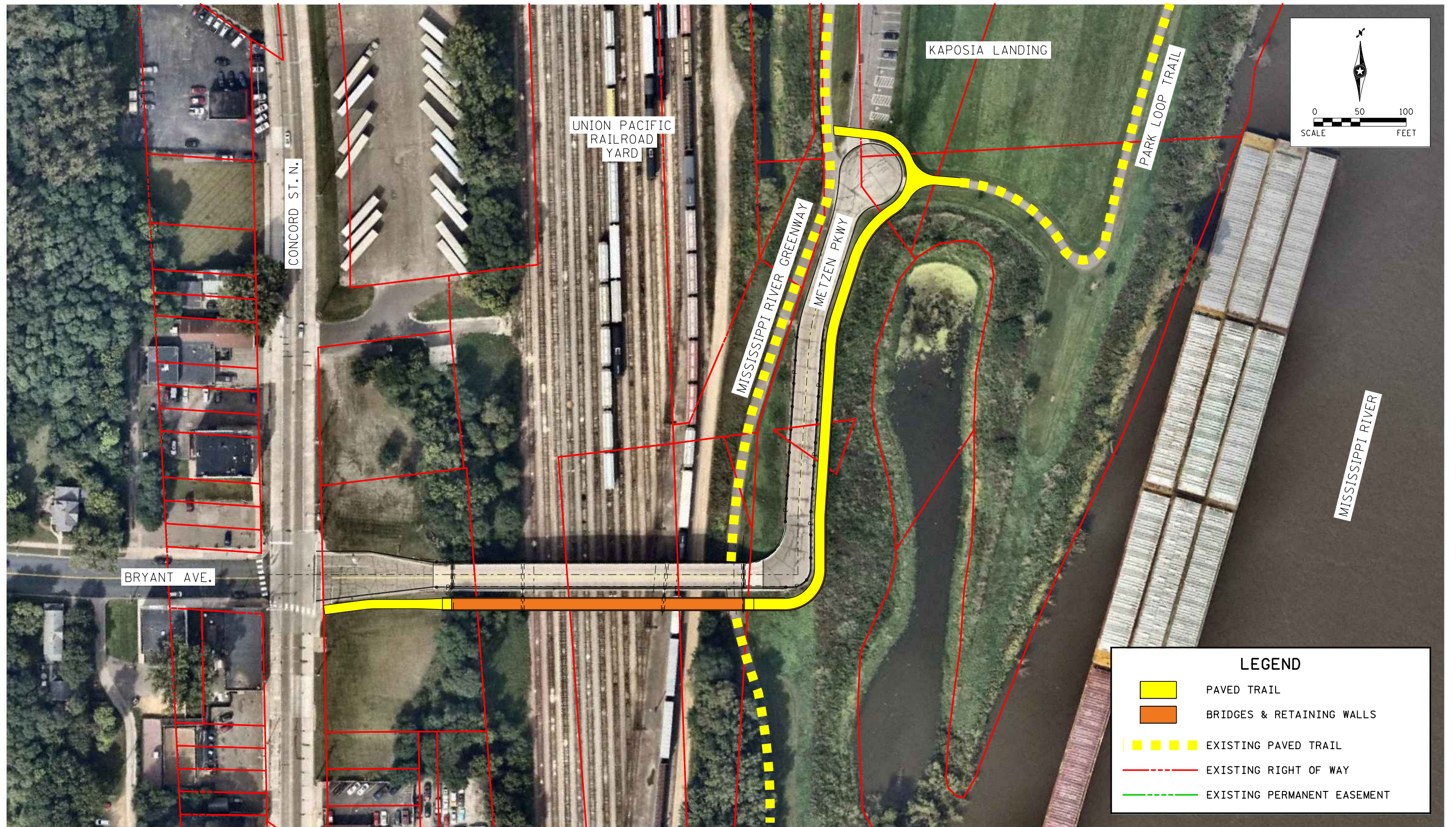
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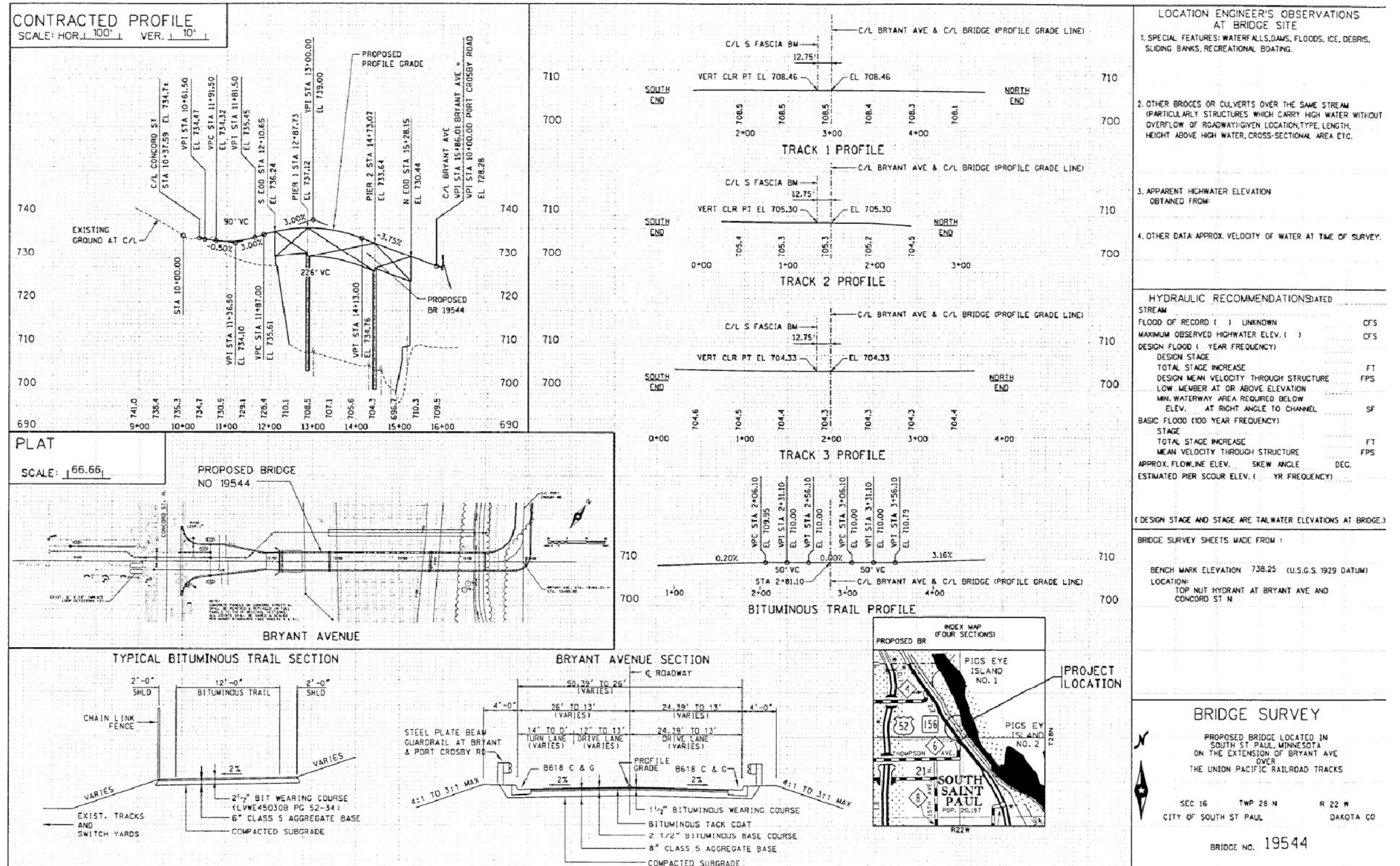


Job #13908
2/27/2021

Proposed Geometry - Concept C: Separate Pedestrian Bridge Adjacent to Bridge No. 19544

Bryant Avenue Pedestrian Bridge Feasibility Study
for the City of South St. Paul

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Appendix C

Existing Utilities

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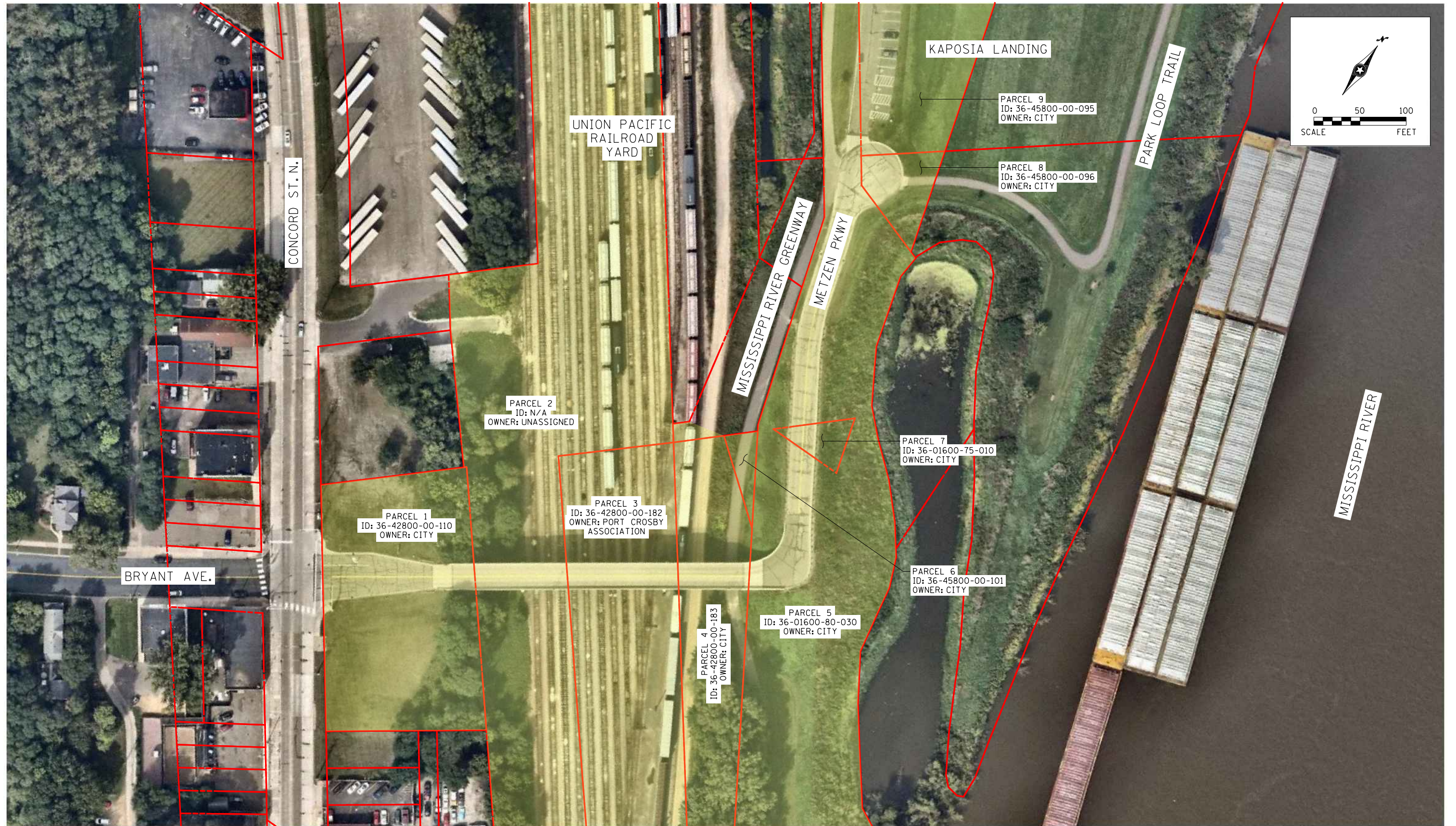
EXISTING UTILITIES

Bryant Avenue Pedestrian Bridge Feasibility Study
for the City of South St. Paul

Appendix D

Property Identification

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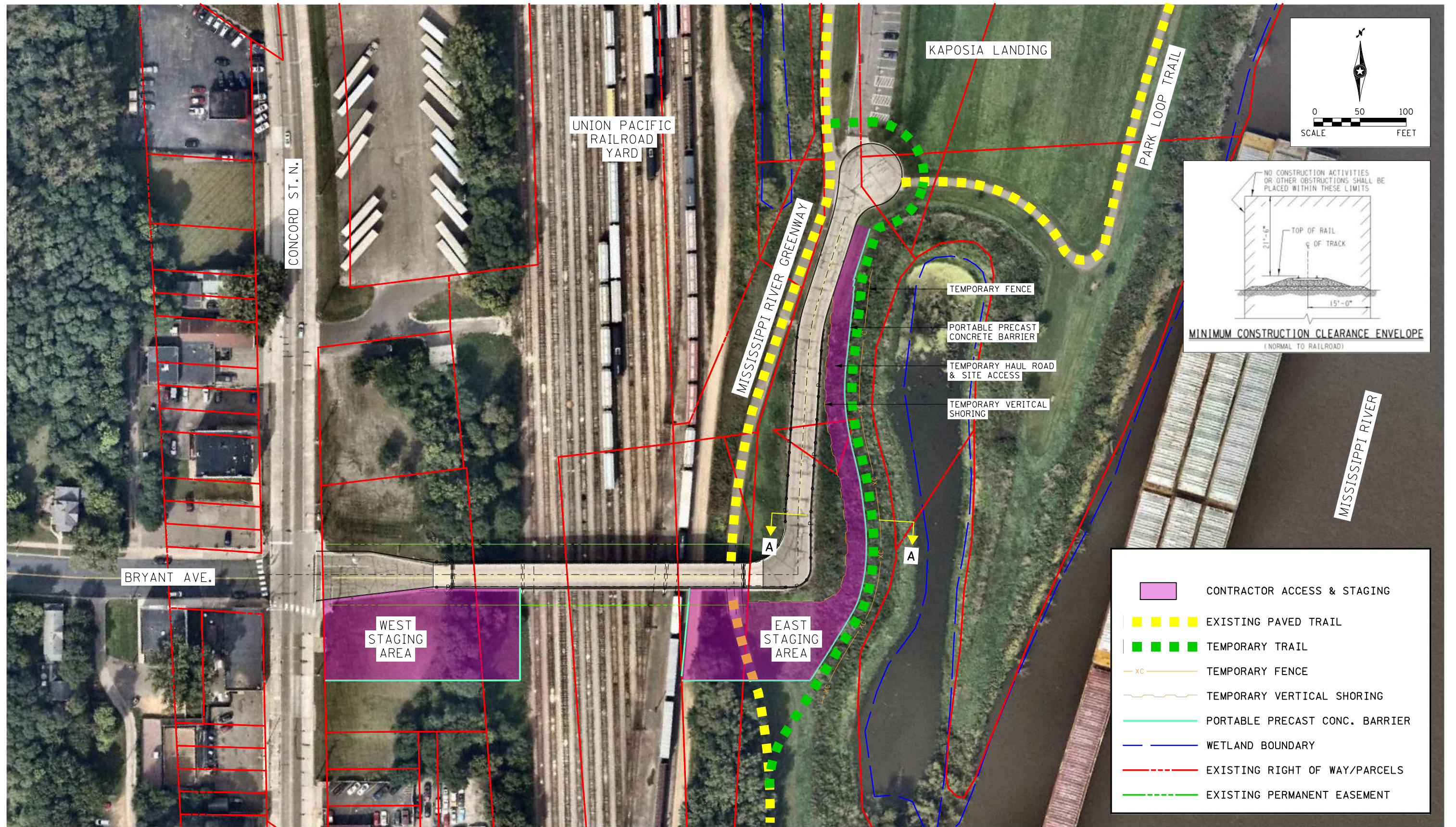
Property Identification

Bryant Avenue Pedestrian Bridge Feasibility Study
for the City of South St. Paul

Appendix E

Easements & Acquisitions

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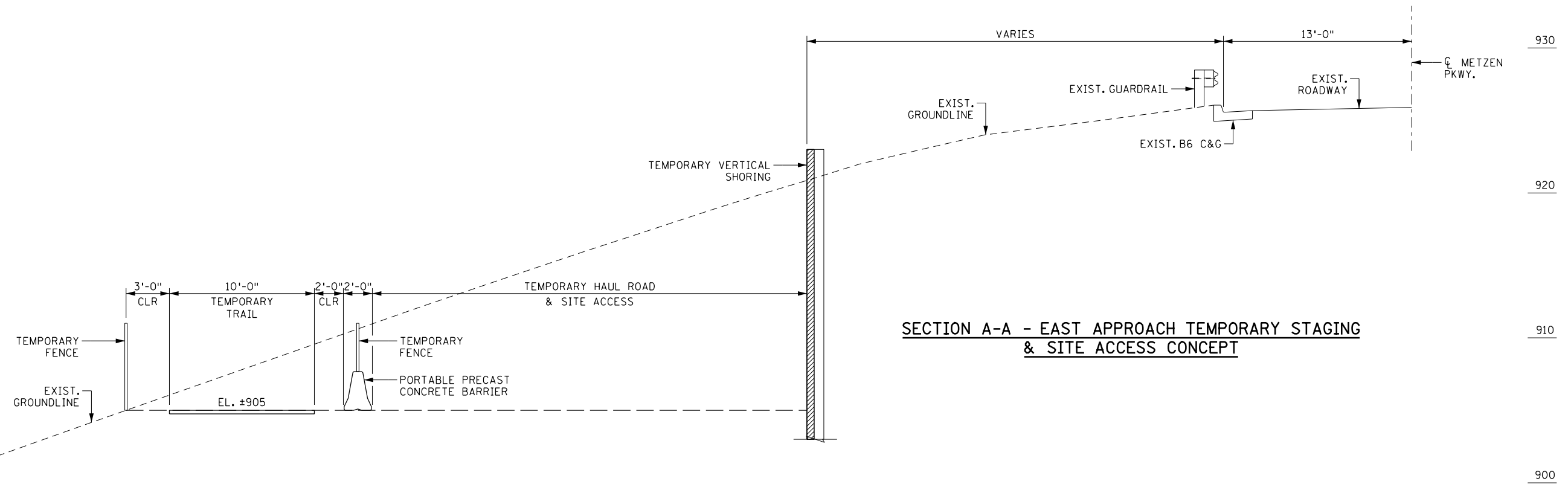
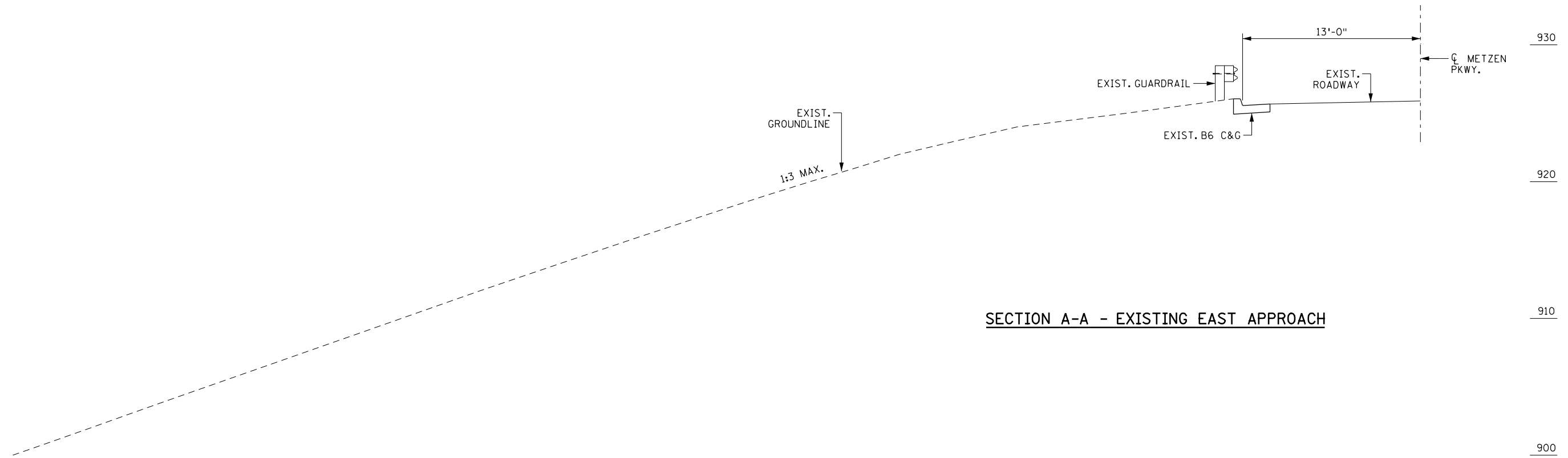


Job #13908
2/27/2021

Easements & Acquisitions - Construction Staging Concept - Plan View

Bryant Avenue Pedestrian Bridge Feasibility Study
for the City of South St. Paul

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Appendix F

Existing Union Pacific Railroad Permanent Easement

LAND ACQUISITION

Route : BRYANT AVENUE
 Section :
 County : DAKOTA
 Project No.: 19544
 Parcel :
 Sta. :
 Owner : Union Pacific Railroad Company

DATE RECEIVED 2-1-03
 DAKOTA COUNTY
 TREASURER-AUDITOR

CITY OF SOUTH ST. PAUL
 COUNTY OF DAKOTA
 STATE OF MINNESOTA

UPRR Folder No. 2107-22

PERMANENT EASEMENT

KNOW ALL MEN BY THESE PRESENTS, that UNION PACIFIC RAILROAD COMPANY, a Delaware corporation, ("Grantor") whose address for purposes of this instrument is 1416 Dodge Street, Omaha, Nebraska, 68179 for and in consideration of \$10.00 in hand paid by the City of South St. Paul, State of Minnesota ("Grantee") the receipt of which is hereby acknowledged, and the promises of the Grantee hereinafter specified, does hereby remise, release, quit claim and grant, so far as Grantor lawfully may, unto the Grantee, subject to the terms and conditions hereinafter set forth, a permanent easement for overhead highway bridge purposes on, along, over and across the property that is described in Exhibit A, hereto attached and hereby made a part hereof, (the "Premises") for the purpose of the construction, maintenance, repair and use of highway bridge carrying Bryant Avenue over the Grantor's right-of-way in Dakota County, Minnesota.

RESERVING, however, unto the Grantor, its successors and assigns, the right to construct, maintain, use, operate, relocate, reconstruct and renew such tracks and facilities as it may at any time, and from time to time, desire within the limits of the Premises hereinbefore described, including the right and privilege to use said Premises for any and all purposes, not inconsistent with the use thereof for highway bridge purposes.

ALSO RESERVING, however, unto the Grantor, its successors and assigns, the right to construct, place, operate, maintain, alter, repair, replace, renew, improve and remove communication lines above, below and on the surface of the Premises, including, without limitation, transmission by conduit, fiber optics, cable, wire or other means, of electricity, voice data, video, digitized information, or other materials or information, including the right of ingress and egress in any such manner as does not unreasonably interfere with Grantee's use of the Premises for said roadway, and further reserving unto Grantor, its successors and assigns, all right and privilege or ingress and egress to said

Premises as Grantor, its successors and assigns may require to investigate and remediate environmental contamination and hazards.

Said easement is granted subject to the following conditions:

1. To existing interests in the above-described Premises to whomsoever belonging and of whatsoever nature and any and all extensions and renewals thereof, including but not limited to underground pipe line or lines, or any type of wire line or lines, if any.
2. Any and all cuts and fills, excavations or embankments necessary in the construction, maintenance, or future alteration of said roadway shall be made and maintained in such manner, form and extent as will provide adequate drainage of and from the adjoining lands and premises of the Grantor, and wherever any such fill or embankment shall or may obstruct the natural and pre-existing drainage from such lands and premises of the Grantor, the Grantee shall construct and maintain such culverts or drains as may be requisite to preserve such natural and pre-existing drainage, and shall also, wherever necessary, construct extensions of existing drains, culverts or ditches through or along the Premises of the Grantor, such extensions to be of adequate sectional dimensions to preserve the present flowage of drainage or other waters, and of materials and workmanship equally as good as those now existing.
3. The Grantee shall bear the cost of removal, relocation or reconstruction of any and all right-of-way fences, telephone or telegraph poles, or other facilities, the removal, relocation or reconstruction of which may be made necessary by reason of the use of said Premises for roadway purposes.
4. The Grantee shall, at its own cost and expense, make adjustment with industries or other lessees of Grantor for buildings or improvements that may have to be relocated, reconstructed or destroyed by reason of the construction and maintenance of said roadway on said Premises.
5. All contracts between Grantee and its contractor(s) for Project 19544 shall provide that the contractor(s) shall:
 - A) Furnish to the Grantor a railroad protective liability policy in the form provided by the Federal-Aid Policy Guide issued by the Federal Highway Administration on December 9, 1991 (23 CFR 646), or as such form may be hereafter amended or supplanted, and any other pertinent instructions issued by the Federal Highway Administration, Department of Transportation. The combined single limit of said policy shall not be less than Two Million Dollars (\$2,000,000.00), for all damages arising out of bodily injury to or death of any person or persons and for all damage arising out of loss or destruction of or injury or damage to property in any one occurrence, and, subject to that limit a total (or aggregate) limit of not less than Six Million Dollars (\$6,000,000.00), for all damages arising out of bodily injuries to or death of any person or persons and for all damages arising out

of or loss or destruction of or injury or damage to property during the policy period. Said insurance policy executed by a corporation qualified to write the same in the State in which the work is to be performed, shall be in the form and substance satisfactory to the Grantor and shall be delivered to and approved by the Grantor's Chief Engineer prior to the entry upon or use of Grantor's property and/or the Premises by the contractor.

- B) Carry regular Contractor's Public Liability and Property Damage Insurance as specified in the Federal-Aid Policy Guide issued by the Federal Highway Administration on December 9, 1991 (23 CFR 646), or as such form may be hereafter supplanted or amended, and any other pertinent instructions issued by the Federal Highway Administration, Department of Transportation, providing for a limit of not less than Five Million Dollars (\$5,000,000.00) for all damages arising out of the bodily injuries to or death of one person, and, subject to the limit for each person, a total limit of not less than Ten Million Dollars (\$10,000,000.00) for all damages arising out of bodily injuries to or death of two or more persons in any one accident; and providing for a limit of not less than Five Million Dollars (\$5,000,000.00) for all damages to or destruction of property in any one accident and subject to that limit a total (or aggregate) limit of not less than Ten Million Dollars (\$10,000,000.00) for all damages to or destruction of property during the policy period. A certified copy of the policy providing said Contractor's Public Liability and Property Damage Insurance executed by a corporation qualified to write the same in the State in which the work is to be performed, in form and substance satisfactory to the Grantor, shall be delivered to and approved by the Grantor's Chief Engineer prior to the entry upon or use of the Grantor's property and/or the Premises by the Contractor.
6. The Grantee or its contractor(s) shall telephone Grantor's communication Network Control Center at 1-800-336-9193 (a 24-hour number) to determine if fiber optic cable is buried anywhere on the Premises; and if so, the Grantee or its contractor(s) will contact the Telecommunications Company(ies) involved, and make arrangements with the Telecommunication company(ies) for protection of the fiber optic cable prior to beginning any work on the Premises.
7. If at any time the use of the Premises for the purposes herein should be abandoned or discontinued by the Grantee, the said easement over the herein described Premises shall thereupon cease and determine and the Grantee shall surrender or cause to be surrendered to the Grantor to its successors and assigns, the peaceable possession of said described Premises, and title to the said Premises shall remain in the Grantor, or its successors or assigns, free and clear of all rights and claims of the Grantee and of the public for use and occupancy of the said Premises.
8. The Grantor does not warrant its title to said Premises nor undertake to defend the Grantee in the peaceable possession, use or enjoyment thereof,

and the grant herein made is subject to all outstanding rights or interest of others, including the tenants and licensees of the Grantor.

TO HAVE AND TO HOLD THE SAME, together with all the hereditaments and appurtenances thereunto belonging to Grantee for public use and enjoyment for the purposes aforesaid and for no other purpose whatsoever subject to the terms and conditions hereinbefore stated.

IN WITNESS WHEREOF, said Union Pacific Railroad Company has caused this instrument to be signed by its authorized officers, and the corporate seal affixed on the 4th day of November, 2002.

Attest:

M. E. Heenan
Print Name: M. E. HEENAN
Title: Assistant Secretary
(Seal)

By: G. L. PINKER
UNION PACIFIC RAILROAD COMPANY

Name: G. L. PINKER
Title: Director-Contracts

ACCEPTED:
CITY OF SOUTH ST. PAUL
STATE OF MINNESOTA

By: Kathleen A. Gaylord
Name: Kathleen A. Gaylord
Title: Mayor

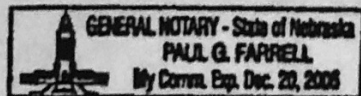
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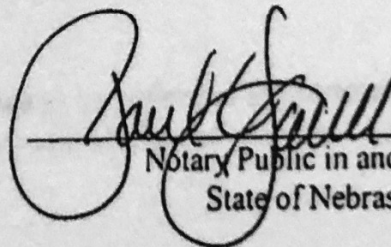
STATE OF NEBRASKA)
) ss
COUNTY OF DOUGLAS)

On this 4th day of November, 2002, before me, a Notary Public in and for said County and State personally appeared, Gregory L. Pinker and Mike Hecnan who are the Director Contracts and Assistant Secretary, respectively, of **UNION PACIFIC RAILROAD COMPANY**, a Delaware corporation, and who are personally known to me (or proved to me on basis of satisfactory evidence) to be the persons whose names are subscribed to in the within instrument, and acknowledged to me that they executed the same in their authorized capacities, and by their signatures on the instrument the persons, or the entity upon behalf of which persons acted, executed the instrument.

WITNESS my hand and official seal.

(Seal)





Notary Public in and for the
State of Nebraska

My Commission Expires: December 20, 2005

Exhibit A

Description of Permanent Easement:

A permanent easement for bridge, roadway, and utility purposes over, under and across that part of the Union Pacific Railroad Company right-of-way lying in Lot C, KOCHENDORFER'S ADDITION TO SOUTH ST. PAUL, MINN., according to the recorded plat thereof, and part of government Lot 6, Section 16, Township 28 North, Range 22 West, all in Dakota County, Minnesota, which lies within a 66.00 foot wide strip of land being 33.00 feet wide on each side of the following described center line:

Commencing at the south quarter corner of Section 16, Township 28 North, Range 22 West; thence on an assumed bearing of North 89 degrees 49 minutes 16 seconds East along the south line of the Southeast Quarter of said Section 16, a distance of 2682.31 feet to the southeast corner of said Section 16; thence North 0 degrees 40 minutes 47 seconds East, along the east line of said Section 16, a distance of 287.76 feet to Meander Corner Number 6, on the east line of Section 16; thence continuing North 0 degrees 40 minutes 47 seconds East, along a northerly extension of the last described line, a distance of 328.93 feet to the point of beginning of the center line to be described; thence South 55 degrees 02 minutes 51 seconds West, a distance of 636.02 feet to the southwesterly line of said Lots 1 through 11, inclusive, of said plat of KOCHENDORFER'S ADDITION TO SOUTH ST. PAUL, MINN., and said center line there terminating.

The side lines of said 66.00 foot wide strip of land to be prolonged or shortened to begin on the northeasterly right-of-way line, and end on the southwesterly right-of-way line, of said Union Pacific Railroad Company.

DOC. NO.

1997147

OFFICE OF THE COUNTY RECORDER
DAKOTA COUNTY, MINNESOTA

CERTIFIED THAT THE WITHIN INSTRUMENT
WAS RECORDED IN THIS OFFICE ON AND AT

2003 FEB -7 PM 12:00

JOEL T. BECKMAN, COUNTY RECORDER

BY: *JB* Deputy
FEE *25.00* SURCHARGE *5.00*

CASH ☐ CHECK ☐ ESCROW ☐

WELL ☐ CHARGE ☒

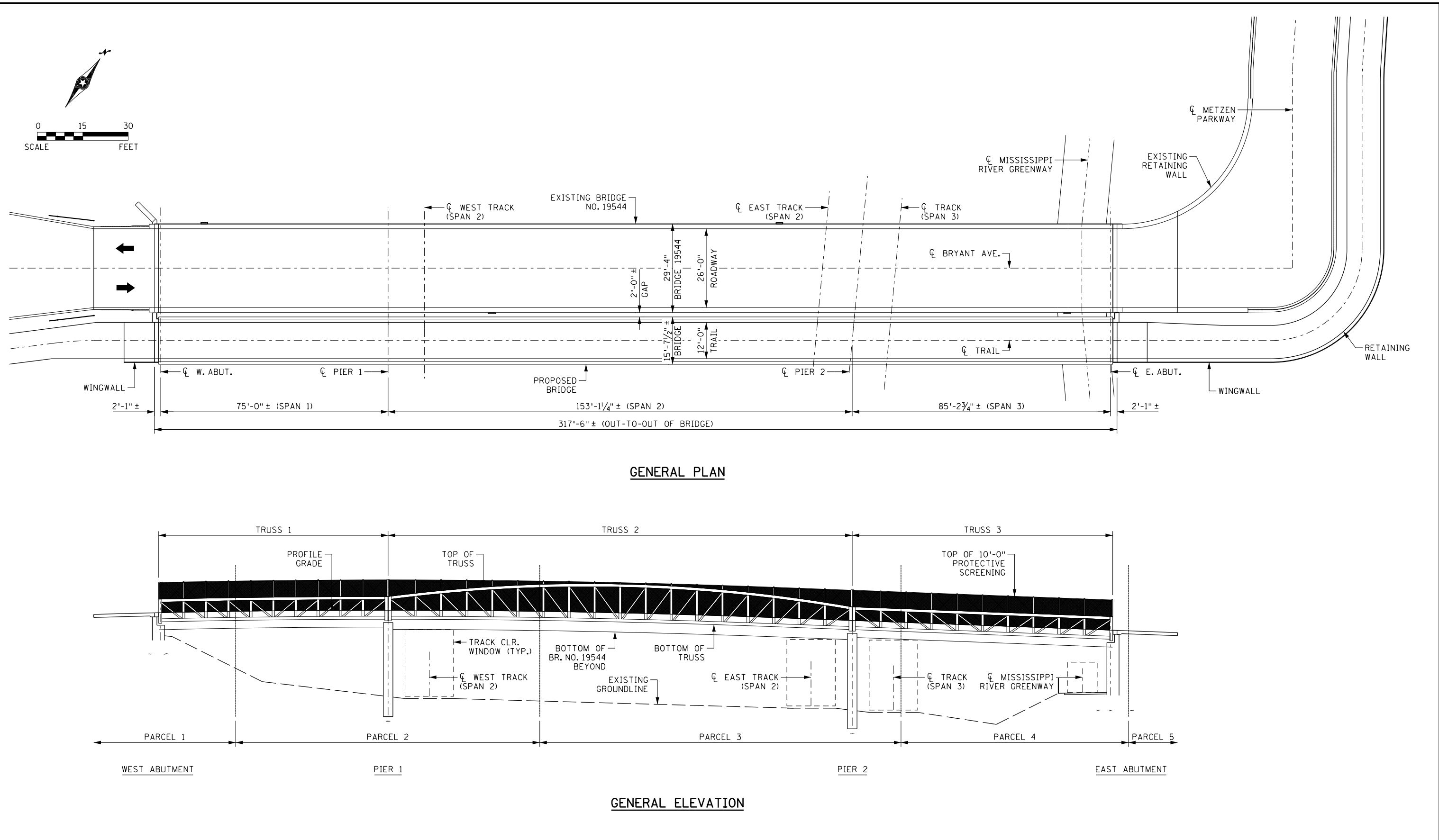
CHARGE TO: *CITY of So. St. Paul*
O/R _____

*1-2 City of So. St. Paul
125 Third Ave. N.
So. St. Paul, MN 55075*

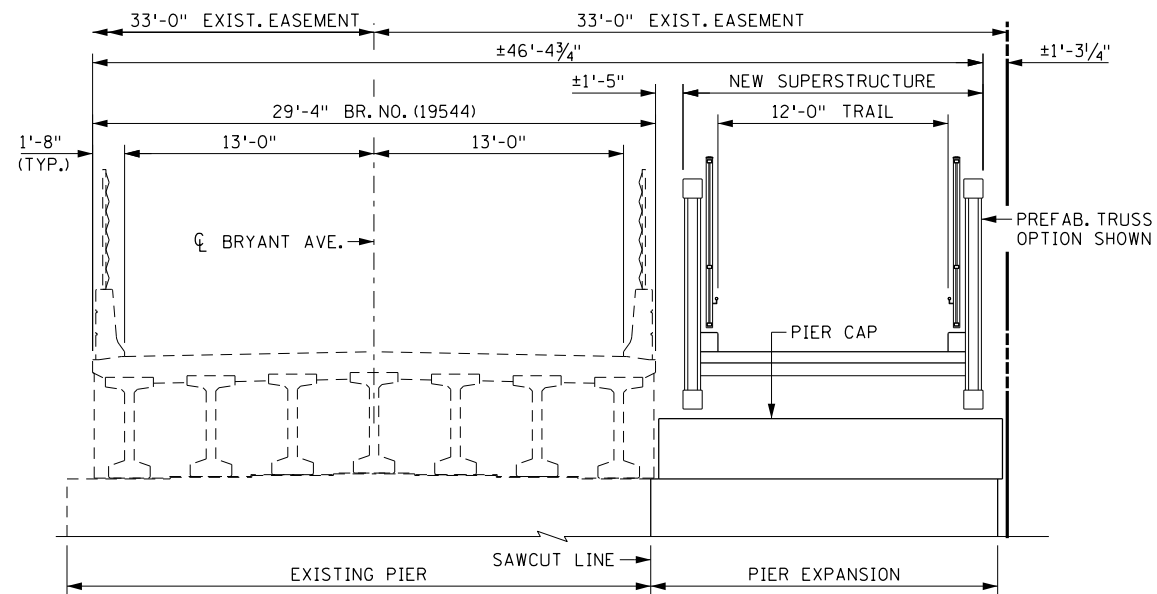
Appendix G

Bridge & Approach Type Alternatives

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TRANSVERSE SECTION - ALTERNATIVE B: BRIDGE NO. 19544 SUBSTRUCTURE MODIFICATION

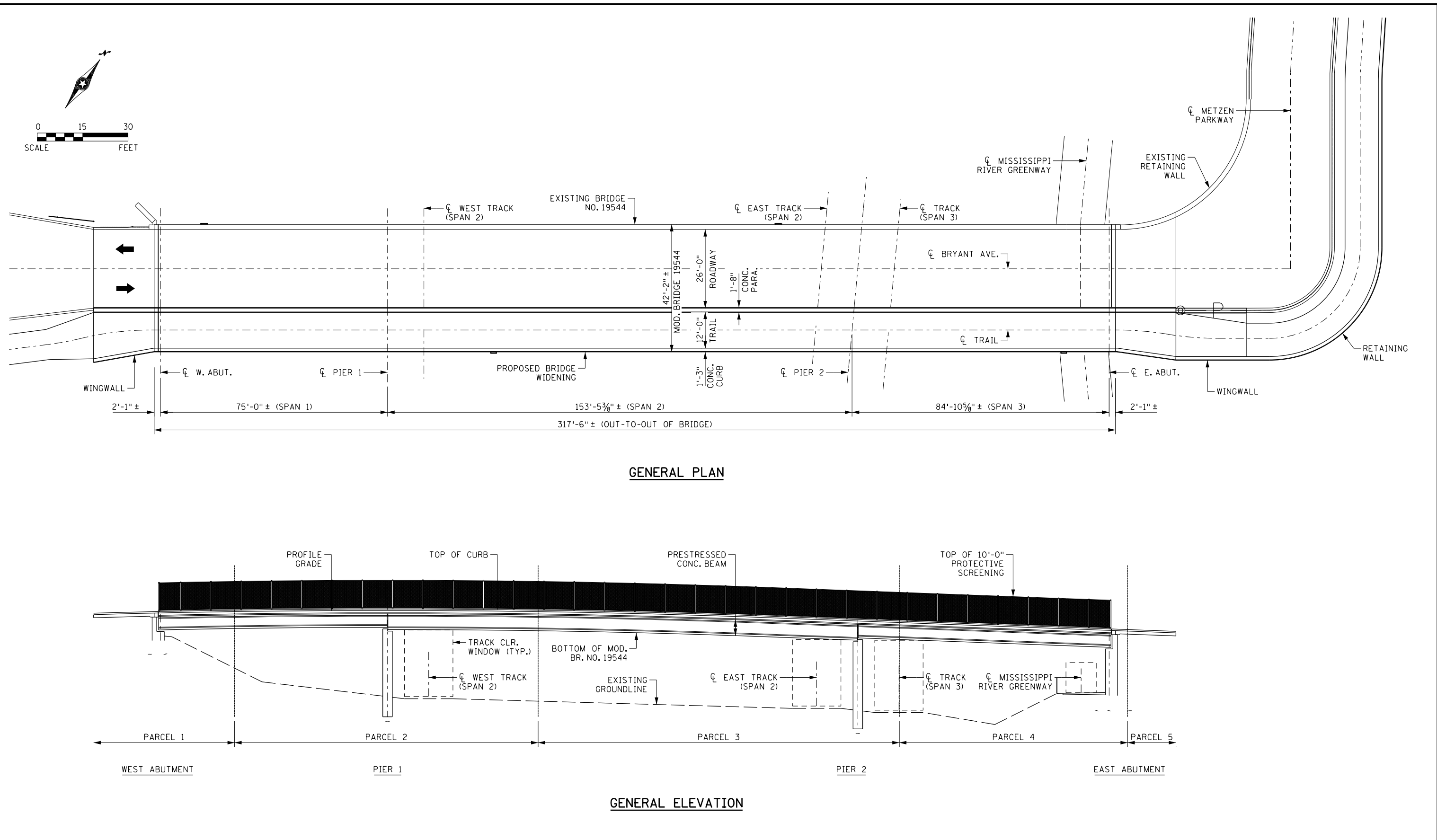


BRIDGE TYPE: PREFABRICATED STEEL TRUSS
PROJECT: LUCE LINE REGIONAL TRAIL BRIDGE OVER CSAH 61
LOCATION: PLYMOUTH, MN

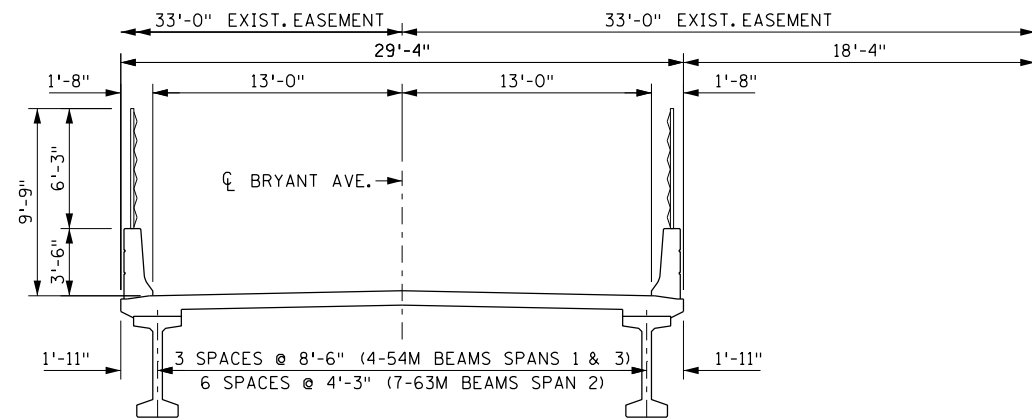


BRIDGE TYPE: PREFABRICATED STEEL TRUSS
PROJECT: COLUMBIA HEIGHTS PEDESTRIAN BRIDGE OVER CENTRAL AVE.
LOCATION: COLUMBIA HEIGHTS, MN

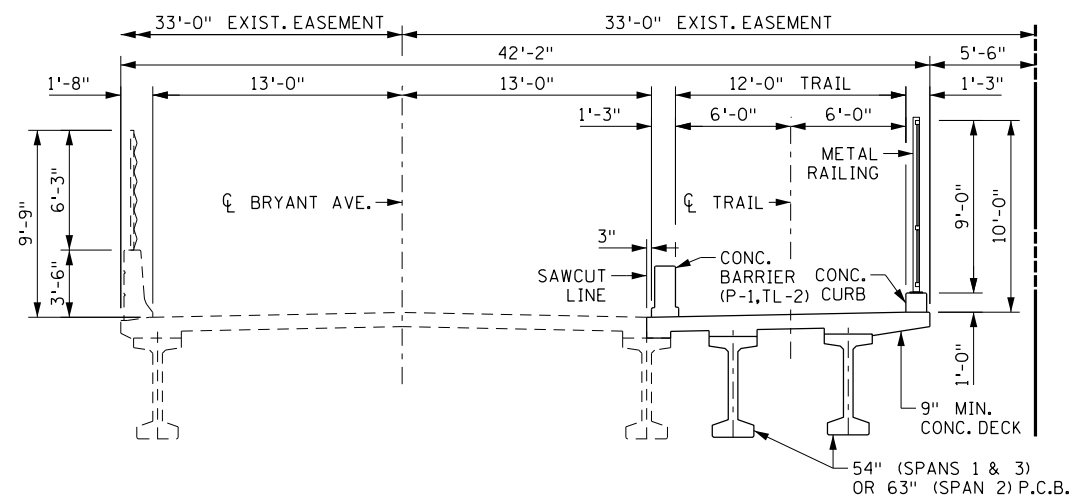
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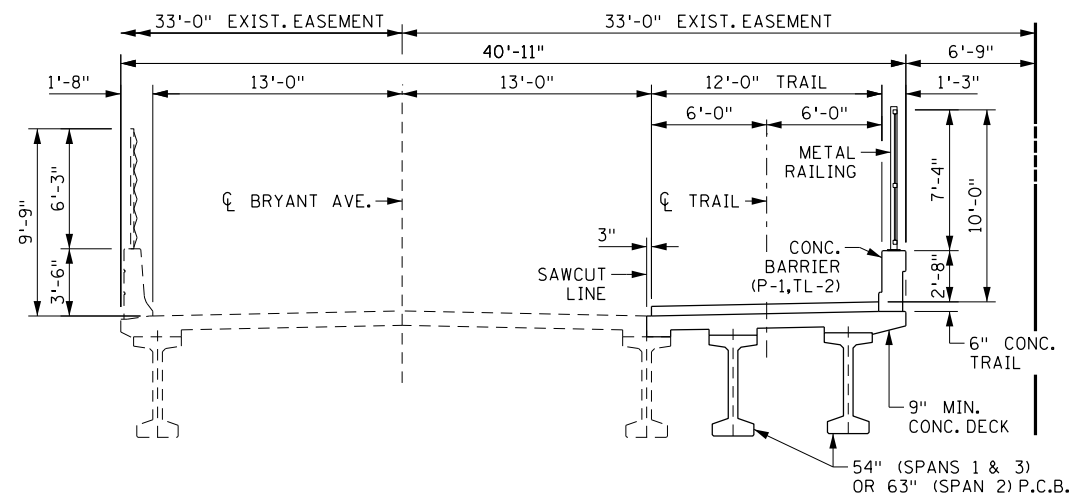
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TRANSVERSE SECTION - EXISTING BRIDGE NO. 19544



TRANSVERSE SECTION - ALTERNATIVE A1: BRIDGE NO. 19544 MODIFICATION



TRANSVERSE SECTION - ALTERNATIVE A2: BRIDGE NO. 19544 MODIFICATION

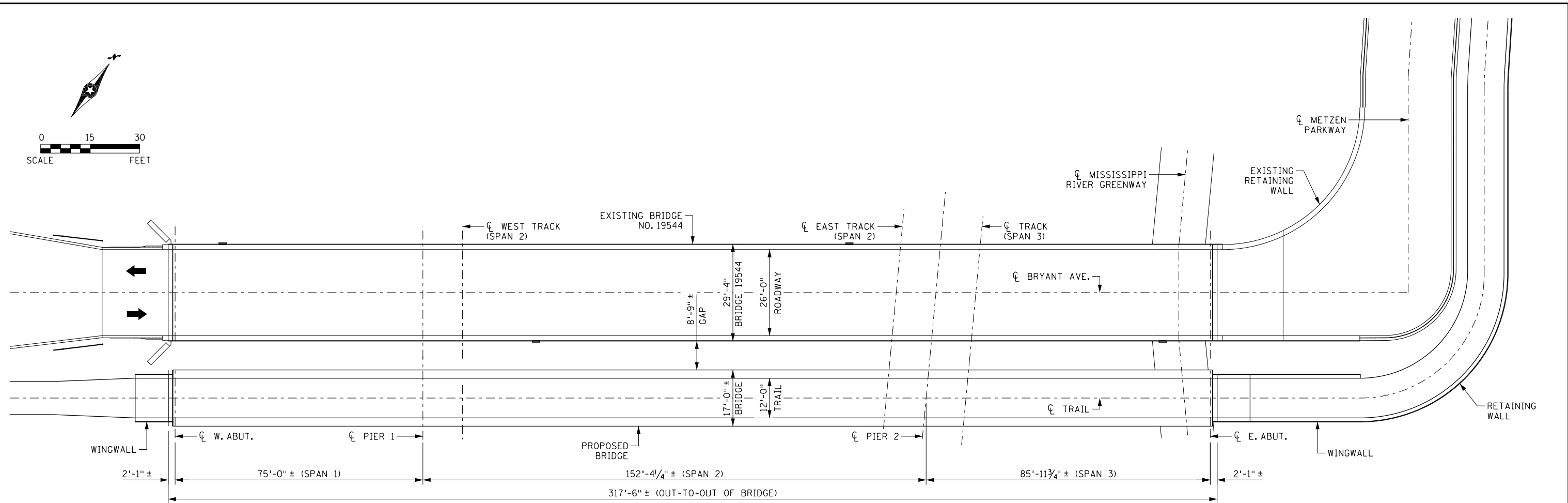


BRIDGE TYPE: PRESTRESSED CONCRETE BEAM (PROPOSED SIMILAR TO EXISTING)
PROJECT: EXISTING BRIDGE No. 19544, BRYANT AVENUE BRIDGE OVER RAILROAD
LOCATION: SOUTH ST. PAUL

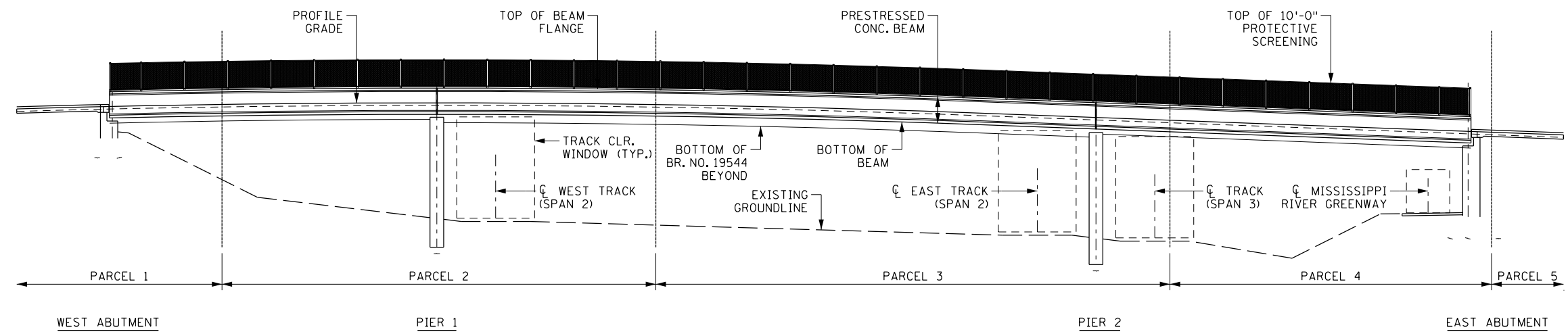


BRIDGE TYPE: BARRIER-SEPARATED TRAIL ON LOW SPEED ROAD BRIDGE
PROJECT: PLYMOUTH ROAD OVER MINNEHAHA CREEK
LOCATION: MINNETONKA, MN

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GENERAL PLAN



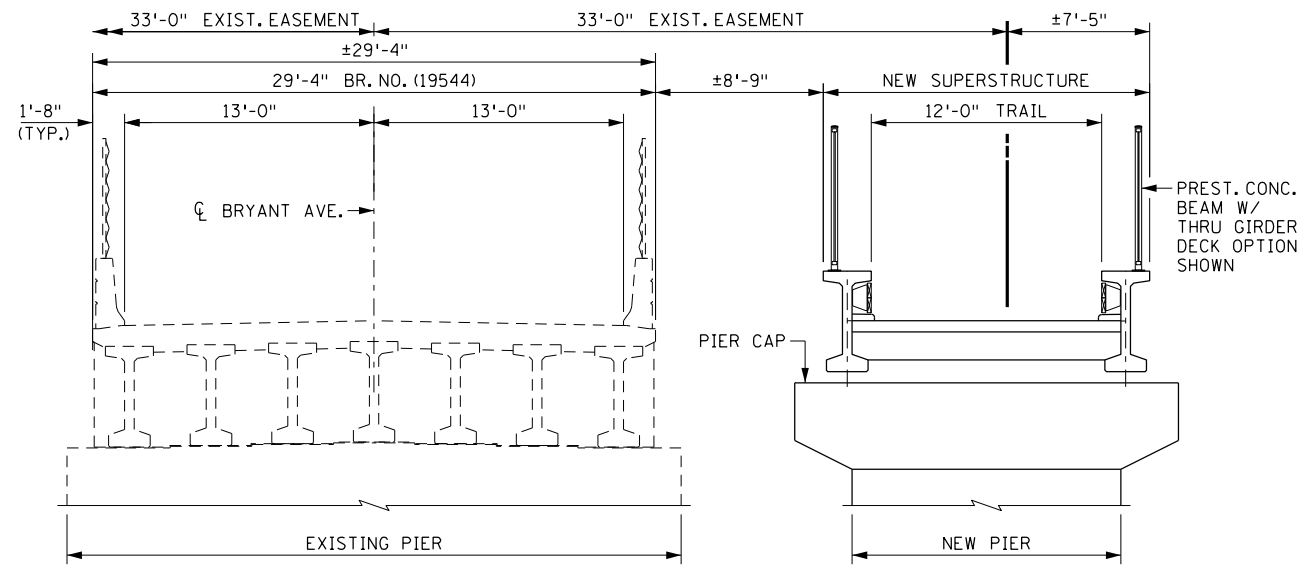
GENERAL ELEVATION



Job #13908
3/4/2021

Bridge Type - Alternative 3: Separate Trail Bridge
Bryant Avenue Pedestrian Bridge Feasibility Study
for the City of South St. Paul

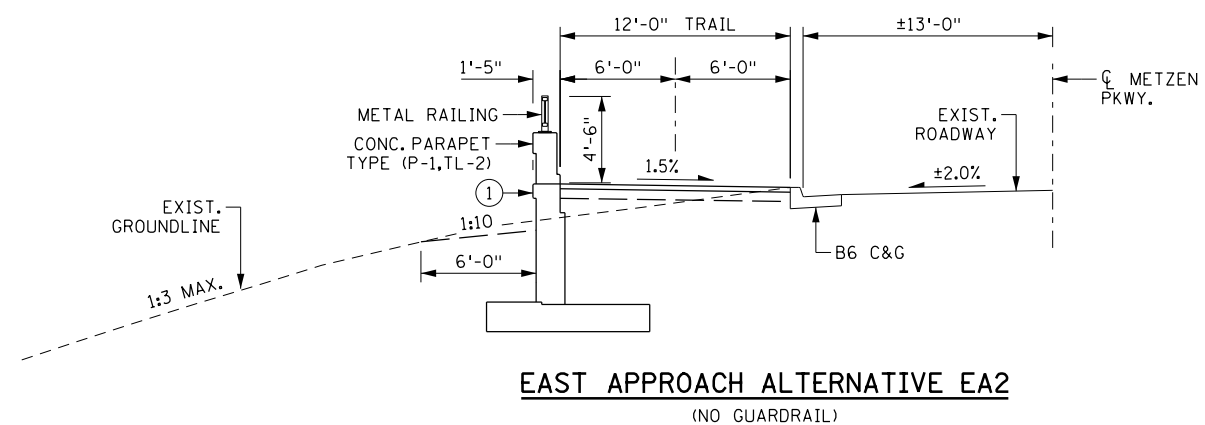
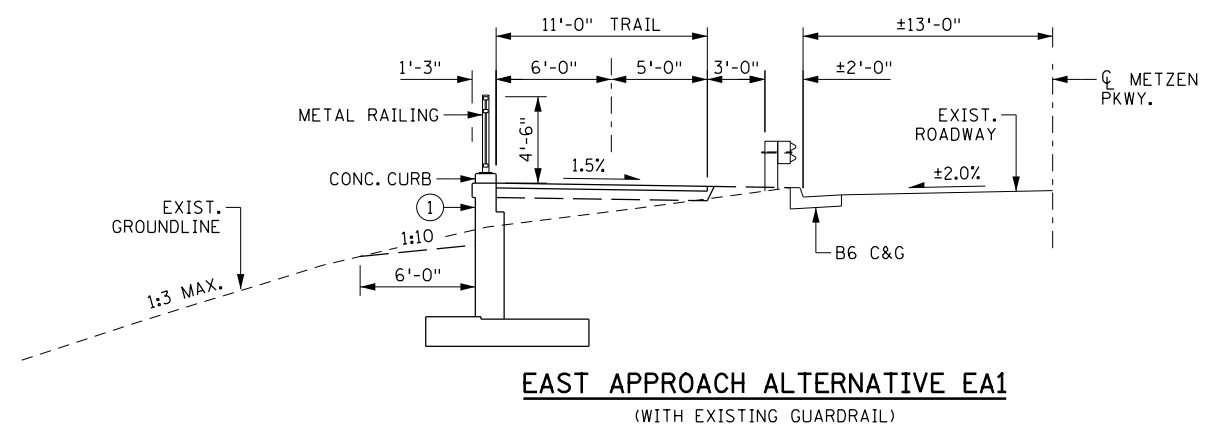
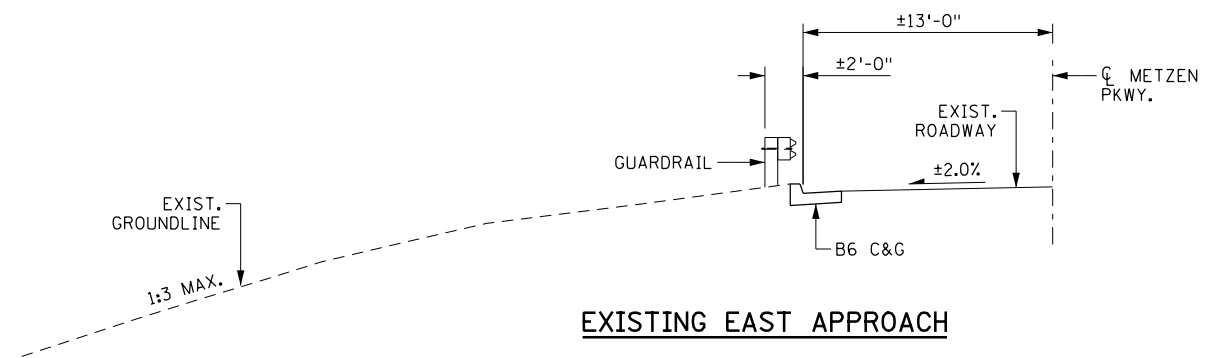
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TRANSVERSE SECTION - ALTERNATIVE B: BRIDGE NO. 19544 SUBSTRUCTURE MODIFICATION



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NOTES:

- ① RETAINING WALL, CAST-IN-PLACE (CIP) CONCRETE WALL SHOWN. ALTERNATE WALL TYPES MAY INCLUDE, BUT ARE NOT LIMITED TO:
- REINFORCED SOIL SLOPE (RSS)
 - PRECAST MODULAR BLOCK WALL (PMBW)



BRIDGE APPROACH TYPE: SEPARATED TRAIL W/ GUARDRAIL PROTECTION
PROJECT: 55TH STREET BRIDGE OVER S.FORK ZUMBRO RIVER
LOCATION: ROCHESTER, MN



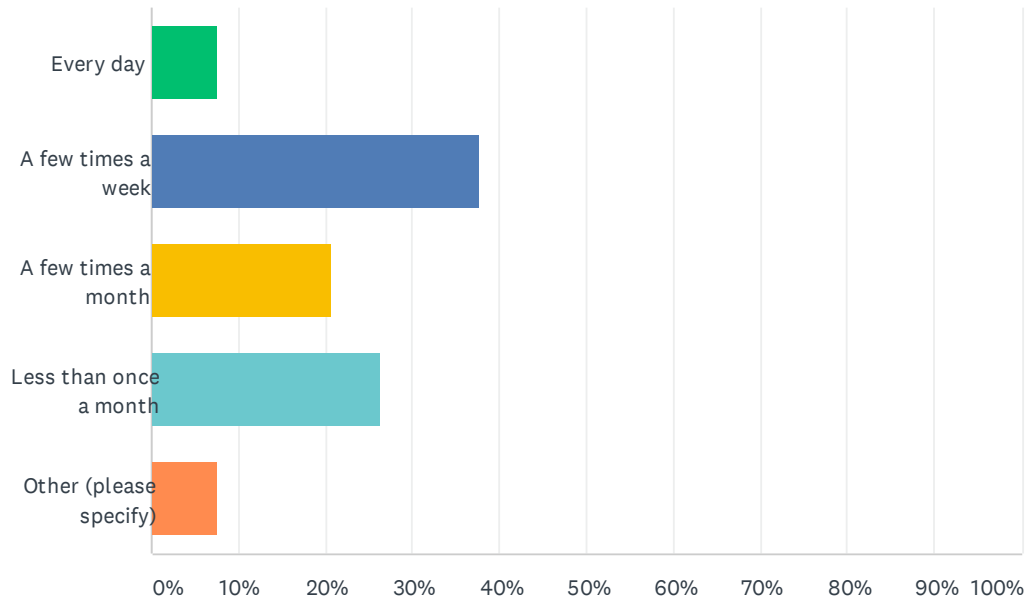
BRIDGE APPROACH TYPE: TRAIL AT BACK OF CURB & GUTTER
PROJECT: HANSON BLVD. OVER BNSF RAILWAY CO.
LOCATION: COON RAPIDS, MN

Appendix H

Survey Results & Public Comment Log

Q1 How often do you visit the Kaposia Landing area?

Answered: 53 Skipped: 0

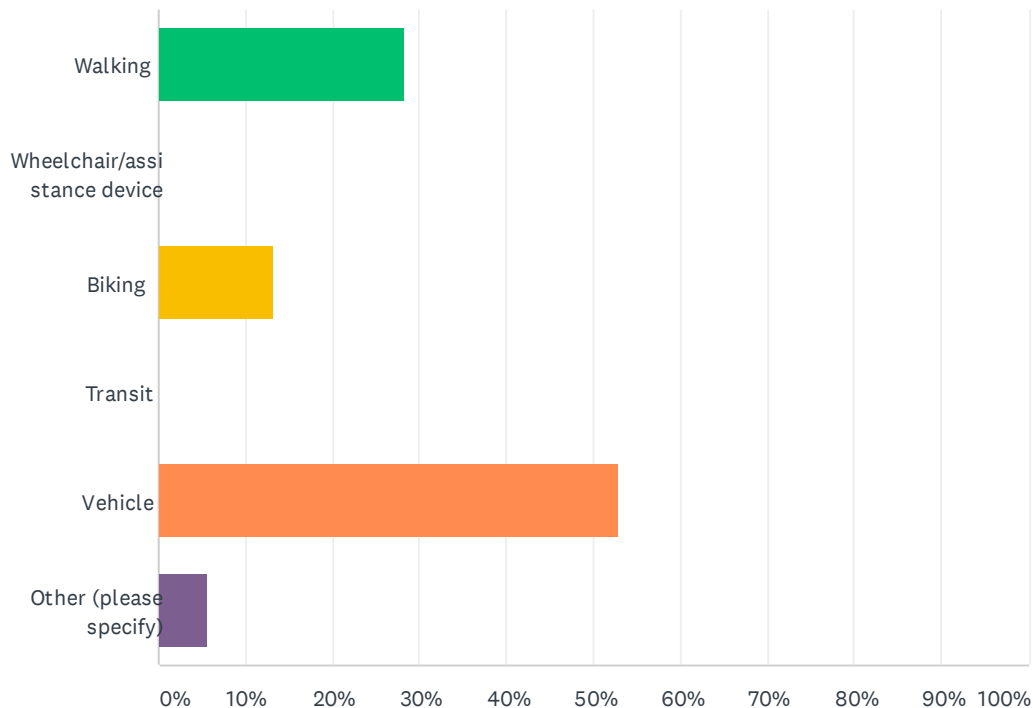


| ANSWER CHOICES | RESPONSES | |
|------------------------|-----------|----|
| Every day | 7.55% | 4 |
| A few times a week | 37.74% | 20 |
| A few times a month | 20.75% | 11 |
| Less than once a month | 26.42% | 14 |
| Other (please specify) | 7.55% | 4 |
| TOTAL | | 53 |

| # | OTHER (PLEASE SPECIFY) | DATE |
|---|---|--------------------|
| 1 | A few times a week to almost every weekday. | 6/23/2021 8:23 PM |
| 2 | Never | 6/22/2021 9:05 AM |
| 3 | Never and lived in this city 38 years | 6/21/2021 11:38 PM |
| 4 | rarely | 6/21/2021 3:26 PM |

Q2 When you visit Kaposia Landing, how do you get there?

Answered: 53 Skipped: 0

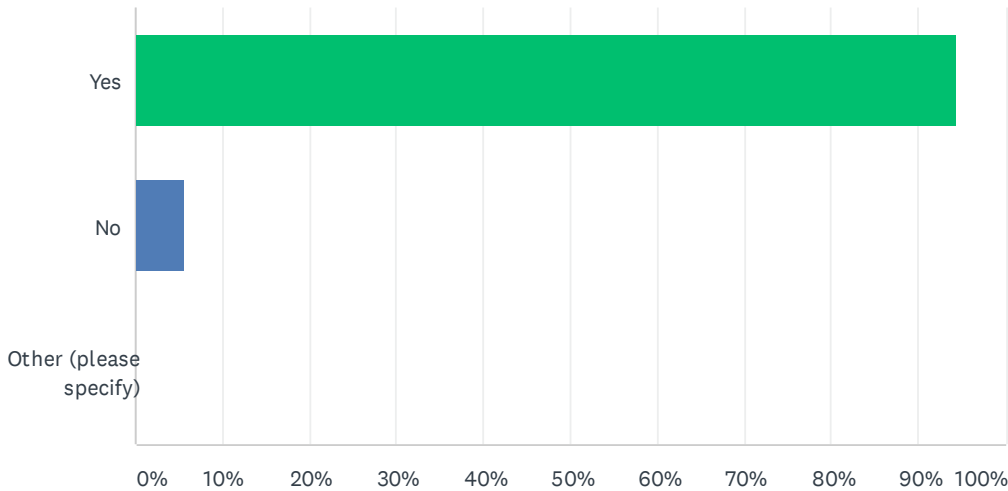


| ANSWER CHOICES | RESPONSES | |
|------------------------------|-----------|-----------|
| Walking | 28.30% | 15 |
| Wheelchair/assistance device | 0.00% | 0 |
| Biking | 13.21% | 7 |
| Transit | 0.00% | 0 |
| Vehicle | 52.83% | 28 |
| Other (please specify) | 5.66% | 3 |
| TOTAL | | 53 |

| # | OTHER (PLEASE SPECIFY) | DATE |
|---|--------------------------|--------------------|
| 1 | Walking and biking | 6/23/2021 8:23 PM |
| 2 | N/A | 6/22/2021 9:05 AM |
| 3 | I dont go there no point | 6/21/2021 11:38 PM |

Q3 After viewing the project video, do you feel that a pedestrian bridge and crossing improvements will improve safety and accessibility to the Kaposia Landing area?

Answered: 53 Skipped: 0



| ANSWER CHOICES | | RESPONSES | |
|------------------------|--|-----------|----|
| Yes | | 94.34% | 50 |
| No | | 5.66% | 3 |
| Other (please specify) | | 0.00% | 0 |
| TOTAL | | | 53 |

| # | OTHER (PLEASE SPECIFY) | DATE |
|---|-------------------------|------|
| | There are no responses. | |

Q4 Please provide any comments or questions you may have about the improvements being considered for Bryant Avenue at Kaposia Landing.

Thank you!

Answered: 37 Skipped: 16

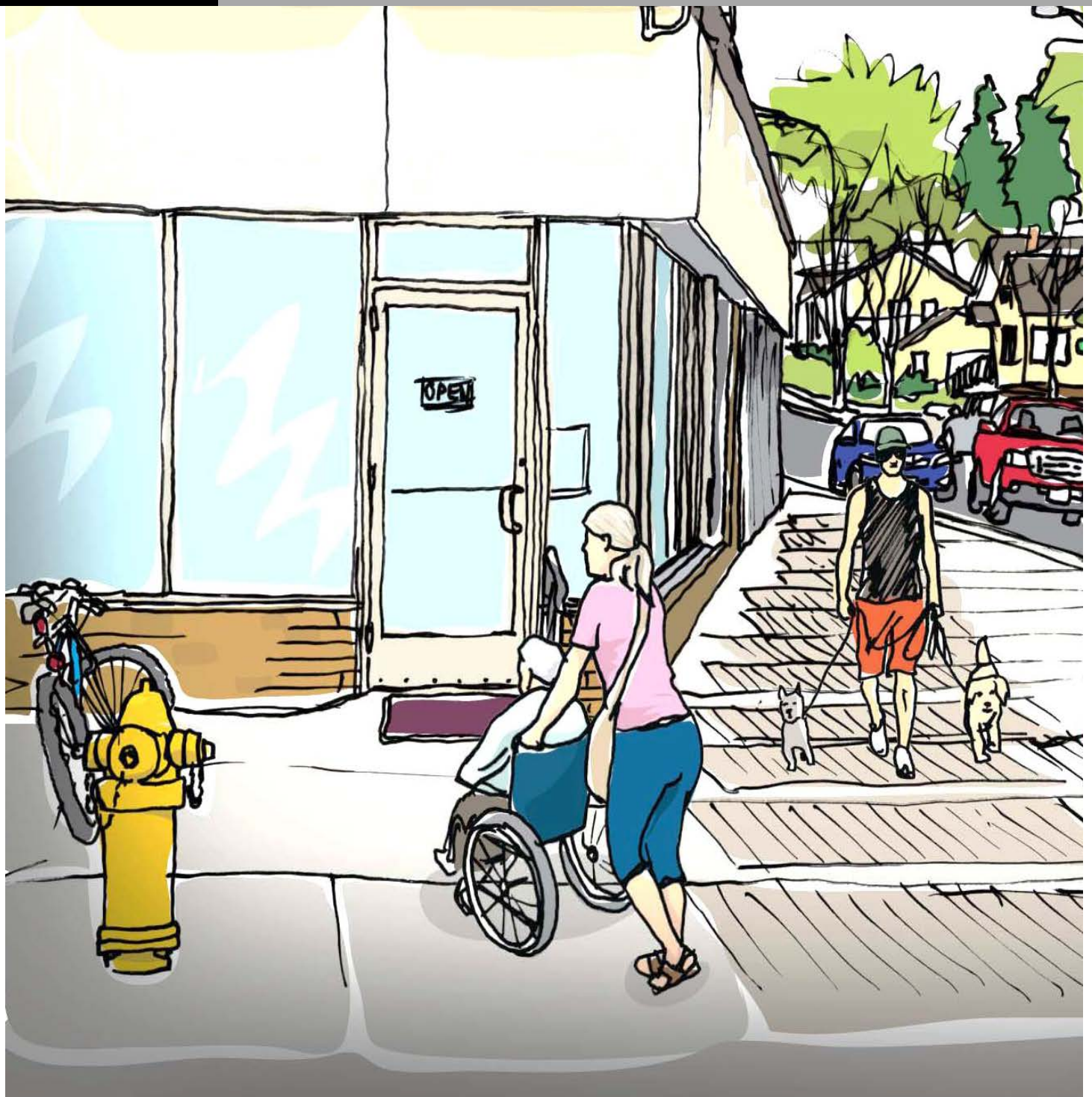
| # | RESPONSES | DATE |
|----|--|--------------------|
| 1 | Amazing! So needed! Thank you! | 7/10/2021 4:38 PM |
| 2 | So glad this has been flagged as a safety concern and that plans are underway. This is very needed as slightlines around the 90 degree turn are a problem. Some suggestions: 1. Where the new path crosses the roadway at the bottom of the hill after going over the bridge, make the path a different color so cars realize bikes may be coming from the side and that they are crossing a bike way. Make it clear who will have the right of way. Bikes, I hope - like in the Netherlands! This circle at the bottom is presently kind of a free-for-all. No one knows who should go first. 2. At the Bryant/concord lights install signals that allow bikes/pedestrians to enter the intersection before the light turns for cars. That way the cars see the bikes before they turn. Making the crossing path a different color here, too, would also be great, so cars don't stop on top of the bike way. 3. The older trails in the park need to be resurfaced. They are very bumpy and hard to ride on. | 7/4/2021 12:47 PM |
| 3 | The park is great! I bike to it often on the north and south entrance's. Please build a extra path on the south side along Bryant as you proposed. Thanks a ton. Also please maintain the trails in kaposia park, because they are cracking up all over. Please fix the big cracks on the trails. Thanks | 7/4/2021 12:45 PM |
| 4 | This is much needed--have walked across the bridge many times and always a bit nerve-wracking. Too bad this wasn't incorporated in the original bridge! The option recommended in the video looks like it will work well. Thanks! | 7/2/2021 7:45 PM |
| 5 | I can't wait for this to be done! | 7/2/2021 2:20 PM |
| 6 | Cheapest option looks great. | 7/1/2021 1:52 PM |
| 7 | Will definitely make me feel safer while crossing the bridge on my bike - to access the trail system and/or return home after a ride. | 6/30/2021 12:22 PM |
| 8 | Needs to be done. Should have been done a while ago. I know it is not on the list but it seems as the park grows and continues to have more users and eventually a band shell wouldn't the better thing be to build at least a three lane bridge along with a dedicated bike lane(s)? It seems we were short sighted in the use of the park previously so why make this mistake again? | 6/23/2021 8:23 PM |
| 9 | There is a severe left turn when traveling into the part, (after crossing the bridge) if adding pedestrian or bike lanes here it would be beneficial to keep us safe as currently it's a little scary if walking across the bridge. | 6/23/2021 7:25 PM |
| 10 | There isn't much traffic on the Bryant Ave bridge. Until there is more why spend the money. | 6/23/2021 2:41 PM |
| 11 | I know people bitch about construction, but making things more accessible on foot is a huge step in creating a more equitable city. Thank you! | 6/23/2021 12:17 PM |
| 12 | Thank you for doing this! It always felt a bit unsafe to use the bridge with my kid in a stroller - this will make accessing the park much easier. | 6/23/2021 11:06 AM |
| 13 | What equipment will you be using for snow removal? Can the option handle the weight and will it be wide enough for that equipment? What kind of rails are you using to prevent things from falling below and train jumpers? What are each of the options estimated life spans and future maintenance costs (repainting or decking)? Where and how is the rain water going to be shed off and to? | 6/22/2021 10:36 PM |
| 14 | Why didn't they think about this issue when they initially built this project? | 6/22/2021 10:11 PM |

Bryant Avenue Pedestrian Bridge Study

| | | |
|----|--|--------------------|
| 15 | Pedestrian Bridge is so needed! | 6/22/2021 8:23 PM |
| 16 | Great improvement | 6/22/2021 4:52 PM |
| 17 | Bryant Bridge is the easiest way for me to access the trail. I walk with my dogs and if there are cars in both lanes it doesn't leave a lot of room for me and my dogs. I would feel much safer with a pedestrian bridge | 6/22/2021 12:39 PM |
| 18 | I think this is a must, I am always worried about the safety of bikers and walkers that use the existing Bryant Ave Bridge | 6/22/2021 8:06 AM |
| 19 | People walk on the vehicle bridge all the time, which can be dangerous given the sharp turn at the end. There absolutely needs to be a way to make Kaposia landing more accessible at Bryant/Concord, it's ridiculous that a pedestrian lane/sidewalk wasn't included when it was first built. | 6/21/2021 11:57 PM |
| 20 | No point never go there never will no need for this | 6/21/2021 11:38 PM |
| 21 | This is definitely needed to keep people safe trying to go over this bridge who are on foot or on bikes, it's so narrow. | 6/21/2021 10:39 PM |
| 22 | When I do visit, I am always concerned about pedestrians, I think Option 1 would be great! | 6/21/2021 10:36 PM |
| 23 | I think safety should always be a priority and this is a good idea. | 6/21/2021 10:23 PM |
| 24 | I think option c is the better choice. It is easier to do at least that's my opinion | 6/21/2021 10:19 PM |
| 25 | Please put something so drivers will quit honking and acting like you're in their way. I walk my dogs and had a man call me, a 60 year old woman, a stupid bitch because I was "on the road". No, I was on the side. He then told me to use the sidewalk. There is no sidewalk. We use it daily whilst walking our dogs. We would so appreciate something! Thank you | 6/21/2021 6:35 PM |
| 26 | I think it will greatly improve pedestrian safety, with distracted driving being a common concern now | 6/21/2021 5:34 PM |
| 27 | I live on Byrant Ave. I drive down there because I don't feel safe walking across the bridge. | 6/21/2021 5:14 PM |
| 28 | We live on the north end of SSP and would likely visit the park more often if it was safer to walk there. I would want signage about bikes and whether or not they would yield to pedestrians or how that would go. | 6/21/2021 4:58 PM |
| 29 | Would much rather see some shaded seating over looking the river. | 6/21/2021 4:55 PM |
| 30 | Option 1 seems best | 6/21/2021 4:43 PM |
| 31 | This is long over do and should be done as soon as possible. | 6/21/2021 3:33 PM |
| 32 | I have always wondered why there wasn't already a foot bridge there. Right now people crossing on foot or on a bicycle have to be extremely careful, and I've seen many people walking and biking across trying to avoid cars. It's a pretty dangerous situation right now, so I feel a footbridge there is essential there. | 6/21/2021 3:26 PM |
| 33 | No comments in the video were made about the life of the existing vehicle bridge. That should be put into consideration before tying a pedestrian bridge onto the existing bridge vs taking the cheaper option of constructing a stand alone pedestrian bridge. Also I need better explanation why more expensive option 1 is better than cheaper option 3 other than it's less administrative effort of dealing with the railroad company. You need to pinch every penny available when you're spending someone else's money. | 6/21/2021 3:25 PM |
| 34 | My wife and I live up the hill and often go down to the landing on walks. The added pedestrian bridge would be a very welcome addition to make it safer for our walks. | 6/21/2021 2:53 PM |
| 35 | Would be a great addition to be able to take my dog on longer walks from the dog park | 6/21/2021 2:49 PM |
| 36 | None at this time. How long will the project to build from start to finish? | 6/21/2021 2:33 PM |
| 37 | The current bridge is dangerous for walkers/dogs. The blind corner coming out of the clinic is tricky. A pedestrian bridge would be a useful and helpful option in accessing this beautiful park. | 6/21/2021 2:07 PM |

SOUTH
ST. PAUL

DRAFT BICYCLE AND PEDESTRIAN PLAN NOVEMBER 7, 2014



I. Executive summary

The City of South St. Paul recognizes that walking and bicycling infrastructure benefit its residents and businesses. Every person is a pedestrian at some point in their day; whether they are walking to school, a bus stop, a park, or simply walking from their parking space into their office building. While bicycling remains less common than walking, many residents enjoy bicycling for exercise and are interested in bicycling more often.

Walkable and bikeable communities have a high quality of life, improve personal and environmental health, and promote vibrant and connected communities. Walkable and bikeable communities are economically sustainable. Residents do not have to rely on a costly personal vehicle, and are more likely to support local businesses that can be easily reached on foot and bike. Pedestrian and bicycle infrastructure is also cost-effective for public agencies: sidewalks and bikeways are less expensive to maintain than roadways, and walkable and bikeable communities result in less land use tied up in parking.

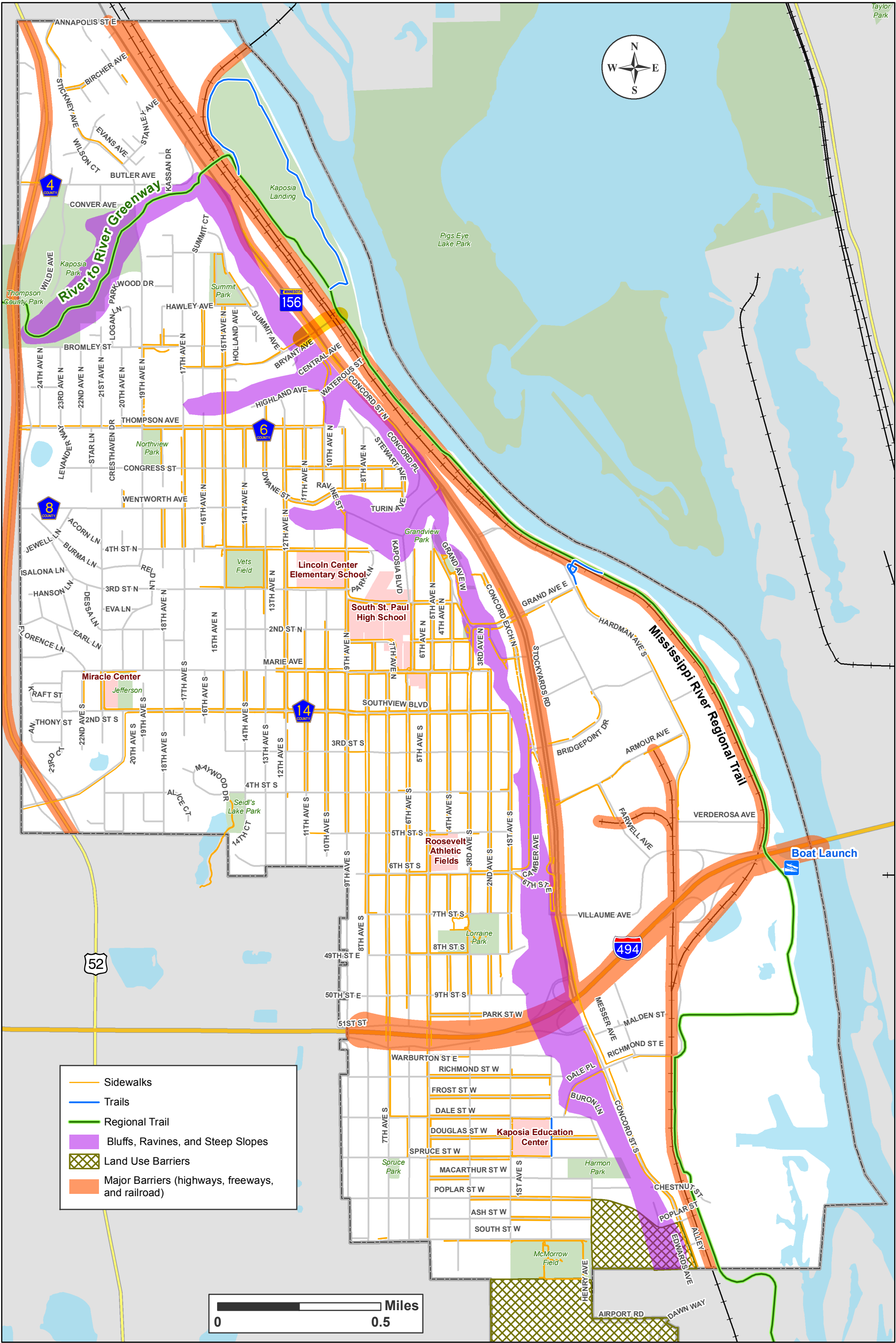
This plan addresses the City of South St. Paul's role in making walking and bicycling safe and easy choices for residents. This plan will guide the city's efforts to reach the following goals:

1. Improve opportunities for walking and bicycling through development of a sidewalk, trail, and bikeway system that connects to community destinations and public transit
2. Plan and provide a safe and comfortable sidewalk, trail, and bikeway system that meets the needs of residents of all ages and abilities
3. Ensure that critical links in the sidewalk, trail, and bikeway system receive regular and year-round maintenance
4. Improve the health of South St. Paul residents through walking and bicycling
5. Build a vibrant, healthy, sustainable, and livable community by making walking and bicycling easy, convenient, and safe
6. Increase rates of walking and bicycling

The recommendations of this plan are tailored to help the city reach these goals. This plan is guided by a 5 Es approach to bicycle and pedestrian planning: engineering, education, encouragement, enforcement, and evaluation. Recommendations in this plan include:

- Establish an Arterial Sidewalk Network based on priority pedestrian connections
- Identify critical gaps in the sidewalk system
- Identify bicycle network, including multi-use trails, bike lanes, and bicycle boulevards
- Maintenance recommendations for sidewalks and bikeways
- Community outreach to encourage walking and bicycling
- Support Safe Routes to School programs
- Educate residents about safe walking, bicycling, and driving behavior

The City of South St. Paul will lead the implementation of this plan, following the strategies and priorities outlined in Chapter II: Implementation. The city will track key performance measures on an annual basis to ensure progress towards the goals of this plan.



South St. Paul Bicycle & Pedestrian Plan
Figure 6: Barriers to Walking and Bicycling



Interstate 494 (I-494)

I-494 separates South St. Paul's southernmost neighborhoods from the rest of the city. As I-494 is a freeway, all pedestrian and bicycle crossings are grade-separated. The pedestrian and bicycle crossings of I-494 are in the following locations:

- 5th Avenue South (overpass)
- 7th Avenue South (overpass)
- Concord Street (underpass)
- Verderosa Avenue (underpass)
- Mississippi River Regional Trail (underpass)

Union Pacific Railroad

The Union Pacific Railroad is a barrier to access to the MRRT. The railroad parallels the Mississippi River and is located between the MRRT and Concord Street. Most pedestrian and bicycle crossings of the railroad are grade-separated. Grade-separated crossings are in the following locations:

- Kaposia Landing: near Simon's Ravine Trailhead
- Bryant Avenue
- Grand Avenue

Though there are no sidewalks or trails on most of these roadways, pedestrians and bicyclists can cross the railroad at-grade on the following roadways:

- Verderosa Avenue (underneath I-494)
- Hardman Avenue (south of I-494)
- Richmond Street
- Chestnut Street

Known pedestrian/bicycle safety problems

Pedestrian and bicycle safety is a primary concern for the City of South St. Paul. Understanding where pedestrian and bicycle crashes have occurred will help the city better target safety improvements. A search of the Minnesota Department of Transportation (MnDOT) Crash Mapping Analysis Tool (CMAT) data showed a total of 24 pedestrian-vehicle and 60 bicycle-vehicle crashes in South St. Paul in the 10-year period between 2004 and 2013. **Figure 7** shows the location of traffic crashes involving pedestrians and bicyclists.

Crashes involving pedestrians are not concentrated at any particular intersection. Southview Boulevard, Thompson Avenue, 5th Avenue S, and South Street W are the only streets with multiple pedestrian-vehicle crashes over the 10-year period. There were no pedestrian fatalities reported during this time.

Crashes involving bicyclists were concentrated along several corridors, including Concord Street, Thompson Avenue, Marie Avenue, Southview Boulevard, and 5th and 7th Avenues South. There were two fatal bicycle-vehicle crashes over the 10-year period: at the intersections of Marie and 3rd Avenues and Poplar and Concord Streets.

7. Community Engagement

Input from residents was a major component in the development of this plan. A number of community engagement techniques were used to gather input and feedback. A summary of those activities and the input they garnered are described in this chapter.

| Strategies Employed | |
|---|---|
| Mobile Display Materials June – August 15 | The mobile display materials were developed as part of a traveling booth that was set up at key community locations to advertise the study and to encourage residents to participate in the planning process. These materials were used to advertise the online survey, to provide background information on the study and to highlight upcoming events associated with the study. The display materials included flyers, bookmarks and a community survey. The mobile display was set up at the library, city hall and at the community engagement events. |
| Website and Social Media Updates May - December | The city has regularly posted information about the study on its website and on other social media outlets. It will continue to provide updates throughout the course of the study so that residents can easily obtain current information about the study process. |
| Survey June 1 - August 15 | A survey was developed to collect information on existing bicycling and walking habits, barriers to walking and bicycling, and desires for a pedestrian and bicycle network within the community. This survey was posted online and hard copies were made available as part of the mobile display materials that were used at community events and were stationed at the library and at city hall. |
| Project Press Release June/November | <p>In an attempt to provide broad coverage about the bicycle and pedestrian plan and its associated community engagement activities, a press release was prepared and distributed to contacts at the St. Paul Pioneer Press, Star Tribune, South St. Paul Voice, and television and web-based media. The first news release was distributed on June 9, 2014. The press release generated an article about the plan which was published in the St. Paul Pioneer Press on June 14, 2014. An article was also published in the June/July edition of the South St. Paul city newsletter.</p> <p>A second press release was distributed in November when the draft plan was ready for public review and comment.</p> |
| City Council Meetings June/November | Information about the study was presented at the June 2, 2014 City Council meeting. At this meeting, the general scope of the plan was presented to the council. |

| | |
|--|---|
| | <p>The council also provided information about their interests regarding the plan and some of their priorities.</p> <p>A second meeting with the City Council was held in November to present a draft of the Bicycle and Pedestrian Plan and to incorporate any council comments.</p> |
| Community Outreach Events | |
| Kaposia Days June 29 | <p>The mobile display was set up at the Kaposia Days event to gather input on the needs and priorities for bicycling and walking within the city. Input was received by 15 people. Staff was available at the event to answer questions and to encourage residents to provide feedback.</p> |
| Fare for All July 8 | <p>The mobile display was set up at the Fare for All event at Central Square to solicit input from the public. 20 participants provided input. Staff was available at the event to answer questions and to encourage residents to provide feedback.</p> |
| Senior Outreach July 14 | <p>The city and its consultant led a discussion at the John Carrol Senior high rise building to engage input from seniors living within the community. They were asked to provide their input on the existing networks, barriers that limit their use of the networks and new opportunities they would like to have incorporated into the plan. This was a great way to make certain the senior and disabled populations were heard during this process. Approximately 10 residents and staff members participated in this event.</p> |
| Mayor's Youth Task Force July 30 | <p>The city and its consultant attended a meeting of the Mayor's Youth Task Force to conduct a visioning and information gathering session on the needs and priorities for bicycling and walking. 20 students provided their feedback.</p> |
| Swimming Under the Stars July 30 | <p>The mobile display was set up at the Swimming Under the Stars event party at Northview Pool. Surveys were available to fill out and a drawing for a prize was held for those that successfully completed the survey. 15 children participated at this event. Staff was available at the event to answer questions and to encourage children to participate in the survey.</p> |
| Southview Boulevard/3rd Avenue Open House August 7 | <p>The mobile display was set up at one of the open houses being held for the Southview Boulevard/3rd Avenue project. Staff was available at the event to answer questions and to encourage attendees to participate in the survey. Approximately 7 people provided their input at this meeting.</p> |

Findings

Below is a summary of the findings from the community engagement activities. This information will be used to aid in the development of the Bicycle and Pedestrian Plan.

In-Person Community Outreach

The project team collected in-person feedback from approximately 87 residents at six events. The following is a summary of the general themes provided by community members. Detailed summaries of each event are included as an appendix to this memo.

Community assets for walking and bicycling:

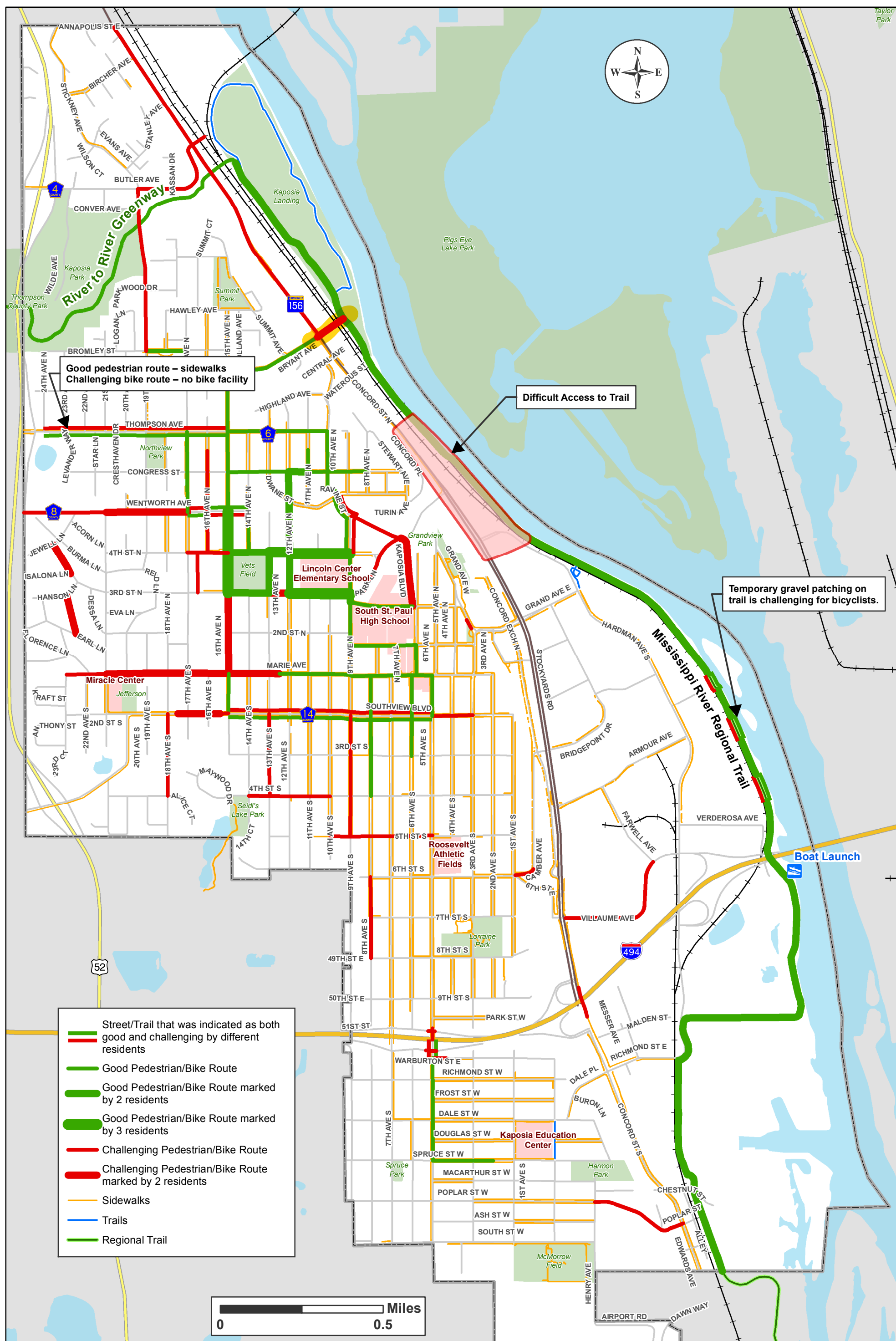
- Overall, people commented that they enjoy walking and biking in South St. Paul and are very pleased with the winter and summer maintenance provided by the city for trails.
- The Mississippi River Regional Trail is an asset to the community and is used regularly by residents.

General comments:

- Lighting along trails is desired to improve conditions during the fall and winter months.
- The bluffs and ravines are a barrier to walking and bicycling in the city, particularly for people trying to access the Mississippi River Regional Trail.
- A trail or shoulder is needed on the Bryant Ave to Kaposia Park to provide access to Kaposia Landing.
- Several residents mentioned the need for education about walking and bicycling in the city. Seniors suggested the city distribute flags for wheelchairs/scooters to improve visibility of these users. Younger members of the community mentioned that drivers need to be educated about stopping for pedestrians in crosswalks.



Meeting with Mayor's Youth Task Force



South St. Paul Bicycle & Pedestrian Plan

Figure 8: Results of Community Mapping Exercise

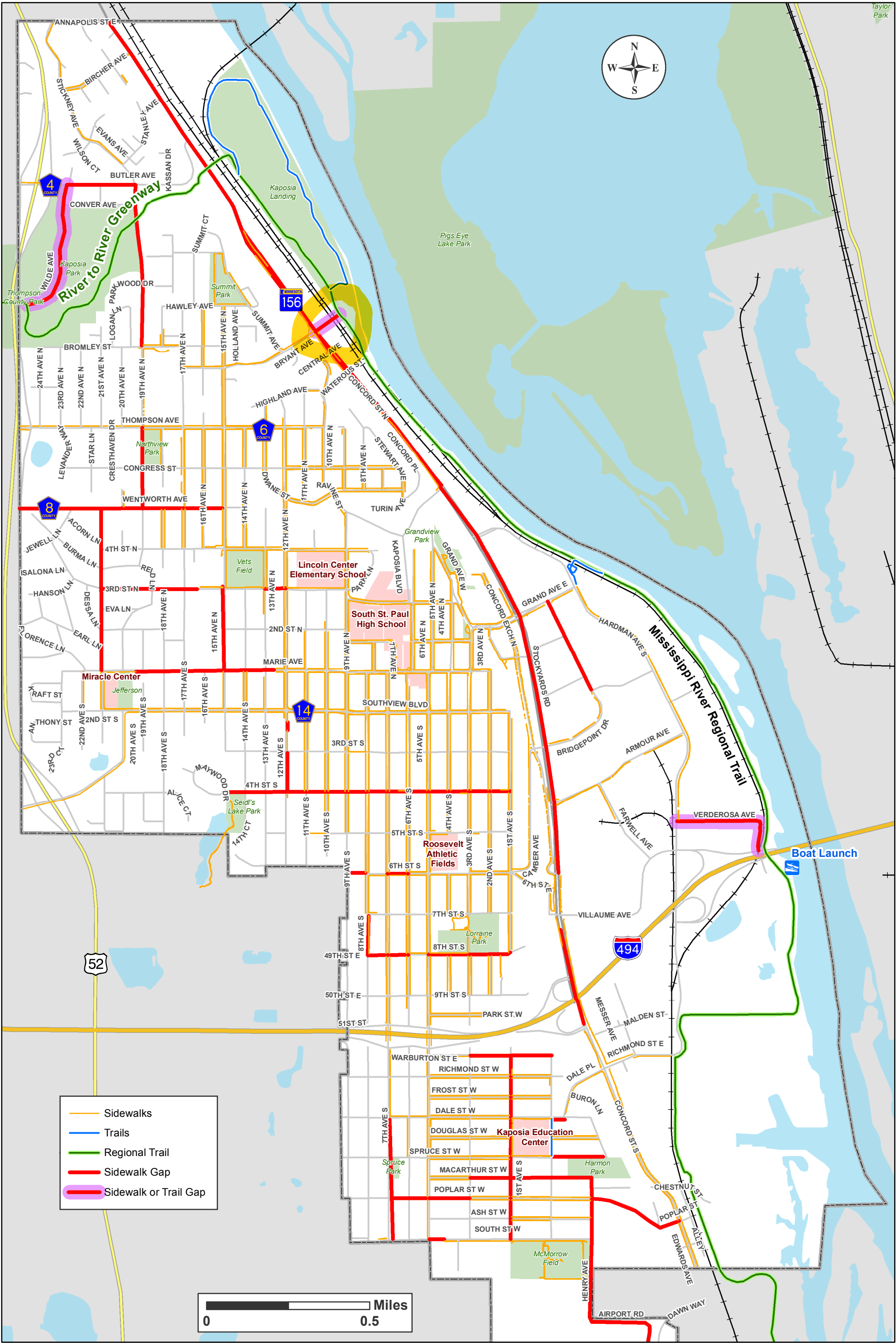


Good walking and bicycling routes:

- Mississippi River Regional Trail
- River to River Greenway
- 5th Avenue S between I-494 and Spruce Street E
- Spruce Street E between 1st and 5th Avenues S
- Southview Boulevard between 5th and 15th Avenues S
- 8th Avenue S between 4th Street S and Marie Avenue
- 6th Avenue S between Southview Boulevard and Marie Avenue
- 2nd Street N between 6th and 9th Avenues N
- 3rd Street N between 9th and 12th Avenues N and 13th and 15th Avenues N
- 4th Street N between 9th and 15th Avenues N
- 12th Avenue N between 3rd Street N and Congress Street
- Thompson Avenue between 10th and 24th Avenues N is good for walking
- 15th Avenue N between 3rd Street N and Bryant Ave
- Bryant Ave between 17th Avenue N and Concord Street N

Challenging walking and bicycling routes:

- Poplar Street E between Henry Ave and Concord Street S
- Crossing 5th Avenue S near I-494 can be challenging due to heavy traffic
- Villaume Avenue between Concord Street and Farwell Avenue
- 5th Street S between 5th and 10th Avenues S
- 9th Avenue N between 5th Street S and Southview Boulevard
- 4th Street S between 10th and 14th Avenues S
- Southview Boulevard between 3rd Avenue S and 18th Avenue S
- Marie Avenue between 13th and 23rd Avenues N
- Streets within the Tangletown neighborhood as they do not have sidewalks
- 15th Avenue S between Marie Avenue and 3rd Street N
- Wentworth Avenue between 14th Avenue and western city limits
- 16th Avenue N between 4th Street N and Thompson Avenue
- Thompson Avenue between 15th and 24th Avenue is challenging for bicyclists
- 19th Avenue N between Bromley Street and Butler Avenue
- **Bryant Avenue between Concord Street N and the Mississippi River Regional Trail**
- Concord Street N between Bryant Ave N and the northern city limits
- Butler Avenue between 19th Avenue and Concord Street N.
- Temporary gravel segments along the Mississippi River Regional Trail are challenging for bicycling.



South St. Paul Bicycle & Pedestrian Plan
Figure 10: Sidewalk Gaps



| Sidewalk Gap | Description | Length | On Arterial Sidewalk Network? |
|---|--|--------------------------|--------------------------------------|
| Bridge Point Drive from Grand Avenue to Bridge Point Drive at Bridge Point way | Sidewalk gap | 1,591 | No |
| Bryant Avenue from Concord Street N to MRRT | Sidewalk or trail gap | 475 | Yes |
| Butler Avenue (CSAH 4) from eastern US 52 entrance ramp to 19th Avenue | Sidewalk gap | 1,091 | Yes |
| Concord Street from St. Paul to eastbound I-494 entrance ramps | Existing sidewalk in some areas of Concord Street. The city's long-term goal is to construct continuous sidewalk on the west side of the street and continuous multi-use trail on the east side of the street. | 22,770 feet (4.31 miles) | Yes |
| Dale Street W from Syndicate Avenue to Dale Place | Sidewalk gap | 172 | Yes |
| Henry Avenue from MacArthur Street E to Airport Road | Sidewalk gap | 2,335 | No |
| MacArthur Street from 3rd Avenue S to Henry Avenue | Sidewalk gap | 1,967 | No |
| Marie Avenue from 19th to 12th Avenues N | Sidewalk gap | 2,227 | Yes |
| Poplar Street from 7 th Avenue S to 3 rd Avenue S, Henry Avenue to Concord Street S | Sidewalk gap | 2,802 | No |

Proposed on-street bicycle facilities

South St. Paul has identified several streets for future on-street bicycle facilities. It is anticipated that most of these facilities will be traditional bike lanes or shoulders; however, the city may consider buffered bike lanes, cycle tracks, or advisory bike lanes in certain situations. On-street bike lanes or shoulders identified for South Saint Paul are shown on **Figure 11** and listed in **Table 4**.

Table 4: Proposed Bike Lanes or Shoulders

| Proposed Bike Lane/Shoulder Location | Length in feet |
|--|-------------------------|
| 3rd Avenue N from Marie to Grand Avenues | 657 |
| 4th Avenue N from Grand Ave W to Marie Avenue | 1,475 |
| 5th Avenue S from 9th Street S to Warburton Street W | 963 |
| 15th Avenue N from Bryant to Thompson Avenues | 904 |
| Bryant Avenue from 15th Avenue N to Kaposia Landing Park/MRRT | 2,161 |
| Butler Avenue (CSAH 4) from western city limits to 19th Avenue N | 1,839 |
| Dale Place/Richmond Street E from Dale Street W to MRRT | 2,197 |
| Grand Avenue from 3rd to Hardman Avenues S | 1,865 |
| Hardman Avenue S from MRRT to Verderosa Avenue | 5,275 feet (1 mile) |
| Marie Avenue from 21st to 3rd Avenues N | 6,024 feet (1.14 miles) |
| Southview Boulevard from turn at 20th Avenue to western city limits | 2,632 |
| Stickney Avenue/19th Avenue N from northern city limits to Wentworth Avenue (CSAH 8) | 8,515 feet (1.61 miles) |
| Thompson Avenue from western city limits to 10th Avenue | 4,878 |
| Wentworth Avenue (CSAH 8) from western city limits to 15th Avenue | 3,294 |
| Wilde Avenue from Butler Avenue (CSAH 4) to River to River Greenway | 2,528 |
| Verderosa Avenue from Hardman Avenue to boat launch | 1,887 |

| Sidewalk Gap | Description | Length | Cost |
|--|--|--------------------------|-------------|
| 19th Avenue from Butler Avenue (CSAH 6) to Bromley Street, Thompson (CSAH 6) to Wentworth (CSAH 8) Avenues | Sidewalk gap | 3,906 | \$292,950 |
| 21st Avenue N from Wentworth (CSAH 8) to Marie Avenues | Sidewalk gap | 2,621 | \$196,575 |
| Airport Road from Henry Avenue to city boundary with Inver Grove Heights | Sidewalk gap | 1,433 | \$107,475 |
| Bridge Point Drive from Grand Avenue to Bridge Point Drive at Bridge Point way | Sidewalk gap | 1,591 | \$119,325 |
| Bryant Avenue from Concord Street N to MRRT | Sidewalk or trail gap | 475 | \$35,625 |
| Butler Avenue (CSAH 4) from eastern US 52 entrance ramp to 19th Avenue | Sidewalk gap | 1,091 | \$81,825 |
| Concord Street from St. Paul to eastbound I-494 entrance ramps | Existing sidewalk in some areas of Concord Street. The city's long-term goal is to construct continuous sidewalk on the west side of the street and continuous multi-use trail on the east side of the street. | 22,770 feet (4.31 miles) | \$1,707,750 |
| Dale Street W from Syndicate Avenue to Dale Place | Sidewalk gap | 172 | \$12,900 |

Table 11: Planning-level Cost Estimates for Proposed Bike Lanes/Shoulders

| Proposed Bike Lane/Shoulder Location | Length in feet | Estimated cost |
|--|-----------------------|-----------------------|
| 3rd Avenue N from Marie to Grand Avenues | 657 | \$2,496 |
| 4th Avenue N from Grand Avenue W to Marie Avenue | 1,475 | \$5,605 |
| 5th Avenue S from 9th Street S to Warburton Street W | 963 | \$3,660 |
| 15th Avenue N from Bryant to Thompson Avenues | 904 | \$3,436 |
| Bryant Avenue from 15th Avenue to Kaposia Landing Park/MRRT | 2,161 | \$8,212 |
| Butler Avenue (CSAH 4) from western city limits to 19th Avenue | 1,839 | \$6,988 |
| Dale Place/Richmond Street E from Dale Street to MRRT | 2,197 | \$8,349 |
| Grand Avenue from 3rd Avenue N to Hardman Avenues S | 1,865 | \$7,088 |
| Hardman Avenue S from MRRT to Verderosa Avenue | 5,275 | \$20,045 |
| Marie Avenue from 21st to 3rd Avenues | 6,024 | \$22,891 |
| Southview Boulevard from turn at 20th Avenue to western city limits | 2,632 | \$10,002 |
| Stickney Avenue/19th Avenue N from northern city limits to Wentworth Avenue (CSAH 8) | 8,515 | \$32,358 |
| Thompson Avenue from western city limits to 10th Avenue | 4,878 | \$18,535 |
| Wentworth Avenue (CSAH 8) from western city limits to 15th Avenue | 3,294 | \$12,519 |
| Wilde Avenue from Butler Avenue (CSAH 4) to River to River Greenway | 2,528 | \$9,606 |
| Verderosa Avenue from Hardman Avenue to boat launch | 1,887 | \$7,170 |

DAKOTA COUNTY PEDESTRIAN AND BICYCLE STUDY

DECEMBER, 2018

EXECUTIVE SUMMARY

PURPOSE OF THE STUDY

In 2017, Dakota County began a study process to create a unified vision for countywide walking and bicycling networks and identify policies, strategies and tools to encourage active living and improve community health.

The primary purposes of this study are to address non-motorized transportation content required for the Dakota County 2040 Comprehensive Plan and recommend bicycling and walking content to be considered in the Dakota County 2040 Transportation Plan update (2019).

This study focuses on infrastructure priorities and supporting strategies and policies for integration of walking and bicycling modes into the Dakota County transportation network. In addition, it highlights awareness, education, enforcement, and evaluation efforts that bolster infrastructure investments.

BENEFITS OF INVESTING IN ACTIVE TRANSPORTATION

We all need to travel to meet every day needs such as getting to work and school, accessing affordable healthy food, and accessing health care. In Dakota County, the roadway network provides safe and convenient transportation for those with personal vehicles. However, one-third of the population does not drive/own a car, and walking, biking and transit infrastructure is less complete than the road network. People who cannot afford a car, people with disabilities, and people who choose not to or are unable to drive face transportation hurdles that make meeting basic needs time consuming, inconvenient, stressful, and sometimes dangerous.

Active transportation is part of the solution to chronic health conditions that many residents face. Chronic conditions are four of the top five leading causes of death in Dakota County. Regular physical activity can decrease risk for major chronic diseases such as heart disease, type 2 diabetes, stroke, and certain types of cancer, as well as mental health problems. Active transportation is a simple way to integrate regular physical activity into daily routines. Communities that invest in physical infrastructure and programs to promote active transportation tend to have more physically active and healthier populations. In many Dakota County communities, incomplete or non-existent trail and sidewalk connections, infrequent transit service, and long distances between destinations are barriers to active transportation.

STUDY PROCESS AND PUBLIC INVOLVEMENT

Development of the Study was continuously guided by a project management team of Dakota County staff and partner agency staff and informed by two phases of public engagement events. As shown in Figure 2, the study process began with analysis of existing conditions and walk/bike system recommendations, followed by research into policies, strategies, and best practices to support walking and biking in the County, and ended with preparation of the Study.

Figure 1-2: Project Schedule



Community engagement, including public events, presentations to the Planning Commission and County Board, and meeting with the Project Management Team, continued simultaneously with research and analysis tasks.

Planning Commission

Updates were presented to the Dakota County Planning Commission at milestones during the process. Issues raised by Planning Commission include:

- Concern about safety of on-road bike lanes and ability for cyclists and drivers to co-exist without physical separation, particularly on roads with higher speed limits
- Importance of coordinating with local and state systems
- Education for drivers and cyclists about safe behavior and following the rules of the road
- Need for enforcement of traffic laws

Project Management Team

The project management team was made up County Staff from the transportation, planning, and public health departments and representatives from MnDOT, the Metropolitan Council and the Cities of Inver Grove Heights, Apple Valley, Burnsville, Rosemount, and South St. Paul.

The project management team raised key issues for consideration in the study development process. These issues are addressed in Chapter 2 of the study and include:

- System continuity
- The relationship between facility types, safety, and levels of traffic stress

-

Figure 1-3: Open House on a Dakota County Trail

Community Engagement

The first phase of community engagement occurred from July through October 2017. The purpose was to engage a representative cross section of Dakota County residents, businesses, and the general public to collect meaningful input, build consensus, and generate excitement for walking and biking in Dakota County. The second phase, to allow opportunity to comment on the Draft Study occurred from June to August 2018.

In total, there were twenty engagement activities with more than 850 people reached. Events were conducted using various formats including an open house, pop-up style events, listening sessions, intercept flyers, an online survey, and ability to comment on the Draft Study on-line.

In addition, the project team directly connected with over 30 community organizations and business (e.g., neighborhood groups, bike shops, major employers, etc.).

Events were planned to leverage existing groups, networks, and high traffic areas. The open house and pop-up events were structured to provide information and collect input in an informal setting, whereas the listening sessions and walking groups were structured to allow an opportunity for

participants to provide information on existing conditions, gaps and barriers, and preferred facility types for walking and biking, in the format of their existing meeting structure. Engagement events are listed in Table 1: Engagement Events on the following page, and highlights of public input are listed in Table 2: Community Engagement General Comments

Table 1-1: Phase 1 Engagement Events

| | Name | Date | Approx. No. of Participants | Target Populations |
|----|---|-------------------------------|-----------------------------|--|
| 1 | Living Longer and Stronger, West St. Paul | Thurs, July 20 2017 | 12 | Older Adults |
| 2 | 50+ Adult Walking Group, West St. Paul | Wed, Aug 2 2017 | 13 | Older Adults |
| 3 | Prince of Peace, Burnsville | Tues, Aug 8 2017 | 15 | Lower income, families, general public |
| 4 | Dakota County Fair, Farmington | Tues, Aug 8– Wed, Aug 9 2017 | 25 | General public |
| 5 | Big Rivers Trail Open House, Mendota Heights | Thurs, Aug 10 2017 | 35 | General public, trail users |
| 6 | Ecua-Volley at Redwood Park, Apple Valley | Tues, Aug 15 2017 | 20 | Latino populations, families, children |
| 7 | Lake Marion Greenway Open House, Burnsville | Wed, Aug 23 2017 | 15 | General public |
| 8 | Burnsville Mosque, Burnsville | Fri, Aug 25 2017 | 60 | Somali populations |
| 9 | Intercept Flyers, Various Locations | Thurs, Sept 7 2017 | 35 | Walkers and bikers |
| 10 | Pedal the Parks and Lakeville Art Festival, Lakeville | Sat, Sept 16 2017 | 30 | General public, bikers |
| 11 | ALMAS Student Group, Henry Sibley High School | Tues, Oct 10 2017 | 27 | Students |
| 12 | Phase 1 Online Survey Map | Mon, Jul 10– Mon, Oct 2, 2017 | 192 | General public |
| 13 | Phase 1 Online Survey | Mon, Jul 10– Mon, Oct 2 2017 | 151 | General public |
| 14 | Thompson Reuters Survey | Tues, Nov 14– Mon, Dec 18 | 55 | Thompson Reuters Staff |
| 15 | Kaposia Days | Sun, June 24 | 25 | General public |

| | | | | |
|------|---------------------------------|---------------|-----|------------------|
| 2018 | | | | |
| 16 | Lakeville Farmer's Market | Wed, June 27 | 30 | General public |
| 2018 | | | | |
| 17 | Dakota County Fair | Thurs, Aug 9 | 50 | General public |
| 2018 | | | | |
| 18 | Draft Plan – On Line Review | June – August | 108 | General public |
| 2018 | | | | |
| 19 | Community organization contacts | Ongoing | 30+ | Community groups |
| 2018 | | | | |
| 20 | Individual Comments | Ongoing | 5 | General public |

Table 1-2: Community Engagement General Comments

| Highlight | Recommendation |
|--|--|
| Many people requested information regarding walking and biking routes in the form of online, paper, and trailhead maps. | Create a central place for finding recommended routes online and widely distribute paper route maps. Update trailhead maps. |
| People, especially older adults, were sensitive to cracks in the pavement, litter on the ground, and snow and ice. | Produce consistent maintenance standards for sidewalks and trails throughout the county. |
| Most people were concerned with the safety, comfort, and health of walkers and bikers. Some were apprehensive about walking by themselves or walking at night without visible gear. Others were interested in the health benefits of walking and biking. | Administer educational programming to focus on sidewalk and road rules for all transportation users throughout the county. Continue to support programs that promote healthy lifestyles. |

Figure 1-4: Pedestrian and Bicycle Demand Analysis

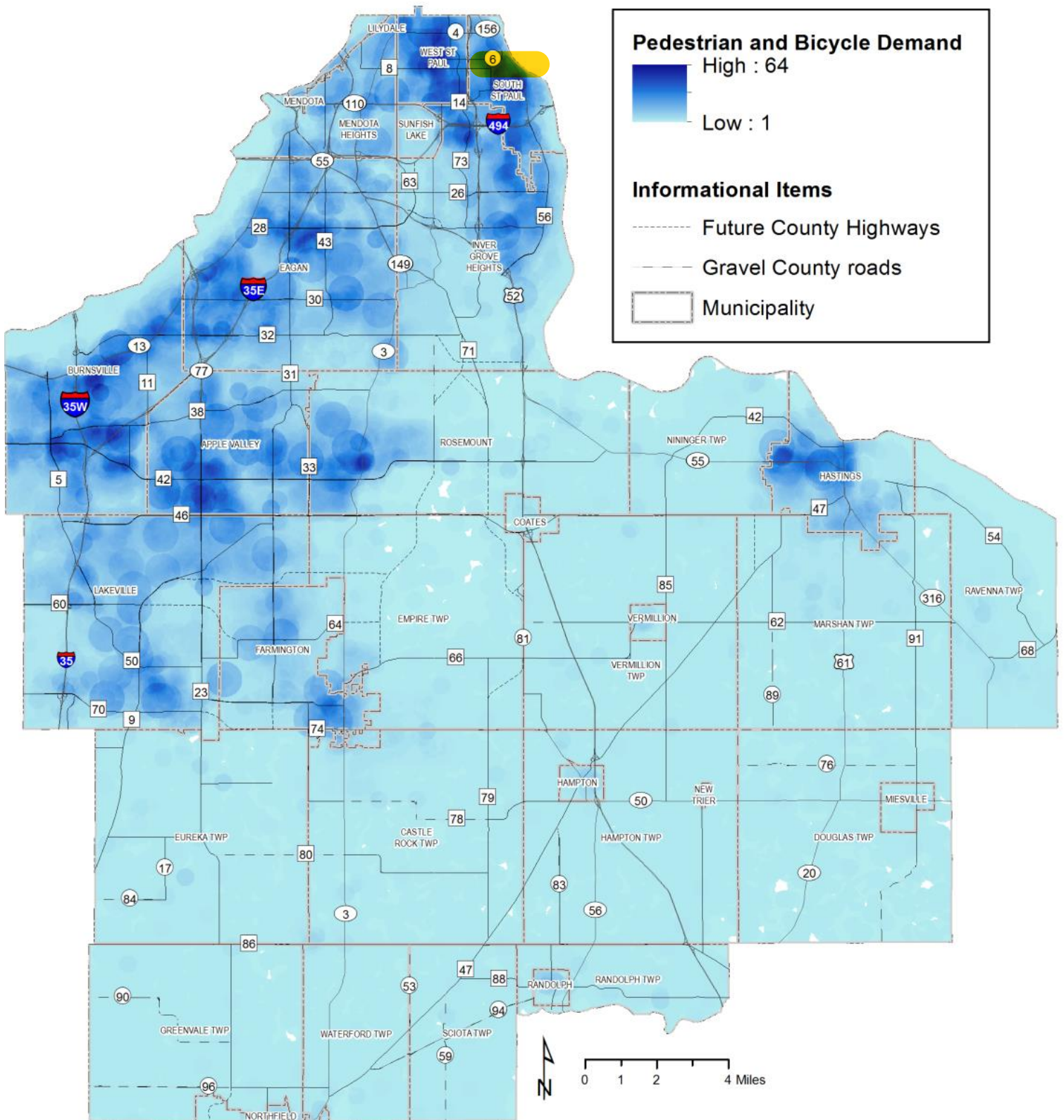
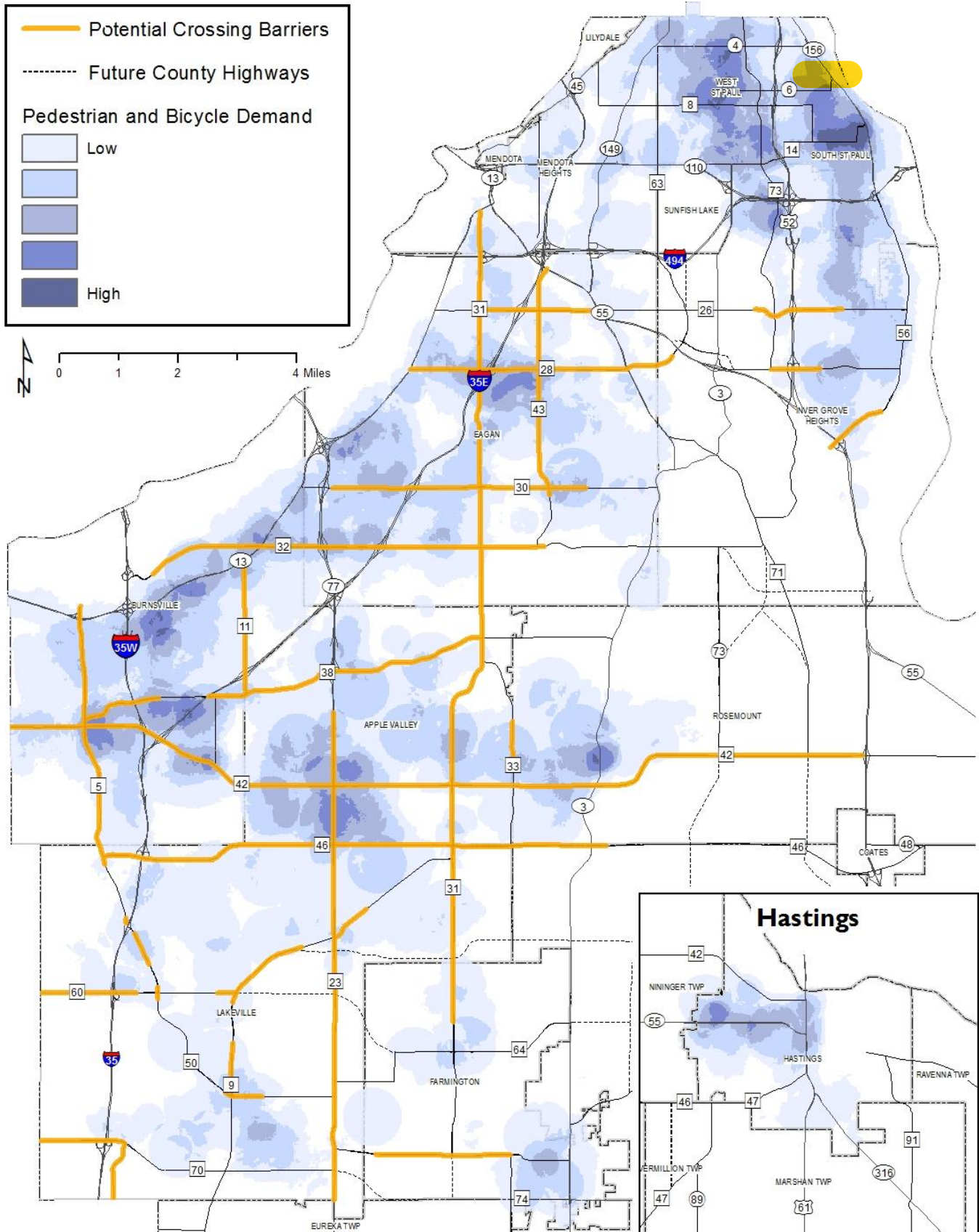
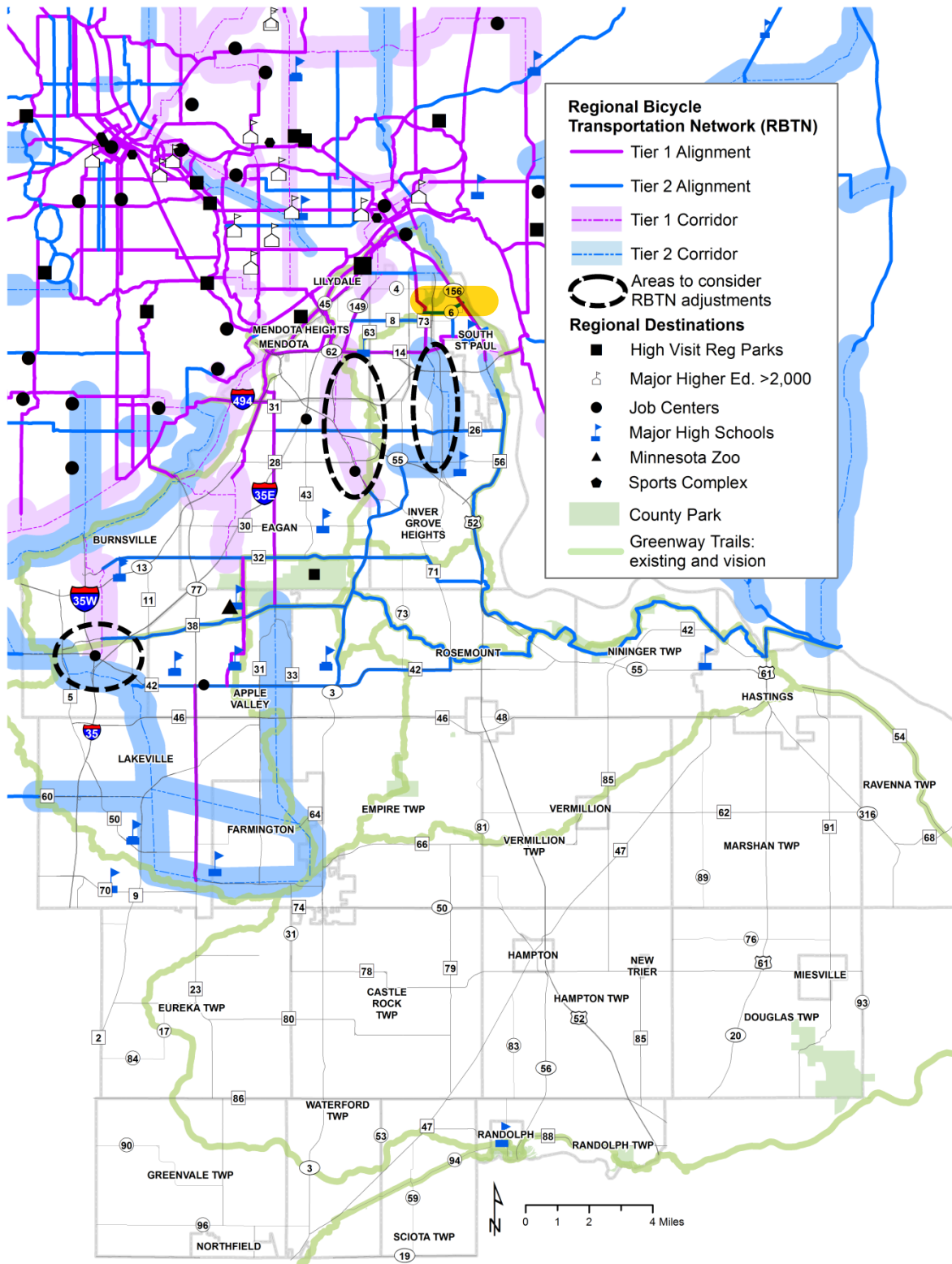


Figure 2-2: County Highway Crossing Barriers



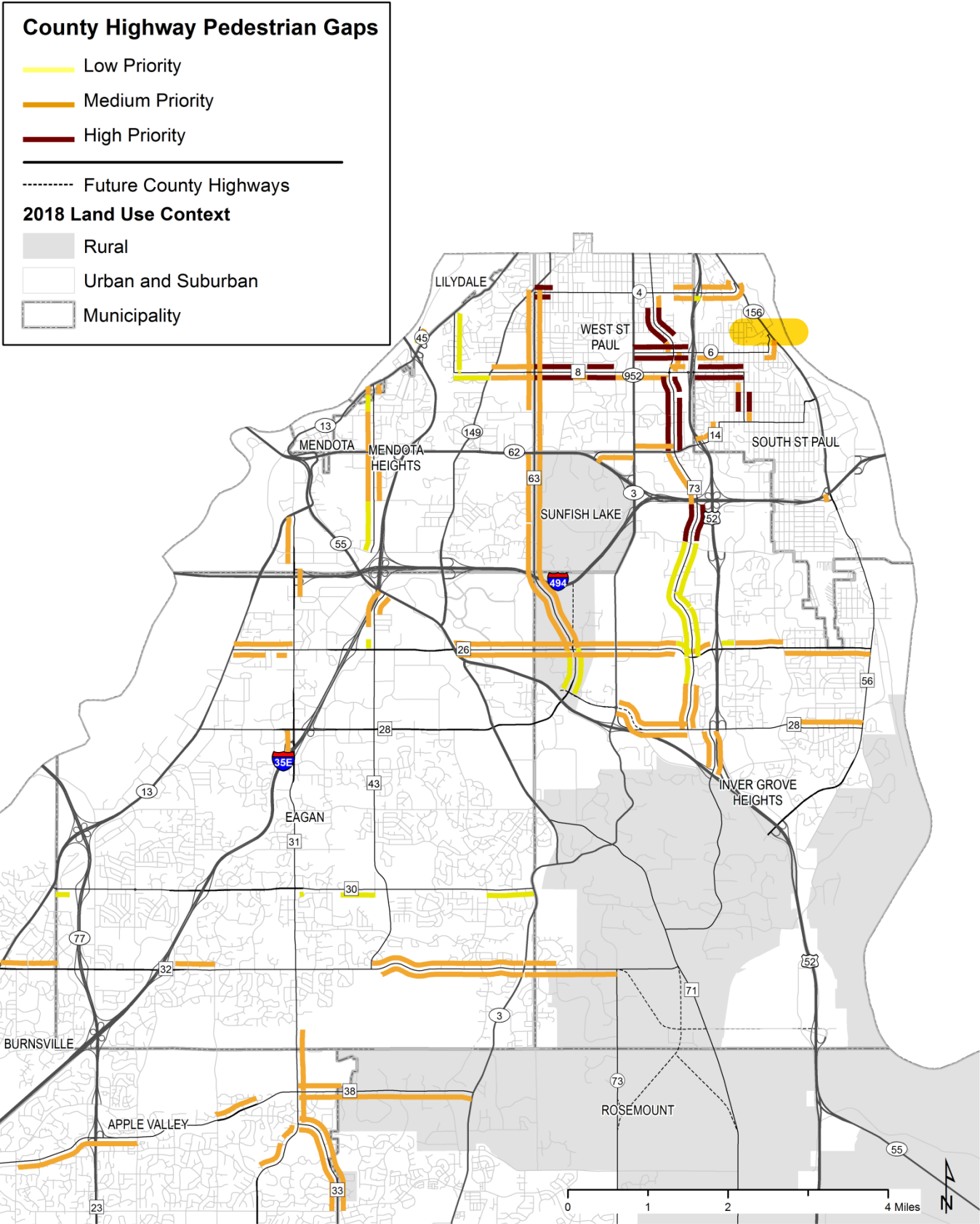
Mendota Heights; State Highway 3 throughout the county, and State Highway 149, Inver Grove Heights. Shared use trail gaps on the State system in Dakota County are identified in Figure 2-5.

Figure 2-4: Metropolitan Council Regional Transportation Network (RBTN)



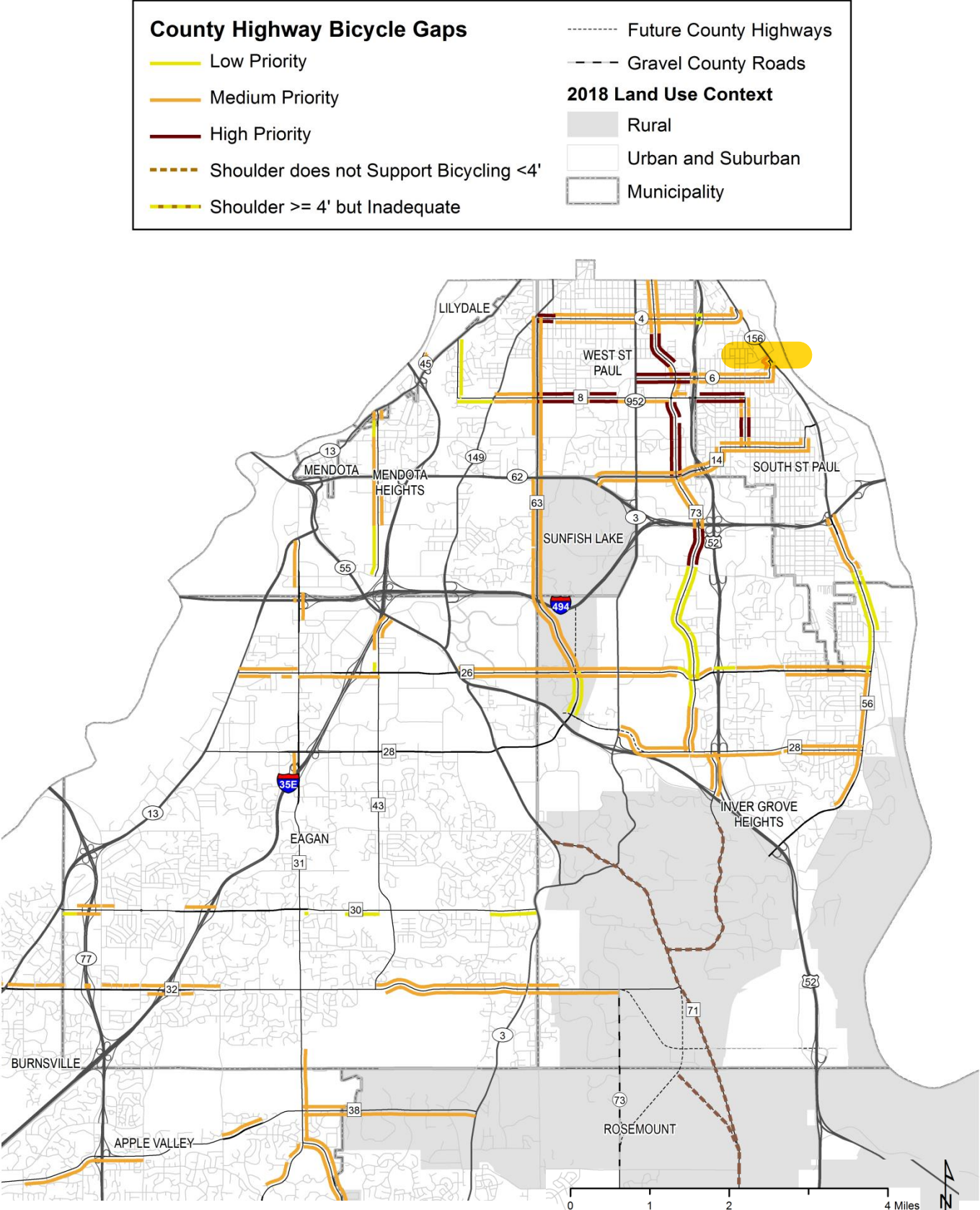
Pedestrian Gap Maps

Figure C1 Map A Pedestrian Gaps: Mendota, Mendota Heights, Lilydale, West St. Paul, South St. Paul, Sunfish Lake, Eagan, Inver Grove Heights



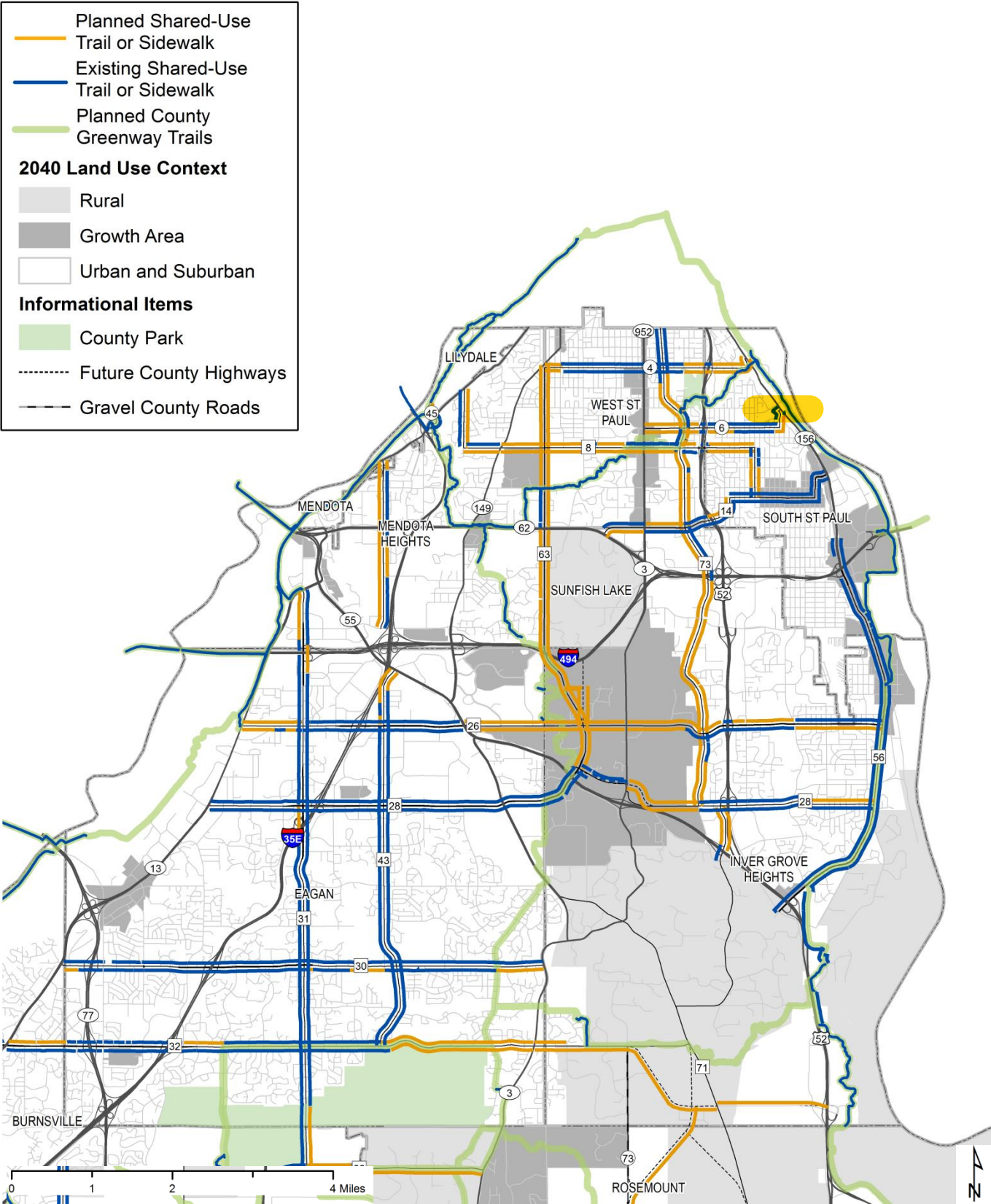
Bicycle Gap Maps

Figure C7 Map A Bicycle Gaps: Mendota, Mendota Heights, Lilydale, West St. Paul, South St. Paul, Sunfish Lake, Eagan, Inver Grove Heights



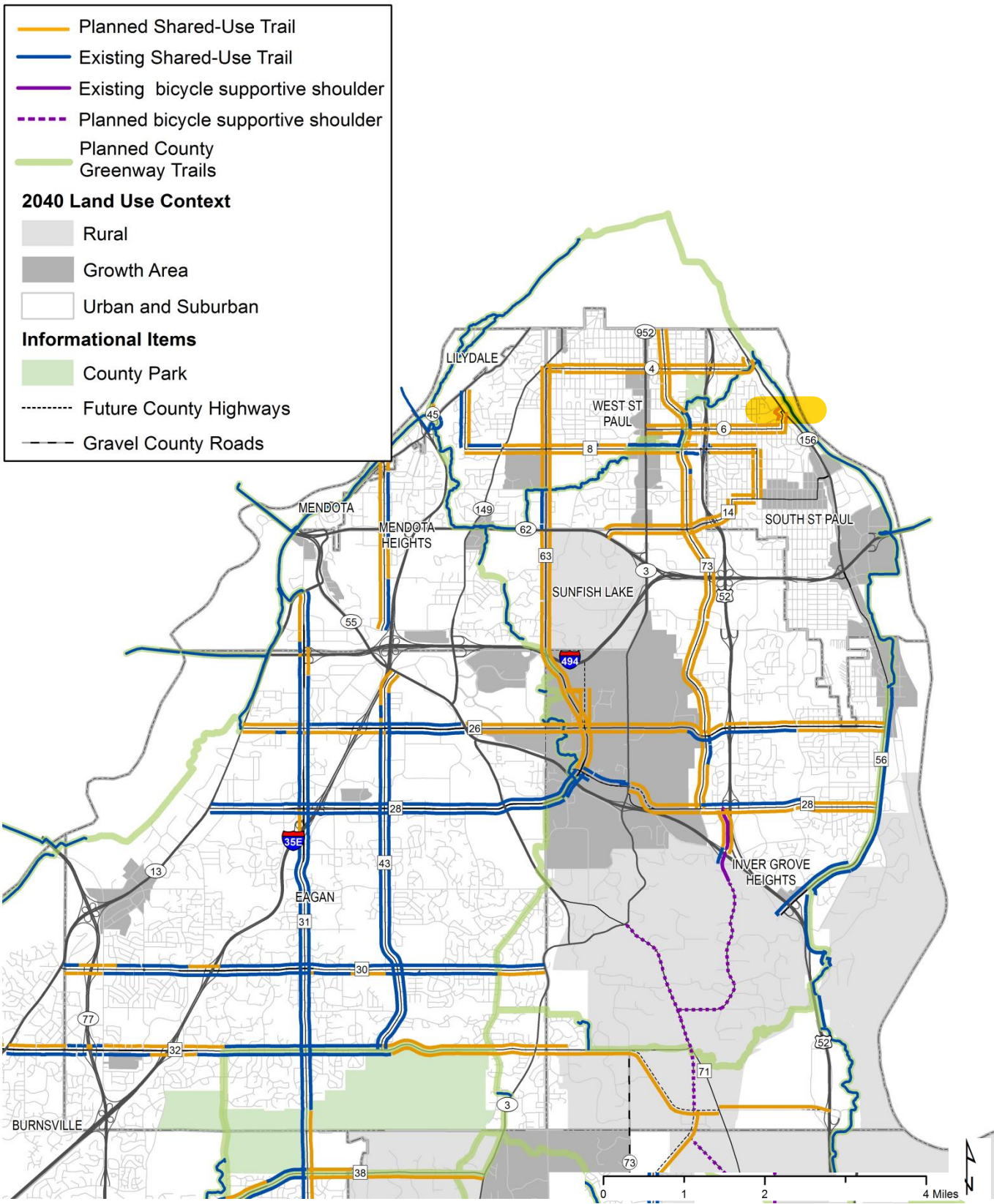
Planned Pedestrian Network Maps

Figure C13 Map A Planned Pedestrian Network: Mendota, Mendota Heights, Lilydale, West St. Paul, South St. Paul, Sunfish Lake, Eagan, Inver Grove Heights



Planned Bicycle Network Maps

Figure C19 Map A Planned Bicycle Network: Mendota, Mendota Heights, Lilydale, West St. Paul, South St. Paul, Sunfish Lake, Eagan, Inver Grove Heights



Mississippi River Greenway

Once completed, Mississippi River Greenway will extend 27 miles along the Mississippi River from St. Paul to Hastings, providing scenic views of the river and connections to local parks and trails and other community destinations. The trail will be paved and is intended for non-motorized uses such as walking, bicycling and in-line skating.

Admission and parking are free.

Trail features

- 26+ miles of paved trails
- Scenic overlooks
- Trail connections to
 - River to River Greenway
 - Big Rivers Regional Trail
 - Robert Pira Regional Trail
- Restrooms at Kaposia Landing Park, Simon’s Ravine Trailhead, Swing Bridge Trailhead, Pine Bend Bluffs Trailhead and Spring Lake Park Reserve.

So everyone can enjoy the park...

- Use trails for their intended purpose.
- Leashed pets welcome on non-ski trails. Please put pet waste in the trash.

Mississippi River Greenway

Trail Map

Trailheads:
South St. Paul
Inver Grove Heights
Rosemount
Hastings



Dakota County Parks
Emergency / Park Ranger: 911
Park Office: 952-891-7000
Lebanon Hills Visitor Center: 651-554-6530
Email: parks@co.dakota.mn.us
www.dakotacounty.us/parks

Please recycle.

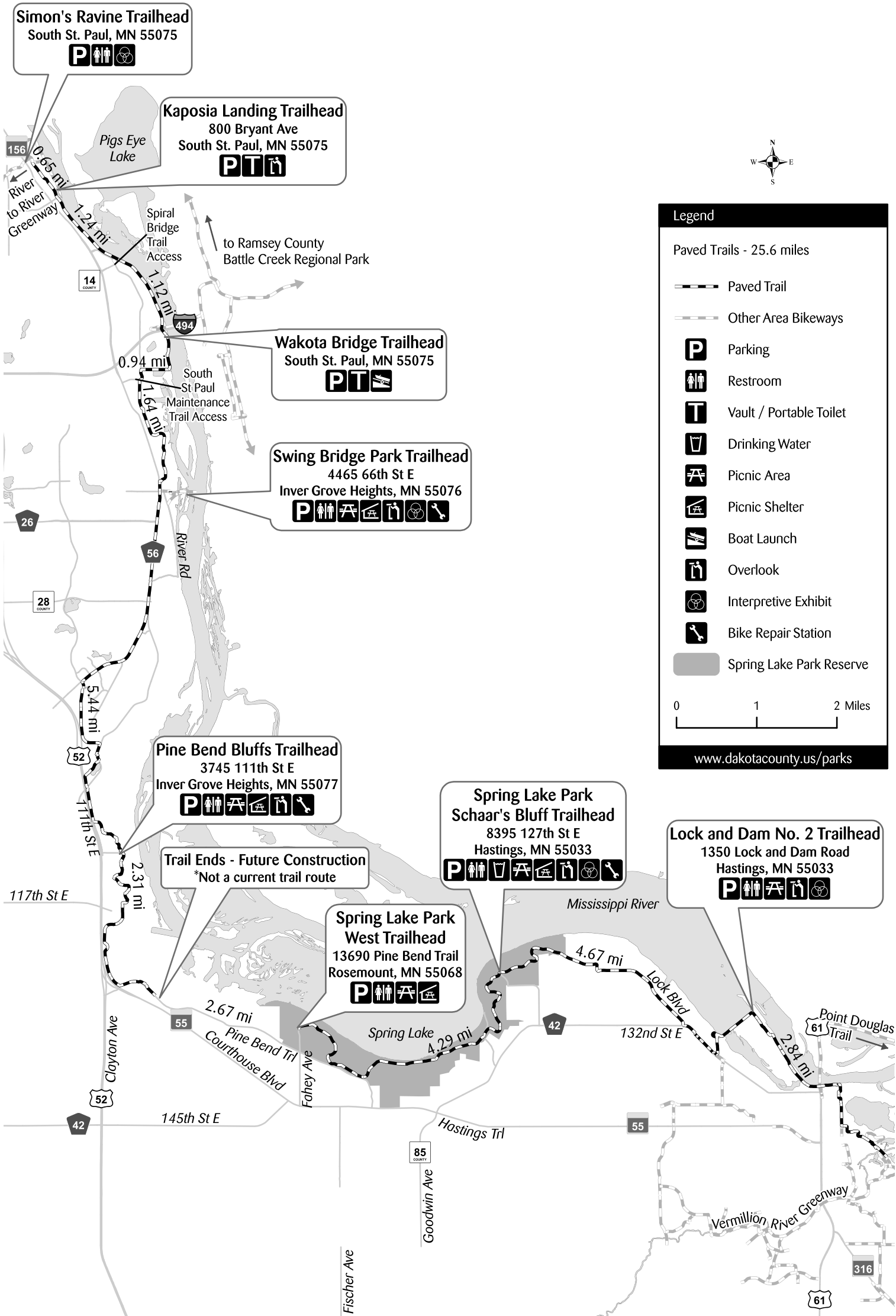


Mississippi River Greenway

Trail hours: 5 a.m. to 10 p.m.

Trail conditions: www.dakotacounty.us/parks, search *trail conditions*.

Track your location in the park using the free Avenza PDF Maps app.





2040 South St. Paul Comprehensive Plan

January 2022



Goal 4.27: Establish a community mixed-use destination area in the vicinity of Concord Street and Bryant Avenue, which capitalizes on the available property for development on Bryant Avenue, access to Kaposia Landing park, and proximity to the Mississippi River.

Policy 4.27.1: Prepare a small area plan for the North Concord Mixed Use area that can help to target specific development types that would be consistent with the highest and best use for that area.

Policy 4.27.2: Explore potential uses in the North Concord Mixed Use area which can be provide complementary services to Kaposia Landing park and the residential properties up Bryant Avenue.

Policy 4.27.3: Explore the potential of this area for transit oriented development which can take advantage of the proximity to Saint Paul and the existing transit routes.

Goal 4.28: Create a clear long-term vision for the planned mixed-use area for the South Concord Corridor area along Concord Street from Interstate 494 to the City's southern border.

Policy 4.28.1: Establish a new small area plan for the South Concord Corridor and develop zoning regulations to ensure that the area is developed consistent with that plan.

Policy 4.28.2: Explore a mixture of residential, office, retail, and service uses along South Concord Street and encourage uses that can provide improved bicycle and pedestrian connections to recreational amenities along the river, takes advantage of the proximity of the area to the Interstate system, can act as a buffer from industrially guided properties east of Concord Street and exhibits a high level of architectural and site design.

Policy 4.28.3: Build off of the information from previous South Concord planning efforts (2009) which identified challenges for the development of this area, including: properties on the west side of Concord Street are very shallow due to the bluffs which limits available space for development, access to some properties to the east can be interrupted for up to 20 minutes at a time due to the location of property road access and location of the rail line, most of the properties south of 494 and east of Concord Street are not protected by the levee and some may be subject to occasional flooding, adjacent heavier industrial uses can limit the appeal of properties in the area.

- Policy 5.3.3: Maintain a balanced and competitive inventory of business and industrial sites with excellent infrastructure, telecommunications capacity and transportation access. Continue to improve the image of this area by setting higher architectural and landscaping standards for the development of new buildings and the redevelopment of existing buildings. Prepare a plan and design guidelines for development along I-494 with increased architectural design, landscaping, and use standards to provide a positive image of the city as seen from I-494.
- Policy 5.3.4: Prioritize opportunities to bring more amenities to Kaposia Landing and the development node at Concord Street and Bryant Avenue, which leads right into Kaposia Landing. Study and plan for development/redevelopment of these areas.
- Policy 5.3.5: Work together with private developers and granting agencies to enhance the attractiveness and draw of the riverfront area.
- Policy 5.3.6: Encourage creative concepts that will enhance the Concord Exchange, Hardman Triangle, and South Concord Corridor (the area along Concord from 494 to the City's southern border). Explore new design standards for Concord Exchange, Hardman Triangle and the South Concord Corridor. Study and plan for development/redevelopment of these areas.
- Policy 5.3.7: Fleming Field Airport is and will continue to be an important community focal point and important piece in the City's economic development efforts. The City will continue to support economic development and redevelopment at the airport.
- Policy 5.3.8: As an established community with a well-established history, the existing buildings help to reinforce the qualities and character that makes South St. Paul a unique and appealing community for existing residents as well as new development. Reutilizing existing buildings is also far more sustainable, utilizing far less waste than demolition and construction of new structures. Encourage adaptive reuse of existing buildings and use demolition only as a "last resort" when properties are determined to be beyond reclamation.

District 2

The District 2 area is bordered on the west by the Union Pacific Railroad switching yard and tracks, on the north by the City's northern border, on the east by the Mississippi River and the City's eastern border, and on the south by Wentworth Avenue.



Land Use Issues

District 2 is another section of the City that is largely separated from the rest of the community. There are only 2 access points from the remainder of the community into this area including a vehicle bridge at Bryant Avenue and one pedestrian bridge at Simon's Ravine, just south of Butler Avenue. Access to the northernmost part of this district is currently only possible by crossing from a property in neighboring Saint Paul. The majority of this district is the 87-acre Kaposia Landing park (formerly referred to as Port Crosby) which was a former construction landfill area that was cleaned and capped for redevelopment as a park. The development of the Kaposia Land site has been a long time coming with construction of the bridge to the property in 2002 and cleanup of the park site beginning in 2006. However, development of the park space began in earnest with the 2010 voter approved parks levy which provided the first phase of funding beginning on the north side of the park with 4 lighted softball fields, a lighted baseball field, concessions/restrooms and parking. A sizable dog park also runs along the western side of this park, abutting the Union Pacific Railway property. Future phases of park development call for a play structure, picnic lawn, a pavilion, bocce/lawn game areas and other recreational amenities.

Residential on the City's Zoning Map. This area also has occasional duplexes and small apartment buildings that can still be found mixed into the neighborhoods.

Planning District 4 has three areas where medium density residential or high density residential is designated. North of the Divine Redeemer site, mentioned in Planning District 3, there is an area of single-family attached housing that is shown as medium density residential. Northeast of the intersection of 15th Avenue and Thompson Avenue are the Thompson Heights senior apartments which are the newest of the three senior apartment buildings in South St. Paul that were built and managed by the Dakota County Community Development Agency. Finally, east of 15th Avenue North and centered on Bryant Avenue there is one of the City's largest areas designated for high density residential. There are currently six apartment buildings built in this area on the northern side of Bryant Avenue. The south side of Bryant Avenue currently has a couple of single-family homes but has previously been approved for a mixture of single-family homes and condominiums.

At the eastern edge of Planning District 4 is the North Concord area centered on Bryant Avenue and Concord Street. This has also been known as "South Park" which is one of the first areas of development in the community. The area currently has a mixture of commercial, office, residential, and some industrial uses but the parcels are all quite shallow and development typically extends right up to the toe of the bluff. Heading south from this area along Concord Street there are some business sites located on narrow parcels between the road and adjacent railroad. Across from the Thompson Heights development there is a small stretch of neighborhood oriented commercial property.

Future Land Use

Most of the property in District 4 is fully developed and substantial land use changes are not anticipated. Given the development of the new park at Kaposia Landing, with the only access via bridge at Bryant Avenue, and the connection up Bryant Avenue to the large mixed-use parcel and connection provided to the surrounding community, the node at Bryant and Concord could be one of the key areas of development over the next 20-30 years. Similar to the issue noted in District 1, the topography in District 4 presents a challenge for development and redevelopment. In particular, the topography makes for shallow parcels for development, which require more frontage to have adequate development space, and poses challenges for accommodating parking on the sites. The City should study this development node to provide more clarity on future land uses and provide direction on specific development or zoning standards. The City should also work to preserve historic buildings in the area by encouraging adaptive reuse of the structures and using demolition as a last resort when buildings are determined to be beyond reclamation.

Goal 4.27: Establish a community mixed-use destination area in the vicinity of Concord Street and Bryant Avenue which capitalizes on the available property for development on Bryant Avenue, access to Kaposia Landing park, and proximity to the Mississippi River.

Policy 4.27.1: Prepare a small area plan for the North Concord Mixed Use area that can help to target specific development types that would be consistent with the highest and best use for that area.

Policy 4.27.2: Explore potential uses in the North Concord Mixed Use area which can be provide complementary services to Kaposia Landing park and the residential properties up Bryant Avenue.

Policy 4.27.3: Explore the potential of this area for transit oriented development which can take advantage of the proximity to Saint Paul and the existing transit routes.

Goal 4.28: Create a clear long-term vision for the planned mixed-use area for the South Concord Corridor area along Concord Street from Interstate 494 to the City's southern border.

Policy 4.28.1: Establish a new small area plan for the South Concord Corridor and develop zoning regulations to ensure that the area is developed consistent with that plan.

Policy 4.28.2: Explore a mixture of residential, office, retail, and service uses along South Concord Street and encourage uses that can provide improved bicycle and pedestrian connections to recreational amenities along the river, takes advantage of the proximity of the area to the Interstate system, can act as a buffer from industrially guided properties east of Concord Street and exhibits a high level of architectural and site design.

Policy 4.28.3: Build off of the information from previous South Concord planning efforts (2009) which identified challenges for the development of this area, including: properties on the west side of Concord Street are very shallow due to the bluffs which limits available space for development, access to some properties to the east can be interrupted for up to 20 minutes at a time due to the location of property road access and location of the rail line, most of the properties south of 494 and east of Concord Street are not protected by the levee and some may be subject to occasional flooding, adjacent heavier industrial uses can limit the appeal of properties in the area.

- Policy 5.3.3: Maintain a balanced and competitive inventory of business and industrial sites with excellent infrastructure, telecommunications capacity and transportation access. Continue to improve the image of this area by setting higher architectural and landscaping standards for the development of new buildings and the redevelopment of existing buildings. Prepare a plan and design guidelines for development along I-494 with increased architectural design, landscaping, and use standards to provide a positive image of the city as seen from I-494.
- Policy 5.3.4: Prioritize opportunities to bring more amenities to Kaposia Landing and the development node at Concord Street and Bryant Avenue, which leads right into Kaposia Landing. Study and plan for development/redevelopment of these areas.
- Policy 5.3.5: Work together with private developers and granting agencies to enhance the attractiveness and draw of the riverfront area.
- Policy 5.3.6: Encourage creative concepts that will enhance the Concord Exchange, Hardman Triangle, and South Concord Corridor (the area along Concord from 494 to the City's southern border). Explore new design standards for Concord Exchange, Hardman Triangle and the South Concord Corridor. Study and plan for development/redevelopment of these areas.
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- » Safe intersection treatments for pedestrians and bicyclists that utilize an appropriate combination of signalization, pavement marking, and physical improvements.
- » Ongoing sidewalk maintenance and pavement re-striping.

Trail Access Points

The following are the points where the regional trails can be accessed in South St. Paul:

- » Kaposia Park
- » 19th Avenue
- » Simon's Ravine Trailhead
- » Kaposia Landing park and at Bryant Avenue in Kaposia Landing (Bryant Ave./Concord St. intersection)
- » The Spiral Bridge at Grand Avenue East and Hardman Avenue
- » The DNR Boat Launch site off of Verderosa Avenue
- » Richmond Street at Hardman Avenue

WHAT WE HEARD

During the community engagement events held as part of this planning process, City residents and community members mentioned the need for usable sidewalks throughout the City. Currently, there are some sections of the City lacking complete sidewalks while others are in disrepair. Residents stated that sidewalks should be ADA-accessible and should be maintained and cleared of snow and ice throughout the winter months, especially at the corners.

Sidewalk System

Other than the trails, the City has an extensive sidewalk system (Figure 7.11). There are, however, areas that need improved sidewalk facilities:

- » Southview Boulevard from 20th Street to West St. Paul border.
- » North Concord Street from Grand Avenue to St. Paul border.
- » 19th Avenue North from Bromley Street to Butler Avenue.
- » 7th Avenue South from I-494 to Inver Grove Heights border.
- » East-West trail/sidewalk connections between Lorraine Park and Roosevelt Parks and the existing (mostly) North-South network of sidewalks.

The addition of these sidewalk segments would allow pedestrian access to all facilities, schools, neighboring cities, and commercial access in and near the City.

The City has established a maintenance program for its sidewalks. This program replaces segments in need of repair and assesses the cost to the adjacent homeowner. A program has not been established to implement new segments of sidewalk as identified above. The City will work with Dakota County to develop a maintenance program for the regional trail.

In addition to physical improvements, education and encouragement strategies are also needed to give people more information about how and where to walk and bike safely, as well as how to drive safely around those who are biking and walking. Education and encouragement can be done through community outreach/communications, a Safe Routes to School plan/program, and enforcement.

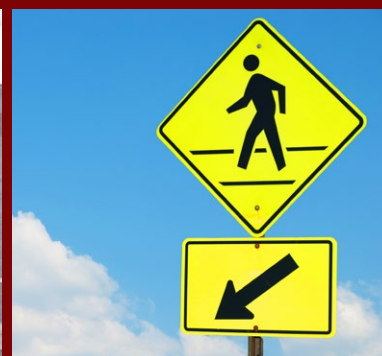
Goal 9.11: Maintain strong partnerships with the public schools and operators of semi-public places.

Policy 9.11.1: Continue to collaborate with the School District to offer sufficient recreational and community-oriented programming for youth and families.

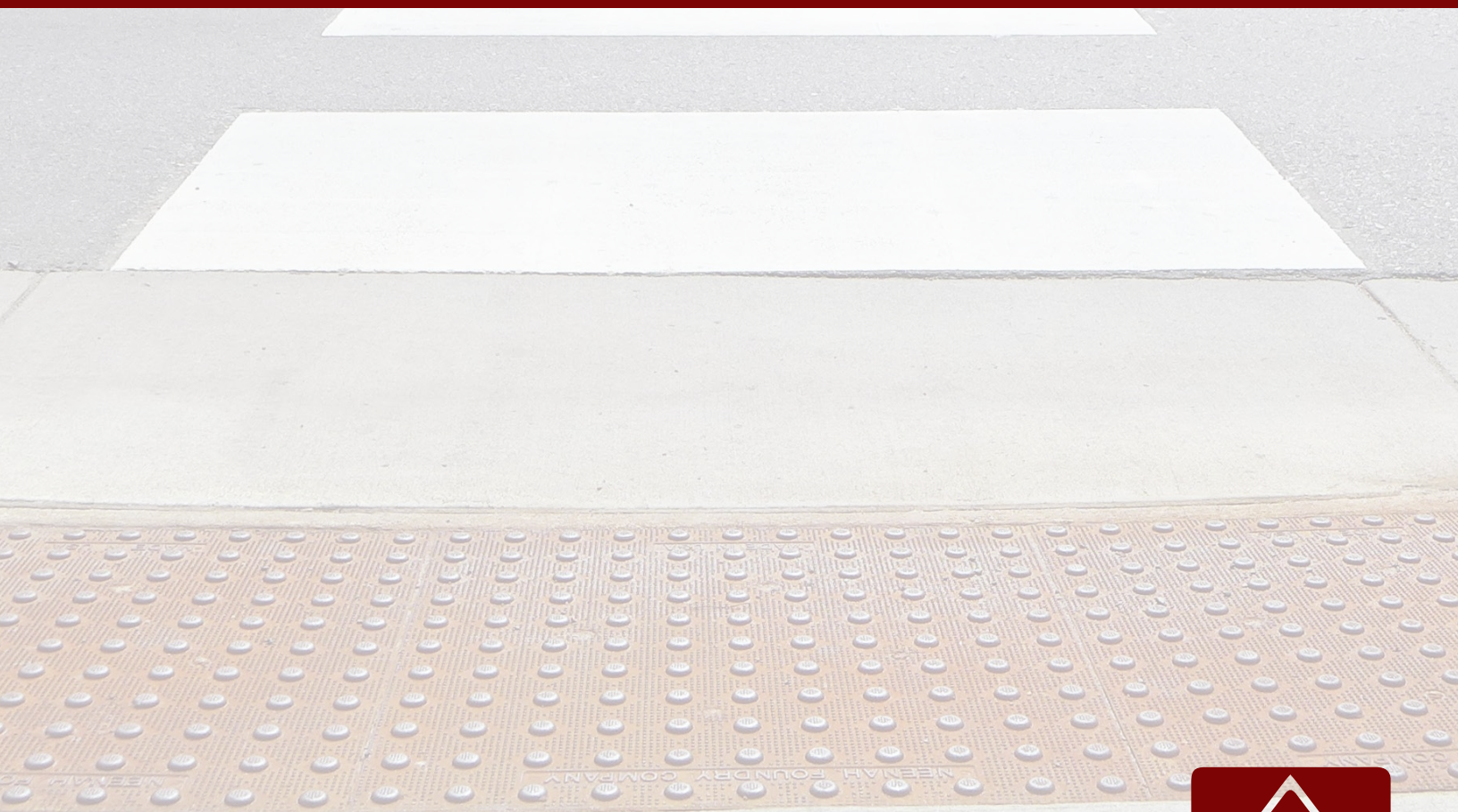
Policy 9.11.2: Ensure that City athletic facilities efficiently and effectively serve the needs of the community in concert with other providers such as the school district.

Table 9.3: Parks, Open Space, Trails, & Recreation Actions

| Action | Time Frame | Same As |
|---|-----------------------------|---------------------------|
| Action 9.1: Follow the Parks Master Plan to continue to improve Kaposia Landing into a premier community park for the city » Focus on implementing Phase II and optimizing the connection with Bryant Avenue to the North Concord Corridor » Construct the Mississippi River Regional Trail (MRRT) extension to the northern border of the city in 2019 | 5-10 5-10 0-5 | |
| Action 9.2: Construct a trail connection at Bromley Street; signage, information kiosk to show linkage to Kaposia Park and Dakota County River to River Greenway Trail (R2RG) | 10+ | Action 7.4 |
| Action 9.3: Plan and implement bank erosion control and stabilization measures in Simon's Ravine and Kaposia Park Ravine, particularly west of 19th Avenue on the north side of the ravine. | 10+ | |
| Action 9.4: Work with the School District to identify the highest and best uses for the former Jefferson School site, with possibility of residential uses and the inclusion of some neighborhood park space on this parcel to fill a gap in the park service area. | 5-10 | Action 4.9; Action 6.3 |
| Action 9.5: Develop the Wakota Trailhead and Overlook at the DNR Boat Launch site and adjacent trail property » Include a multi-purpose park building with restrooms, information kiosk at the DNR boat launch » The building could serve as a southern trailhead for the Mississippi River Regional Trail (MRRT) | 5-10 | |
| Action 9.6: Convert the portion of the former Metropolitan Council Environmental Services (MCES) land along the Mississippi River into recreational facilities or open space for the City » Per the standards and guidelines of Executive Order 79-19 » This shall include working with MRCCA to improve and restore natural habitat and restore natural vegetation | 5-10 | *Critical Area 1 |
| Action 9.7: Develop a citywide trail map, especially for off-street trails » Identify and prioritize missing connections between city sidewalks and trails | 5-10 (ongoing) | Action 7.7 |



City of South St. Paul ADA Transition Plan



January 2018



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INTRODUCTION

Transition Plan Need and Purpose

The Americans with Disabilities Act (ADA), enacted on July 26, 1990, is a civil rights law prohibiting discrimination against individuals on the basis of disability. ADA consists of five titles outlining protections in the following areas:

- I. Employment
- II. State and local government services
- III. Public accommodations
- IV. Telecommunications
- V. Miscellaneous Provisions

Title II of ADA pertains to the programs, activities and services public entities provide. As a provider of public transportation services and programs, City of South St. Paul must comply with this section of the Act as it specifically applies to public service agencies. Title II of ADA provides that, "...no qualified individual with a disability shall, by reason of such disability, be excluded from participation in or be denied the benefits of the services, programs, or activities of a public entity, or be subjected to discrimination by any such entity." ([42 USC. Sec. 12132](#); [28 CFR. Sec. 35.130](#))

As required by Title II of [ADA, 28 CFR. Part 35 Sec. 35.105 and Sec. 35.150](#), the City of South St. Paul has conducted a self-evaluation of its facilities within public rights of way and has developed this Transition Plan detailing how the organization will ensure that those facilities are accessible to all individuals. A glossary of terms is included in **Appendix A**.

This Transition Plan has been created to specifically cover accessibility within the public rights of way and does not include information on City programs, practices, or building facilities not related to public rights of way.

ADA and its Relationship to Other Laws

Title II of ADA is companion legislation to two previous federal statutes and regulations: the [Architectural Barriers Acts of 1968](#) and [Section 504 of the Rehabilitation Act](#) of 1973.

The Architectural Barriers Act of 1968 is a Federal law that requires facilities designed, built, altered or leased with Federal funds to be accessible. The Architectural Barriers Act marks one of the first efforts to ensure access to the built environment.

Section 504 of the Rehabilitation Act of 1973 is a Federal law that protects qualified individuals from discrimination based on their disability. The nondiscrimination requirements of the law apply to employers and organizations that receive financial assistance from any Federal department or agency. Title II of ADA extended this coverage to all state and local government entities, regardless of whether they receive federal funding or not.

Agency Requirements

Under Title II, the City of South St. Paul must meet these general requirements:

- Must operate their programs so that, when viewed in their entirety, the programs are accessible to and useable by individuals with disabilities ([28 CFR Sec. 35.150](#)).
- May not refuse to allow a person with a disability to participate in a service, program or activity simply because the person has a disability ([28 CFR Sec. 35.130 \(a\)](#)).
- Must make reasonable modifications in policies, practices and procedures that deny equal access to individuals with disabilities unless a fundamental alteration in the program would result ([28 CFR Sec. 35.130\(b\) \(7\)](#)).
- May not provide services or benefits to individuals with disabilities through programs that are separate or different unless the separate or different measures are necessary to ensure that benefits and services are equally effective ([28 CFR Sec. 35.130\(b\)\(iv\) & \(d\)](#)).
- Must take appropriate steps to ensure that communications with applicants, participants and members of the public with disabilities are as effective as communications with others ([28 CFR Sec. 35.160\(a\)](#)).
- Must designate at least one responsible employee to coordinate ADA compliance [[28 CFR Sec. 35.107\(a\)](#)]. This person is often referred to as the "ADA Coordinator." The public entity must provide the ADA coordinator's name, office address, and telephone number to all interested individuals [[28 CFR Sec. 35.107\(a\)](#)].
- Must provide notice of ADA requirements. All public entities, regardless of size, must provide information about the rights and protections of Title II to applicants, participants, beneficiaries, employees, and other interested persons [[28 CFR Sec. 35.106](#)].
- Must establish a grievance procedure. Public entities must adopt and publish grievance procedures providing for prompt and equitable resolution of complaints [[28 CFR Sec. 35.107\(b\)](#)]. This requirement provides for a timely resolution of all problems or conflicts related to ADA compliance before they escalate to litigation and/or the federal complaint process.

SELF-EVALUATION CONDITION ASSESSMENT

Overview

The City of South St. Paul is required, under Title II of the Americans with Disabilities Act (ADA) and [28 CFR 35.105](#), to perform a self-evaluation of its current transportation infrastructure policies, practices, and programs. This self-evaluation will identify what policies and practices impact accessibility and examine how the City implements these policies.

The goal of the self-evaluation is to verify that, in implementing the City's policies and practices, the department is providing accessibility and not adversely affecting the full participation of individuals with disabilities.

The self-evaluation also examines the condition of the City's Pedestrian Circulation Route/Pedestrian Access Route) (PCR/PAR) and identifies potential need for PCR/PAR infrastructure improvements. This includes consideration of the sidewalks, bicycle/pedestrian trails, and curb ramps that are located within the City rights of way.

Summary

In 2017, the City of South St. Paul conducted an inventory of pedestrian facilities within its public right of way consisting of the evaluation of the following facilities:

- Pedestrian Ramps at intersections and mid-block crossings that include trail or sidewalk facilities
- Sidewalks and trails adjacent to roadways were assessed by City Staff

Pedestrian ramps were assessed by Stonebrooke staff and categorized into three priority rating tiers:

1. Tier 1: largely or fully compliant.
2. Tier 2: substantially compliant and working well.
3. Tier 3: several elements are not compliant.

Sidewalks adjacent to roadways were assessed by City staff and assigned priority rating tiers. Assessments were done at locations where apparent deficiencies existed.

1. Tier 1: minor pavement deficiencies
2. Tier 2: vertical and gap faults greater than ½ inch.
3. Tier 3: obstacles in sidewalk, vertical faults greater than ¾ inch, gap greater than ½ inch.

Maps showing how the pedestrian ramp and sidewalk facilities are categorized can be found on the City's website, detailed in **Appendix B**, and will be updated periodically.

POLICIES AND PRACTICES

Previous Practices

Since the adoption of the ADA, the City of South St. Paul has striven to provide accessible pedestrian features as part of the City capital improvement projects. As additional information was made available as to the methods of providing accessible pedestrian features, the City has updated their procedures to accommodate these methods. Recently, more standardized design and construction methods have evolved. This has resulted in the ability of local agencies to receive additional exposure and training on accessible features. This has improved the City of South St Paul staff's ability to understand available options and to explore the feasibility of implementing accessibility improvements. This information also assists in providing guidance for developing transition plans.

Policy

The City of South St. Paul will inspect, inventory and plan for any required improvements to facilities located in the public right-of-way, to ensure compliance with the ADA. The City's goal is to continue to provide accessible pedestrian design features as part of the City capital improvement projects. The City has established ADA design standards and procedures as detailed in **Appendix C**. These standards and procedures will be kept up to date with nationwide and local best management practices.

The City of South St. Paul will consider and respond to all accessibility improvement requests. Requests should be sent to the ADA Coordinator as specified in **Appendix D**. All accessibility improvements that have been deemed reasonable will be scheduled consistent with transportation priorities. The City will coordinate with external agencies as necessary to ensure that all new or altered pedestrian facilities within the City jurisdiction are ADA compliant to the maximum extent feasible.

Maintenance of pedestrian facilities within the public right of way will continue to follow the policies set forth by the City. The City will maintain and update the facility database to reflect improvements to inventoried facilities and measure progress.

ADA COORDINATOR

In accordance with [28 CFR 35.107\(a\)](#), the City of South St. Paul has identified an ADA Title II Coordinator to oversee the City policies and procedures. It is the responsibility of the ADA Coordinator to implement this policy. Contact information for this individual is located in **Appendix D**.

IMPROVEMENT SCHEDULE

Priority Areas

The City of South St. Paul has established a tiering system to prioritize ADA improvements based on the level of compliance of facilities. Additional priority will be given to any location where an improvement project or alteration was constructed after January 26, 1991, and accessibility features were omitted.

External Agency Coordination

Many other agencies are responsible for pedestrian facilities within the jurisdiction of the City of South St. Paul, including Dakota County and MNDOT. The City will coordinate with those agencies to track and assist in the facilitation of the elimination of accessibility barriers along their routes and/or associated with their services.

Schedule

The City of South St. Paul has set the following schedule goals for improving the accessibility of its pedestrian facilities within the City jurisdiction:

Pedestrian Ramps

- Ramps with priority ratings in Tier 1. These ramps are considered largely or fully compliant and work on these facilities is not necessary at this time.
- Ramps with priority ratings in Tier 2. These ramps are considered serviceable and are not in need of immediate action. Improvements for these facilities will be addressed in conjunction with adjacent capital improvement projects. Staff will use the CIP and long-range street improvement plans to coordinate these improvements.
- Ramps with priority ratings in Tier 3. Any of these ramps identified as an existing hazard or compliance issue that staff believes needs to be addressed by a set date shall have a

work order initiated or be incorporated into a project in the Capital Improvement Plan (CIP).

Sidewalks

- Sidewalks with priority ratings in Tier 1. These facilities have minor pavement deficiencies and work on these facilities is not necessary at this time.
- Sidewalks with condition ratings in Tier 2. These sidewalks have vertical and gap faults greater than ½ inch but less than ¾ inch and are considered serviceable and are not in need of immediate action. Improvements for these facilities will be addressed in conjunction with adjacent capital improvement projects. Staff will use the CIP and long-range street improvement plans to coordinate these improvements.
- Sidewalks with condition ratings in Tier 3. These sidewalks have obstacles and/or vertical faults greater than ¾ inch, and gap faults greater than ½ inch. Any of these sidewalk locations identified as an existing hazard or compliance issue that staff believes needs to be addressed by a set date shall have a work order initiated or be incorporated into a project in the CIP.

After 20 years, the City of South St. Paul has a goal for 80% of accessibility for pedestrian features within the jurisdiction to be ADA compliant. The remaining 20% would include the Tier 2 locations that have not had an adjacent road project within the twenty-year period.

IMPLEMENTATION SCHEDULE

Methodology

The City of South St. Paul will utilize two methods for upgrading pedestrian facilities to the current ADA standards. The first and most comprehensive of the two methods are the scheduled street and utility improvement projects. All pedestrian facilities impacted by these projects will be upgraded to current ADA accessibility standards. The second method includes standalone sidewalk and ADA accessibility improvement projects. These projects will be incorporated into the CIP on a case by case basis as determined by the City of South St. Paul staff, or may be completed by internal City forces. The City CIP includes a detailed schedule and budget for specific improvements.

PUBLIC OUTREACH

The City of South St. Paul recognizes that public participation is an important component in the development of this plan. The City has developed a webpage that provides information on the Plan development and provides opportunity for public input.

Public outreach for the creation of this document consisted of the following activities:

- The City's ADA webpage includes information on the Transition Plan development and provides an opportunity for public comment.
- Transition Plan presentation to the City Council.
- The City's ADA Title II Coordinator will continue to be available for questions or discussion.

GRIEVANCE PROCEDURE

Under the Americans with Disabilities Act, each agency is required to publish its responsibilities in regards to the ADA. A draft of this public notice is provided in **Appendix G**. If users of City of South St. Paul facilities and services believe the City has not provided reasonable accommodation, they have the right to file a grievance.

In accordance with [28 CFR 35.107\(b\)](#), the City has developed a grievance procedure for the purpose of the prompt and equitable resolution of citizens' complaints, concerns, comments, and other grievances. This grievance procedure is outlined in **Appendix H**, with a Grievance Form in **Appendix I**.

MONITOR THE PROGRESS

This document, including the Appendices, will be updated as conditions within the City evolve. The City will maintain ADA Transition Plan information on its website. Reporting on facility accessibility improvements will be included in CIP updates and other presentations to the City Council.

APPENDICES

- A. Glossary of Terms
- B. Self-Evaluation
- C. Agency ADA Design Standards and Procedures
- D. ADA Coordinator
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